

THE STATE OF DELAWARE DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY
UNITS



CONSTRUCTION PLANS FOR:

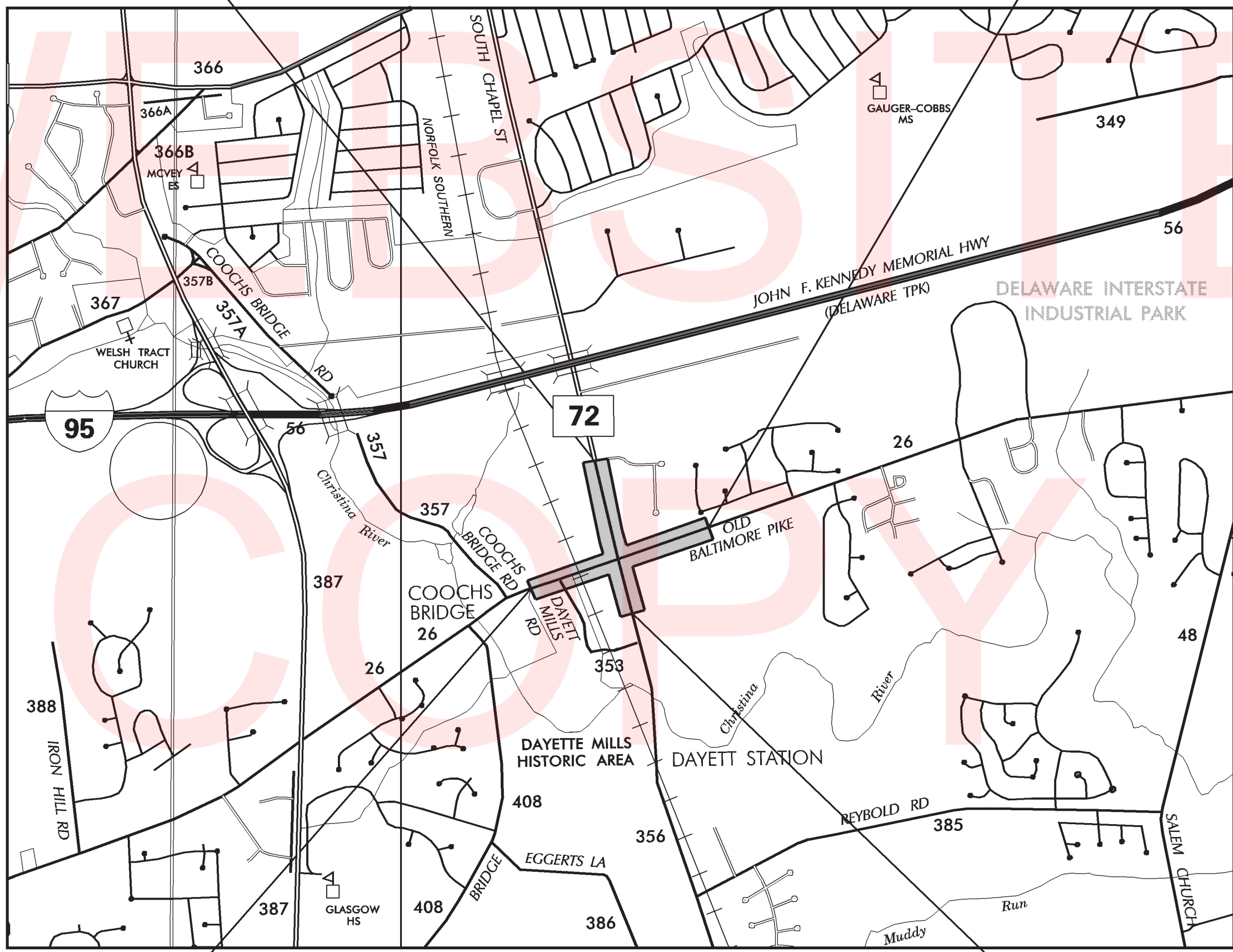
HEP NCC, SR 72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS

CONTRACT NUMBER: **T201200108**
FEDERAL AID PROJECT NUMBER: **ESTP-N026(09)**

COUNTY: **NEW CASTLE** M.R. #: **N356**

**LIMIT OF CONTRACT
STATION 121+27**

**END CONTRACT
STATION 18+25**



**BEGIN CONTRACT
STATION 1+20**

**LIMIT OF CONTRACT
STATION 103+65**

DESIGN DESIGNATION: OLD BALTIMORE PIKE		
FUNCTIONAL CLASS: MINOR ARTERIAL	D.H.V. PROJECTED: 1242	YEAR: 2040
TYPE OF CONSTRUCTION: INTERSECTION IMPROVEMENT	DESIGN SPEED: 50 M.P.H.	
A.A.D.T. CURRENT: 16761	YEAR: 2011	TRUCKS: 8 %
A.A.D.T. PROJECTED: 23000	YEAR: 2040	DIRECTION OF DISTRIBUTION: 60 %

DESIGN DESIGNATION: STATE ROUTE 72		
FUNCTIONAL CLASS: MINOR ARTERIAL	D.H.V. PROJECTED: 2320	YEAR: 2040
TYPE OF CONSTRUCTION: INTERSECTION IMPROVEMENT	DESIGN SPEED: 50 M.P.H.	
A.A.D.T. CURRENT: 32409	YEAR: 2011	TRUCKS: 12 %
A.A.D.T. PROJECTED: 40000	YEAR: 2040	DIRECTION OF DISTRIBUTION: 58 %

INDEX OF SHEETS	
SHEET NO	TABLE OF CONTENTS
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49-52	SIGNALIZATION PLANS

TOTAL SHEETS: 52

APPROVED DESIGN EXCEPTIONS			
DESIGN PARAMETER	REQUIRED	PROVIDED	DATE

ADDENDA & REVISIONS	
DESCRIPTION	NAME & DATE

ASSOCIATED CONTRACTS	
CONTRACT NO.	CONTRACT NAME
79-105-01	OLD BALTIMORE PIKE, SR 72 TO SR 273 (CHRISTIANA BYPASS)
20-015-03	SR 72 BIKEWAY EXTENSION, DAYETT MILLS TO I-95
82-105-01	SR 72, I-95 TO OLD BALTIMORE PIKE

RECOMMENDED

Edward C. Fisher 03/03/2017
SQUAD MANAGER, CONSTRUCTION DATE

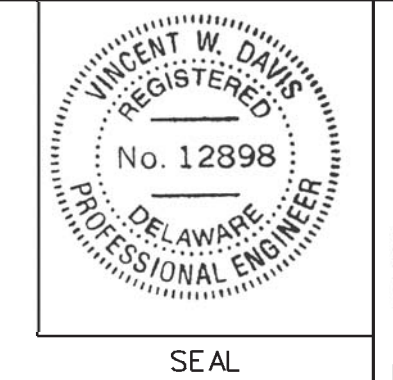
Christopher Costello 03/15/2017
GROUP ENGINEER, CONSTRUCTION DATE

Jim A. Zujewski 03/15/2017
ASSISTANT DIRECTOR, TRANSPORTATION SOLUTIONS (CONSTRUCTION) DATE

RECOMMENDED

Vincent W. Davis
STORMWATER ENGINEER

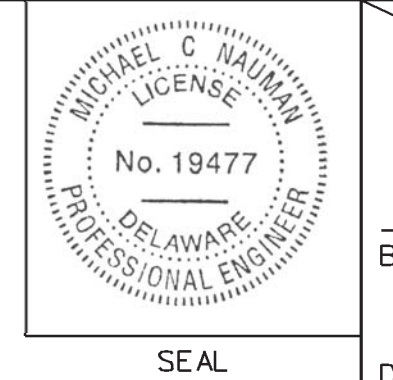
DATE 09/02/2016



RECOMMENDED

Michael Thomas
SQUAD MANAGER, TRANSPORTATION SOLUTIONS (PROJECT DEVELOPMENT OR BRIDGE DESIGN)

DATE 08/31/2016



RECOMMENDED

[Signature]
BRIDGE DESIGN ENGINEER

DATE

RECOMMENDED

[Signature]
GROUP ENGINEER, PROJECT DEVELOPMENT

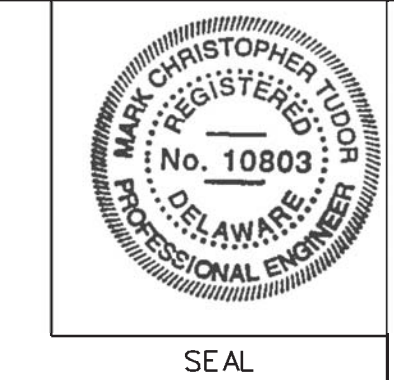
DATE 03/16/2017



RECOMMENDED

Mark C. Tucker
ASSISTANT DIRECTOR, TRANSPORTATION SOLUTIONS

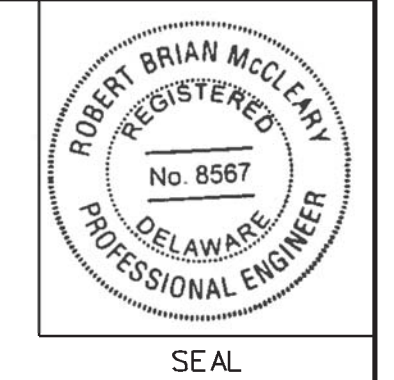
DATE 03/16/2017

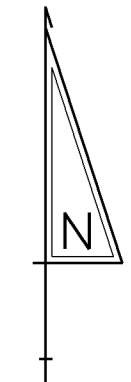


APPROVED

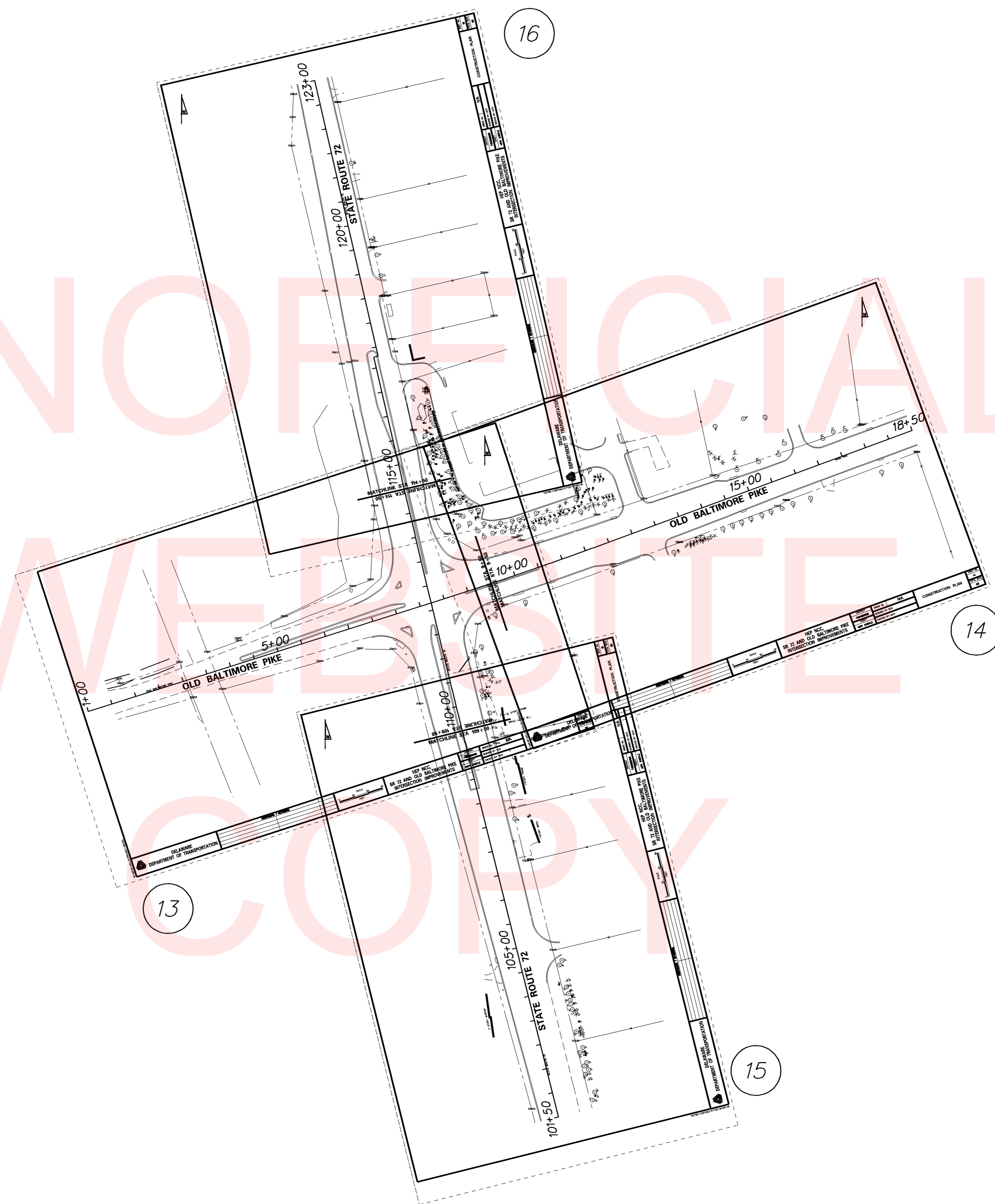
Robert Brian McCleary
CHIEF ENGINEER

DATE 03/17/2017





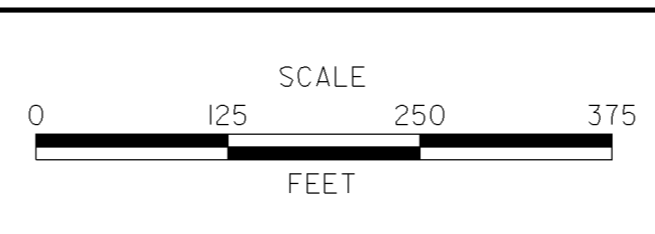
PLAN SHEET INDEX CROSS REFERENCE				
CONSTRUCTION PLAN	13	14	15	16
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CONSTRUCTION PHASING, M.O.T. AND E&S	26-36	26-36	26-36	26-36
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DELAWARE
DEPARTMENT OF TRANSPORTATION

ADDENDUMS / REVISIONS



**HEP NCC,
 SR 72 AND OLD BALTIMORE PIKE
 INTERSECTION IMPROVEMENTS**

CONTRACT T201200108	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: BCD CHECKED BY: MCN

PLAN SHEET INDEX

SHEET NO. 2
TOTAL SHTS. 52

EXISTING SYMBOLS

DRAINAGE	
	DITCH OR STREAM CENTERLINE
	DIRECTIONAL STREAM FLOW ARROW
	DRAINAGE INLET
	DRAINAGE JUNCTION BOX
	DRAINAGE MANHOLE
	DRAINAGE PIPE AND FLOW ARROW
	DRAINAGE PIPE HEADWALL
	RIPRAP - AREA FEATURE
	RIPRAP - LINEAR FEATURE

MANMADE ROADSIDE FEATURES	
	BOLLARD - STEEL POLE
	BOLLARD - WOOD POST
	CURB
	CURB AND GUTTER
	FENCE - CHAINLINK OR STRANDED
	FENCE - STOCKADE OR SPLIT RAIL
	FLAG POLE
	GUARDRAIL - STEEL BEAM
	GUARDRAIL - WIRE ROPE
	LAMP AND POST - RESIDENTIAL
	MAILBOX
	PARKING METER AND POST
	PAVEMENT - FLEXIBLE
	PAVEMENT - RIGID
	PILE - BRIDGE
	PILLAR OR MISCELLANEOUS POST
	TRAFFIC SIGN AND POST
	WALL - BRICK OR BLOCK
	WALL - STONE

NATURAL ROADSIDE FEATURES	
	GRASS LAWN
	HEDGEROW OR THICKET
	MARSH BOUNDARY LINE
	TREE - CONIFEROUS
	TREE - DECIDUOUS
	TREE STUMP
	SHRUBBERY
	DELINEATED WETLAND BOUNDARY LINE
	WOODS LINE BOUNDARY

RIGHT-OF-WAY SYMBOLS	
	PROPERTY MARKER - CONCRETE MON.
	PROPERTY MARKER - IRON PIPE
	HISTORIC RIGHT-OF-WAY BASELINE
	EXISTING RIGHT-OF-WAY
	EXISTING PROPERTY LINE
	EXISTING EASEMENT
	EXISTING DENIAL OF ACCESS
	EXISTING R/W & DENIAL OF ACCESS

SURVEY CONTROL & MONUMENTATION	
	SURVEY BENCHMARK LOCATION
	SURVEY TIE POINT LOCATION
	SURVEY TRAVERSE POINT
	POINT OF CURVATURE OR TANGENCY
	POINT OF INTERSECTING TANGENTS

UTILITY	
	SOIL BORING LOCATION
	UTILITY TEST HOLE LOCATION
	CABLE TV DISTRIBUTION BOX
	ELECTRIC MANHOLE
	ELECTRIC METER
	ELECTRIC TRANSFORMER
	POLE MOUNTED LUMINAIRE
	GAS MANHOLE
	GAS METER
	GAS VALVE
	GAS PUMP - SERVICE STATION
	RAILROAD TRACKS
	SANITARY SEWER MANHOLE
	SANITARY SEWER VALVE
	SANITARY SEWER CLEANOUT OR VENT
	SEPTIC DRAIN FIELD
	TELEPHONE BOOTH
	TELEPHONE MANHOLE
	TELEPHONE TEST POINT
	TRAFFIC - CONDUIT JUNCTION WELL
	TRAFFIC - LIGHT POLE AND BASE
	TRAFFIC - PEDESTRIAN POLE & BASE
	TRAFFIC - SIGNAL CABINET & BASE
	TRAFFIC - SIGNAL POLE AND BASE
	UTILITY BOX
	UTILITY POLE
	UTILITY POLE GUY WIRE ANCHOR
	WATER - FIRE HYDRANT
	WATER METER
	WATER VALVE
	WELL HEAD
	MANHOLE - UNDETERMINED OWNER

UTILITY COMPANY FACILITIES	
	CITY OF NEWARK - WATER
	COMCAST CABLE
	DELDOT MULTIDUCT CONDUIT - EXISTING
	DELMARVA POWER - ELECTRIC
	DELMARVA POWER - GAS
	NEW CASTLE COUNTY - SEWER
	UNITED WATER
	VERIZON
	PEG BANDWIDTH - CONDUIT

PROPOSED SYMBOLS

CONSTRUCTION	
	CONCRETE SAFETY BARRIER - PERMANENT
	BIOFILTRATION SWALE
	BRICK PATTERNED SURFACE
	BUTT JOINT
	CLEAR ZONE
	CONSTRUCTION BASELINE
	CONSTRUCTION SAFETY FENCE
	CURB, TYPE 1 & TYPE 3
	CURB, TYPE 2
	CURB & GUTTER, TYPE 1
	CURB & GUTTER, TYPE 2
	CURB & GUTTER, TYPE 3
	CURB & GUTTER, TYPE 4
	CURB OPENING
	DRAINAGE INLET
	DITCH
	FENCE - METAL
	FENCE - WOOD
	FLARED END SECTION
	GUARDRAIL, TYPE 1
	GUARDRAIL, TYPE 2
	GUARDRAIL, TYPE 3
	GUARDRAIL END ANCHORAGE
	GUARDRAIL END TREATMENT, TYPE 1
	GUARDRAIL END TREATMENT, TYPE 2
	GUARDRAIL END TREATMENT, TYPE 3
	IMPACT ATTENUATOR
	JUNCTION BOX - DRAINAGE
	LATERAL OFFSET
	LIMIT OF CONSTRUCTION
	MAILBOX
	MANHOLE
	PAVEMENT PATCH
	PAVEMENT REMOVAL - TOPSOIL, SEED AND MULCH
	PIPE & DIRECTIONAL FLOW ARROW
	RIPRAP
	P.C.C. SIDEWALK - 4"
	P.C.C. SIDEWALK - 6" (USE 8" DEPTH FOR CHANNELIZATION ISLANDS.)
	UNDERDRAIN
	UNDERDRAIN OUTLET

RIGHT-OF-WAY SYMBOLS	
	PROPOSED RIGHT-OF-WAY MONUMENT
	PROPOSED DENIAL OF ACCESS
	PROPOSED PERMANENT EASEMENT
	PROPOSED RIGHT-OF-WAY
	PROPOSED R/W & DENIAL OF ACCESS
	TEMPORARY CONSTRUCTION EASEMENT
	PROPOSED RIGHT-OF-WAY BASELINE

IDENTIFIERS	
	ADJUST BY CONTRACTOR
	ADJUST BY OTHERS
	CONCRETE SAFETY BARRIER
	CURB OR CURB & GUTTER
	CONVERT TO JUNCTION BOX
	CONVERT TO DRAINAGE MANHOLE
	CURB OPENING
	CURB RAMP / TYPE
	CURB RAMP / TYPE - WITHOUT SIDEWALK SURFACE DETECTABLE WARNING SYSTEM
	CONSTRUCTION SAFETY FENCE
	DRAINAGE INLET
	DO NOT DISTURB
	ENERGY DISSIPATOR
	FENCE
	FLARED END SECTION
	FILL WITH FLOWABLE FILL
	FILTRATION STRUCTURE
	GUARDRAIL
	JUNCTION BOX
	MANHOLE
	MONUMENT - RIGHT-OF-WAY
	PIPE
	RELOCATE BY CONTRACTOR
	RELOCATE BY OTHERS
	REMOVE BY CONTRACTOR
	REMOVE BY TRAFFIC CONTRACTOR
	REMOVE BY OTHERS
	UNDERDRAIN / LENGTH
	UNDERDRAIN OUTLET PIPE

LANDSCAPING	
	LANDSCAPE PLANTINGS
	SHRUBBERY
	CONIFEROUS TREE
	DECIDUOUS TREE

TRAFFIC	
	ITMS CONDUIT
	SIGNAL CONDUIT
	CONDUIT JUNCTION WELL
	LUMINAIRE
	PAVEMENT MARKINGS
	PAVEMENT STRIPING
	TRAFFIC SIGN

PAVEMENT SECTION(S)	
	MILL AND OVERLAY PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS
	RECONSTRUCTED PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS
	DRIVEWAY AND ENTRANCE PAVEMENT - SEE PROJECT NOTES FOR MATERIALS AND DEPTHS

MISCELLANEOUS	
	CAP WATER LINE BY CONTRACTOR

UTILITY COMPANY FACILITIES	
	DELMARVA POWER - GAS
	VERIZON CABLE OVERHEAD
	DELMARVA POWER - ELECTRIC OVERHEAD

LAST REVISED: 05/12/2016
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GENERAL NOTES

1. THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS", DATED AUGUST 2001 AND THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD CONSTRUCTION DETAILS", DATED 2001, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT.

EROSION POTENTIAL FOR THIS PROJECT	CONTRACTOR ESC SUPERVISOR REQUIREMENT
() INSIGNIFICANT	NONE
(X) MINOR	CONTRACTOR TRAINING PROGRAM, AS DEFINED IN SECTION 6.2 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
() MAJOR	CERTIFIED CONSTRUCTION REVIEWER (CCR), AS DEFINED IN SECTION 6.3 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.

3. ELECTRONIC PROJECT FILES THAT WILL BE MADE AVAILABLE TO THE AWARDED CONTRACTOR, INCLUDE:

()	NONE
(X)	ASCII DATA FILES WITH COORDINATES AND ELEVATIONS FOR PROPOSED POINTS AS SELECTED BY THE ENGINEER.
(X)	ALL PLAN SHEETS, IN PDF FORMAT.
(X)	EXISTING DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
()	PROPOSED DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
()	DESIGN FILE, IN .DGN FILE FORMAT, CONTAINING ONLY THE PROPOSED 3D TRIANGLES OF THE PROPOSED DIGITAL TERRAIN MODEL (DTM).

NOTE: THE DOCUMENT ENTITLED "RELEASE FOR DELIVERY OF DOCUMENTS IN ELECTRONIC FORM TO A CONTRACTOR" MUST BE SIGNED BY ALL PARTIES PRIOR TO THE DELIVERY OF ANY ELECTRONIC PROJECT FILES.

4. PROJECT FILES THAT WILL BE MADE AVAILABLE TO THE CONTRACTOR, INCLUDE:

(X)	CROSS SECTIONS
(X)	RIGHT-OF-WAY PLANS (WILL BE MADE AVAILABLE TO THE AWARDED CONTRACTOR)

5. AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR REQUIREMENT FOR THIS PROJECT.

()	THE CONTRACTOR SHALL NOT BE REQUIRED TO HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT.
(X)	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT OR ANOTHER ATSSA CERTIFIED MEMBER OF THE CONTRACTOR'S PROJECT STAFF MAY BE THE ATSSA SUPERVISOR. PAYMENT FOR ATSSA SUPERVISOR IS INCIDENTAL TO ITEM 743000.
()	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE ATSSA SUPERVISOR'S SOLE JOB SHALL BE SUPERVISION OF THE INSTALLATION, OPERATION AND MAINTENANCE OF TRAFFIC CONTROL DEVICES FOR THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT SHALL NOT BE THE ATSSA SUPERVISOR. PAYMENT FOR ATSSA SUPERVISOR SHALL BE PAID FOR UNDER ITEM 743031.

6. THE DISTURBED AREA FOR THIS PROJECT IS 1.45 ACRES.
7. THE SEDIMENT AND STORMWATER MANAGEMENT PLANS HAVE BEEN APPROVED BY DELDOT'S STORMWATER ENGINEER UNDER DELDOT'S DELEGATED AUTHORITY. THE SEDIMENT AND STORMWATER MANAGEMENT PLANS ARE VALID FOR A THREE YEAR PERIOD, BEGINNING ON THE DATE THE STORMWATER ENGINEER SIGNED THE CONSTRUCTION TITLE SHEET. IF THE FINAL ACCEPTANCE OF THE PROJECT IS ANTICIPATED TO EXTEND BEYOND THE THREE YEARS, THE CONTRACTOR WILL INFORM THE ENGINEER THREE MONTHS PRIOR TO THE EXPIRATION OF THE APPROVED SEDIMENT AND STORMWATER MANAGEMENT PLANS. THE STORMWATER ENGINEER WILL REVIEW THE CURRENT SEDIMENT AND STORMWATER MANAGEMENT PLAN AND ISSUE AN EXTENSION WITH ANY APPROPRIATE MODIFICATIONS.

PROJECT NOTES

SECTION 100

1. ANY DAMAGE TO ITEMS NOTED TO BE RELOCATED OR RESET BY THE CONTRACTOR, AT THE DISCRETION OF THE ENGINEER, SHALL BE REPAIRED AND/OR REPLACED IN KIND AT THE CONTRACTOR'S EXPENSE.

SECTION 200

2. THE CONTRACTOR SHALL REMOVE AND RESET ALL MAILBOXES TO MAINTAIN MAIL SERVICE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL RELOCATE MAILBOXES AS REQUIRED BY THE PROPOSED GEOMETRICS AND AS DIRECTED BY THE ENGINEER. WHEN RELOCATING MAILBOXES IN CURBED SECTIONS, THE FACE OF THE MAILBOX SHALL BE FLUSH WITH THE BACK EDGE OF CURB. WHEN RELOCATING MAILBOXES IN OPEN SECTIONS, THE FACE OF THE MAILBOX SHALL SET BACK 8 INCHES FROM THE EDGE OF THE PAVED SHOULDER. THE BOTTOM OF THE MAILBOX SHALL BE SET 46 INCHES ABOVE THE ROADWAY SURFACE. MAILBOXES LOCATED AT DRIVEWAY ENTRANCES SHALL BE PLACED ON THE FAR SIDE OF THE DRIVEWAY IN THE DIRECTION OF TRAVEL. POSTS BEING RESET IN CONCRETE SIDEWALK SHALL BE PLACED IN AN APPROPRIATE SIZE PVC SLEEVE. COST FOR ALL WORK AND MATERIALS SHALL BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING.
3. IN AREAS WHERE TREES OR SHRUBS WILL BE OVERHANGING THE PROPOSED SIDEWALK, PRUNING MAY BE NECESSARY TO ACHIEVE A VERTICAL CLEAR SPACE OF 10 FEET ABOVE THE PROPOSED SIDEWALK ELEVATION. THE CONTRACTOR SHALL PRUNE EXISTING TREE AND SHRUB BRANCHES, WHICH OVERHANG THE SIDEWALK, IN ACCORDANCE WITH I.S.A. STANDARDS. THE CONTRACTOR SHALL NOTIFY DELDOT'S ROADSIDE ENVIRONMENTALIST ADMINISTRATOR, BRIAN URBANEK AT (302) 760-2536 AND/OR HIS DESIGNEE, AT LEAST TWO (2) DAYS PRIOR TO THE PRUNING OPERATION. ALL COSTS ASSOCIATED WITH THE ABOVE WORK TO BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING.
4. THE ENGINEER MAY REQUIRE THE CONTRACTOR TO EXCAVATE TEST PITS ALONG PROPOSED DRAINAGE RUNS, AT POINTS OF POSSIBLE UTILITY CONFLICTS, TO DETERMINE IF A CONFLICT EXISTS. ANY CONFLICTS SHALL BE COORDINATED BY THE CONTRACTOR, WITH THE ENGINEER AND THE UTILITY COMPANY INVOLVED. THE ENGINEER SHALL ULTIMATELY DETERMINE THE SOLUTION TO THE UTILITY CONFLICT. TEST HOLES SHALL BE MEASURED AND PAID FOR IN ACCORDANCE WITH ITEM 202573 - TEST HOLES.
5. ITEMS TO BE REMOVED UNDER ITEM 211000 - REMOVAL OF STRUCTURES AND OBSTRUCTIONS SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING:
- BOLLARDS, PEDESTRIAN SIGNAL POLE BASES, SIGNAL POLE BASES, CABINET BASES, JUNCTION WELLS, SIGN BASES, DRAINAGE INLETS, AND HANDRAILS

SECTION 300

6. A. THE CONTRACTOR MAY ELECT TO USE ANY OF THE FOLLOWING MATERIALS TO MEET THE REQUIREMENTS OF ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B':
a. CRUSHED STONE (PER STANDARD SPECIFICATION 821)
b. CRUSHED CONCRETE (PER STANDARD SPECIFICATION 821)
c. HOT-MIX MILLINGS (PER SPECIAL PROVISION 302514 MILLED HOT-MIX BASE COURSE)

THE CONTRACTOR WILL NOT BE ALLOWED TO MIX DIFFERENT MATERIALS (OR SIMILAR MATERIALS FROM DIFFERENT SOURCES) TO MEET THE REQUIREMENTS OF ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'.

ALL OF THE ABOVE LISTED MATERIALS ARE PERMITTED FOR USE ON THE JOB, PROVIDED THEY ARE SEPARATED INTO APPROVED AREAS. EACH AREA OF BASE COURSE MUST BE CONSTRUCTED USING MATERIALS FROM A SINGULAR SOURCE, FULL DEPTH, IN ORDER THAT PROPER TESTING MAY BE ACCOMPLISHED. THE CONTRACTOR AND ENGINEER SHALL AGREE ON THE LIMITS OF EACH SOURCE OF MATERIAL PRIOR TO PLACEMENT.

B. THE QUANTITY USED FOR BASE OF EACH OF THE ABOVE LISTED MATERIALS WILL BE THE CONTRACTOR'S CHOICE, WITH THE TOTAL BEING EQUAL TO THE ACTUAL QUANTITY USED UNDER ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'.

C. THE CONTRACTOR MAY ALSO ELECT TO RECYCLE MILLINGS FOR USE IN HOT-MIX AS PERMITTED BY THE STANDARD SPECIFICATIONS. THE CHOICE OF THE QUANTITY OF MILLINGS USED FOR THIS PURPOSE, OR FOR BASE COURSE, LIES WITH THE CONTRACTOR.

D. HOT-MIX MILLINGS MAY BE GENERATED FROM THE FOLLOWING SOURCES:
a. MATERIAL MADE AVAILABLE WHEN MILLED ON THIS CONTRACT UNDER ITEM 760000.
b. MATERIAL MILLED ON THIS CONTRACT AT THE CONTRACTOR'S CHOICE UNDER ITEM 202000.
c. MILLED MATERIAL FURNISHED ON THE JOB FROM THE CONTRACTOR'S YARD OR OTHER OUTSIDE SOURCE.
ALL MILLED MATERIALS SHALL MEET THE MATERIAL REQUIREMENTS OF ITEM 302514 - MILLED HOT-MIX BASE COURSE.

E. PAYMENT CLARIFICATION:

- a. SHOULD THE CONTRACTOR ELECT TO MILL PORTIONS OF HOT-MIX SHOWN ON THE PLANS TO BE REMOVED UNDER ITEM 202000 - EXCAVATION AND EMBANKMENT THE COST OF MILLING THIS HOT-MIX WILL BE PAID AS ITEM 202000 - EXCAVATION AND EMBANKMENT. THE MILLINGS GENERATED MAY BE RECYCLED INTO HOT-MIX, UTILIZED FOR BASE COURSE, OR DISPOSED OF TO AN APPROVED SITE. HAULING COSTS FOR DISPOSAL AND/OR RECYCLING ARE INCIDENTAL TO ITEM 202000 - EXCAVATION AND EMBANKMENT.
- b. MILLINGS GENERATED UNDER ITEM 760000 - PAVEMENT MILLING, HOT MIX MAY BE RECYCLED INTO HOT-MIX, UTILIZED FOR BASE COURSE OR DISPOSED OF BY THE CONTRACTOR TO AN APPROVED SITE. NO SEPARATE PAYMENT WILL BE MADE FOR TRANSPORTING MILLINGS ON SITE OR TO AN APPROVED DISPOSAL SITE.
- c. SHOULD THE CONTRACTOR ELECT TO TEMPORARILY STOCKPILE MILLINGS ON THE JOB SITE FOR LATER USE, ALL COSTS FOR STOCKPILING AND SUBSEQUENT REHANDLING SHALL BE INCIDENTAL TO ITEM 202000 - EXCAVATION AND EMBANKMENT.
- d. MILLINGS USED FOR BASE COURSE SHALL BE PLACED IN ACCORDANCE WITH THE REQUIREMENTS OF SPECIAL PROVISION 302514 - MILLED HOT-MIX BASE COURSE. NO SEPARATE PAYMENT WILL BE MADE TO FURNISH MILLINGS FROM AN OUTSIDE SOURCE OR TRANSPORT MILLINGS WITHIN THE PROJECT LIMITS. MILLINGS USED FOR BASE COURSE WILL BE PAID FOR AT THE UNIT BID PRICE FOR ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'.
- e. ALL COSTS TO UTILIZE MILLINGS IN RECYCLED HOT-MIX WILL BE INCIDENTAL TO THE UNIT PRICE BID FOR THE HOT-MIX ITEM USING THE RECYCLED MATERIAL.

f. SPECIAL PROVISION 302514 - MILLED HOT-MIX BASE COURSE IS PROVIDED TO SPECIFY THE MEANS OF LAY DOWN AND COMPACTION AS WELL AS THE MATERIAL REQUIREMENTS FOR MILLINGS USED AS BASE COURSE. ALL COSTS TO BRING THE MILLINGS INTO COMPLIANCE WITH THE REQUIREMENTS OF ITEM - 302514 MILLED HOT-MIX BASE COURSE ARE INCIDENTAL TO ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'. NO PAYMENT WILL BE MADE FOR ITEM 302514 - MILLED HOT-MIX BASE COURSE. THE QUANTITY OF MILLINGS USED FOR BASE COURSE WILL BE PAID FOR UNDER ITEM 302007 - GRADED AGGREGATE BASE COURSE.

SECTION 400

7. THE PAVEMENT SECTION FOR FLEXIBLE PAVEMENT RESIDENTIAL DRIVEWAYS SHALL BE 2" WARM-MIX, TYPE 'C' OVER 8" GRADED AGGREGATE BASE COURSE, TYPE 'B', UNLESS OTHERWISE NOTED ON THE PLANS.

SECTION 600

8. THE DEPARTMENT AND THE CONTRACTOR SHALL INSPECT ALL EXISTING PIPES AND DRAINAGE STRUCTURES TO BE USED IN THE FINAL DRAINAGE SYSTEM AND AGREE ON THE CONDITION PRIOR TO THE START OF CONSTRUCTION. EXISTING PIPES AND DRAINAGE STRUCTURES DAMAGED DUE TO CONTRACTOR OPERATIONS SHALL BE REPAIRED OR REPLACED IN-KIND AT THE CONTRACTOR'S EXPENSE. THE DEPARTMENT WILL VIDEO INSPECT NEW PIPE RUNS TO CONFIRM CONDITION PRIOR TO ACCEPTANCE. PIPE CLEANING PRIOR TO VIDEO INSPECTION AND MAINTENANCE OF TRAFFIC DURING THE VIDEO INSPECTION ARE THE RESPONSIBILITY OF THE CONTRACTOR AND INCIDENTAL TO THE PIPE ITEM THAT IS BEING VIDEO INSPECTED.

SECTION 700

9. IN AREAS WHERE PROPOSED CURB MEETS EXISTING CURB AND THE TWO CURB TYPES ARE NOT SIMILAR, THE PROPOSED CURB SHALL BE TRANSITIONED IN 10 LINEAR FEET, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. PAYMENT FOR THIS WORK, INCLUDING SAW CUTTING EXISTING CURB SHALL BE INCIDENTAL TO THE PROPOSED CURB ITEM.
10. WHERE PROPOSED CONCRETE SIDEWALK IS CONSTRUCTED TO MEET EXISTING SIDEWALK, THE EXISTING SIDEWALK SHALL BE SAWCUT AT THE TIE-IN POINT OR MEET THE NEAREST EXISTING SIDEWALK JOINT. ALL SAW CUTTING SHALL BE FULL DEPTH, UNLESS OTHERWISE NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER AND SHALL BE PAID FOR UNDER ITEM 762002 - SAWCUTTING, CONCRETE, FULL DEPTH.
11. PORTLAND CEMENT CONCRETE CHANNELIZING ISLANDS THAT ARE LESS THAN 75 SQ FT MAY BE POURED MONOLITHICALLY, OR AS DIRECTED BY THE ENGINEER.
12. STATION AND ELEVATION DATA GIVEN FOR DRAINAGE STRUCTURES ARE TO BE APPLIED TO THE CENTER OF THE GRATE FOR INLETS, AND TO THE CENTER OF THE STRUCTURE FOR JUNCTION BOXES AND MANHOLES.
13. THE NEW CASTLE COUNTY DEPARTMENT OF PUBLIC WORKS SHALL SUPPLY AND THE STATE'S CONTRACTOR SHALL INSTALL NEW SELF SEALING MANHOLE FRAMES AND COVERS ON ALL COUNTY SEWER MANHOLES THAT ARE NOT BEING RELOCATED, WITHIN THE PROJECT LIMITS IN ACCORDANCE WITH THE COUNTY'S STANDARD SPECIFICATIONS. THE EXISTING MANHOLE FRAMES AND COVERS THAT ARE REMOVED SHALL BECOME THE PROPERTY OF THE STATE'S CONTRACTOR. PAYMENT SHALL BE INCIDENTAL TO ITEM 710506 - ADJUST AND REPAIR EXISTING SANITARY SEWER MANHOLE.
14. ALL PAVED AREAS TO BE RECONSTRUCTED OR WIDENED SHALL BE SAWCUT AT THE POINT WHERE THE NEW PAVEMENT IS TO TIE INTO THE EXISTING PAVEMENT.
15. THE CONTRACTOR MUST OBTAIN RAILROAD PROTECTIVE LIABILITY INSURANCE AS OUTLINED IN THE RAILROAD MAINTENANCE OF TRAFFIC SPECIFICATION - 763502. THE RAILROAD AND RAILROAD PROJECT MANAGER MUST BE PRESENT AT THE PRECONSTRUCTION MEETING.
16. ALL WORK ON, OVER, UNDER, OR ADJACENT TO NORFOLK SOUTHERN (NS) RIGHT-OF-WAY SHALL BE DONE IN ACCORDANCE WITH THE NORFOLK SOUTHERN "SPECIAL PROVISIONS FOR THE PROTECTION OF RAILWAY INTERESTS." THE CONTACT INFORMATION FOR NORFOLK SOUTHERN RAILROAD IS AS FOLLOWS:

NORFOLK SOUTHERN CORPORATION
BRIDGES AND STRUCTURES DEPARTMENT
ATTN: MR. SCOTT OVERBEY, ENGINEER - PUBLIC IMPROVEMENTS
1200 PEACHTREE STREET NE
ATLANTA, GA 30309
PHONE: (404) 582-5588
EMAIL: SCOTT.OVERBEY@NSCORP.COM
17. WHEN WORKING WITHIN THE RAILROAD RIGHT OF WAY, A DELDOT INSPECTOR FROM THE RAILROAD SECTION MUST BE PRESENT AND RAILROAD FLAGGING WILL BE REQUIRED. THIS CAN BE SCHEDULED THROUGH THE RAILROAD PROJECT MANAGER AT (302) 760-2183.
18. THE RAILROAD AND/OR ITS CONTRACTOR WILL PAVE BETWEEN THE RAILS (GAUGE) AND PERFORM SIGNAL AND COMMUNICATIONS WORK WITHIN THEIR RIGHT OF WAY. THE CLOSURE FOR PAVING BETWEEN THE RAILS WILL BE INSTALLED BY DELDOT'S RAILROAD CONTRACTOR AFTER THE CONCLUSION OF THIS CONTRACT.
19. MISS UTILITY SERVICES DO NOT LOCATE BURIED RAILROAD SIGNAL AND COMMUNICATIONS LINES. THE CONTRACTOR SHALL CONTACT THE RAILROAD'S REPRESENTATIVE TWO (2) DAYS IN ADVANCE OF ANY OPERATION WHERE EXCAVATION, PILE DRIVING, OR HEAVY LOADS MAY DAMAGE UNDERGROUND LINES ON RAILROAD PROPERTY. UPON REQUEST FROM THE CONTRACTOR OR AGENCY, RAILROAD SIGNAL FORCES WILL LOCATE AND PAINT MARK OR FLAG RAILROAD UNDERGROUND SIGNAL, COMMUNICATION, AND POWER LINES IN THE AREA TO BE DISTURBED. THE CONTRACTOR SHALL AVOID EXCAVATION OR OTHER DISTURBANCE OF THESE LINES WHICH ARE CRITICAL TO THE SAFETY OF THE RAILROAD AND THE PUBLIC. IF DISTURBANCE OR EXCAVATION IS REQUIRED NEAR A BURIED RAILROAD SIGNAL, COMMUNICATION, OR POWER LINE, THE LINE SHALL BE POTHOLED MANUALLY WITH CAREFUL HAND EXCAVATION BY THE CONTRACTOR AND PROTECTED BY THE CONTRACTOR DURING THE COURSE OF THE DISTURBANCE UNDER THE SUPERVISION AND DIRECTION OF A RAILROAD SIGNAL REPRESENTATIVE.
20. FOR PROJECTS REQUIRING MORE THAN 30 CONSECUTIVE DAYS OF RAILROAD FLAGGING, THE CONTRACTOR SHALL PROVIDE THE FLAGMAN A SMALL WORK AREA WITH A DESK/COUNTER AND CHAIR WITHIN THE FIELD/SITE TRAILER, INCLUDING THE USE OF BATHROOM FACILITIES, WHERE THE FLAGMAN CAN CHECK IN/OUT WITH THE PROJECT, AS WELL AS TO THE FLAGMAN'S HOME TERMINAL. THE WORK AREA SHOULD PROVIDE ACCESS TO TWO (2) ELECTRICAL OUTLETS FOR RECHARGING RADIO(S), AND A LAPTOP COMPUTER; AND HAVE THE ABILITY TO PRINT OFF NEEDED DOCUMENTATION AND ORDERS AS NEEDED AT THE FIELD/SITE TRAILER. THIS SHOULD AID IN MAXIMIZING THE FLAGMAN'S TIME AND EFFICIENCY ON THE PROJECT.

SECTION 900

21. THIS PROJECT IS COVERED UNDER AN NPDES GENERAL PERMIT FOR CONSTRUCTION. UNDER THE GENERAL PERMIT, COMPLIANCE WITH DELDOT'S APPROVED SEDIMENT AND STORMWATER MANAGEMENT PLANS WILL CONSTITUTE COMPLIANCE WITH THE NPDES INDUSTRIAL PERMITTING REQUIREMENTS FOR THIS CONSTRUCTION PROJECT. A COPY OF THE NPDES GENERAL PERMIT AND NOI IS KEPT ON FILE IN EACH OF THE CONSTRUCTION OFFICES AND THE DEPARTMENT'S STORMWATER SECTION. A COPY OF THE GENERAL PERMIT OR THE NOI CAN BE OBTAINED UPON REQUEST FROM EITHER THE DEPARTMENT'S STORMWATER ENGINEER OR THE APPROPRIATE CONSTRUCTION ENGINEER.

MISCELLANEOUS

22. THE CONTRACTOR SHALL CONTACT JAMILA JONES, THE CHIEF OF SCHEDULING FOR DART FIRST STATE, 14 DAYS PRIOR TO THE START OF CONSTRUCTION AT 302-576-6019.
23. THE CONTRACTOR MUST BE AWARE THAT THE PROJECT TAKES PLACE WITHIN THE COOCH'S BRIDGE HISTORIC DISTRICT. DAYETT MILLS, A NATIONAL REGISTER PROPERTY, IS WITHIN THE DISTRICT AND VERY NEAR THE LIMIT OF CONSTRUCTION. AS SUCH, NO FURTHER WORK SHOULD BE UNDERTAKEN OTHER THAN THE NECESSARY PLANS AND SCOPE, AS INDICATED. ANY PLAN MODIFICATION SIGNIFICANT IN NATURE OR NEW INFORMATION NOT INITIALLY KNOWN THAT ALTERS THE PLANS AND DESIGNS WILL BE COORDINATED WITH THE DISTRICT ENGINEER AND THE ENVIRONMENTAL STUDIES OFFICE (NATHANIEL DELESLINE, 302-760-2278, NATHANIEL.DELESLINE@STATE.DE.US) IF NECESSARY, THEY WILL COORDINATE WITH THE DELAWARE HISTORIC PRESERVATION OFFICE (SHPO) AND FEDERAL HIGHWAY ADMINISTRATION (FHWA) OR ANY OTHER INTERESTED PARTY BEFORE ANY ACTION IS IMPLEMENTED. PROPOSED OR NECESSARY MODIFICATIONS AND CHANGES WILL BE REVIEWED TO ENSURE PROPER TREATMENT AND CONSIDERATIONS TO HISTORIC PROPERTIES. PER SHPO, FHWA AND THE PROPERTY OWNER(S), AND THE CONTRACTOR'S DIALOGUE.
24. BEFORE CONSTRUCTION ACTIVITIES, THE CONTRACTOR WILL RECORD AND MONITOR SITE CONDITIONS BY USE OF VIDEO OR OTHER DATA RECORDATION. AS PART OF THAT EFFORT, THE CONTRACTOR MAY ALSO UNDERTAKE FIELD MEASUREMENTS AND RECORD EXISTING CONDITIONS FOR DESIGN DETAILS AND ANY CONTRASTING SPECIFICATIONS. COPIES WILL BE PROVIDED TO THE APPROPRIATE PERSONNEL DURING THIS PRE-CONSTRUCTION MEETING AND PRIOR TO CONSTRUCTION. THE DEPARTMENT SHALL REVIEW AND APPROVE VIDEO PRIOR TO THE BEGINNING OF CONSTRUCTION ACTIVITIES. THE EFFORTS WILL BE USED FOR ANY DISPUTE RESOLUTIONS, SHOULD THEY ARISE.
25. WITHIN THE PROJECT LIMITS, THE FOLLOWING PROPERTIES ARE CONSIDERED CONTRIBUTING TO THE HISTORIC DISTRICT: 11-014.00-044, 11+010.00-009, 11-010.00-157 AND 11-014.00+045. IF UNFORSEEN DAMAGE OCCURS TO ANY PROPERTIES WITHIN THE PROPOSED HISTORIC DISTRICT (HD), AS A DIRECT RESULT OF THE UNDERTAKING, DELDOT'S ENVIRONMENTAL STUDIES PERSONNEL WILL BE CONTACTED PRIOR TO IMPLEMENTATION TO ENSURE THAT PROPOSED REPAIRS WILL MEET THE SECRETARY OF THE INTERIOR STANDARDS FOR REHABILITATION AND/OR TREATMENT OF HISTORIC PROPERTIES. ANY REPAIR MEASURES WILL AVOID AND MINIMIZE POTENTIAL ADVERSE EFFECTS AS DETERMINED BY DELDOT, SHPO AND FHWA. ALL STOCKPILING OR PARKING OF CONSTRUCTION VEHICLES SHALL TAKE PLACE OUTSIDE OF THE HISTORIC DISTRICT.
26. DUE TO THE SENSITIVE NATURE OF THE COOCH-DAYETT MILL PROPERTY AS PART OF THE NATIONAL REGISTER COOCH'S BRIDGE HISTORIC DISTRICT (CRS# N00190), CONTRACTOR'S ACCESS BEYOND THE LOC/TCE ON PARCELS 11-010.00-157 AND 11-014.00-045 IS STRICTLY PROHIBITED. STAGING AND/OR STOCKPILING ARE NOT PERMITTED ON THESE PARCELS. STAGING AND STOCKPILE LOCATIONS OUTSIDE OF THE LOC ARE TO BE IN NON-ARCHAEOLOGICALLY-SENSITIVE AREAS AND MUST BE REVIEWED AND APPROVED BY HEIDI.KROFFTAT (302)760-2125 OR HEIDI.KROFFT@STATE.DE.US.
27. AS DESIGNED, THERE ARE NO ENVIRONMENTAL PERMITS ASSOCIATED WITH THIS PROJECT SO AN ENVIRONMENTAL COMPLIANCE SHEET WAS NOT PREPARED. THERE ARE WETLANDS LOCATED BEYOND THE LIMITS OF THE PROJECT. IN ORDER TO PROTECT THESE WETLANDS, CONSTRUCTION SAFETY FENCING SHALL BE PLACED ALONG THE LOC FROM STATIONS 6+00LT TO 7+00LT AND 113+00LT TO 114+50LT. CONTRACTORS ACCESS INTO THESE WETLANDS IS STRICTLY PROHIBITED WITHOUT PROPER ENVIRONMENTAL PERMITS. CONTRACTOR'S ACCESS BEYOND THE LOC IS STRICTLY PROHIBITED.

EARTHWORK SUMMARY

EXCAVATION	
EXCAVATION FROM CROSS SECTIONS	3,559 C. Y.
ROCK EXCAVATION FOR ROADWAY AND TRENCHES	0 C. Y.
TOPSOIL STRIPPING	0 C. Y.
TOTAL EXCAVATION	3,559 C. Y.
EXCAVATION AVAILABLE FOR EMBANKMENT	
EXCAVATION MEETING BORROW TYPE 'A'	0 C. Y.
EXCAVATION MEETING BORROW TYPE 'F'	0 C. Y.
EXCAVATION MEETING TOPSOIL	0 C. Y.
EMBANKMENT REQUIREMENTS	
BORROW TYPE 'A' REQUIRED (INCLUDING UNDERCUT)	0 C. Y.
BORROW TYPE 'F' REQUIRED	61 C. Y.
TOPSOIL REQUIRED	523 C. Y.
MATERIAL BALANCE ("+"= EXCESS, "-"= NEED)	
BORROW TYPE 'A'	0 C. Y.
BORROW TYPE 'F'	-61 C. Y.
TOPSOIL	-523 C. Y.
UNSUITABLE MATERIAL	+3,559 C. Y.

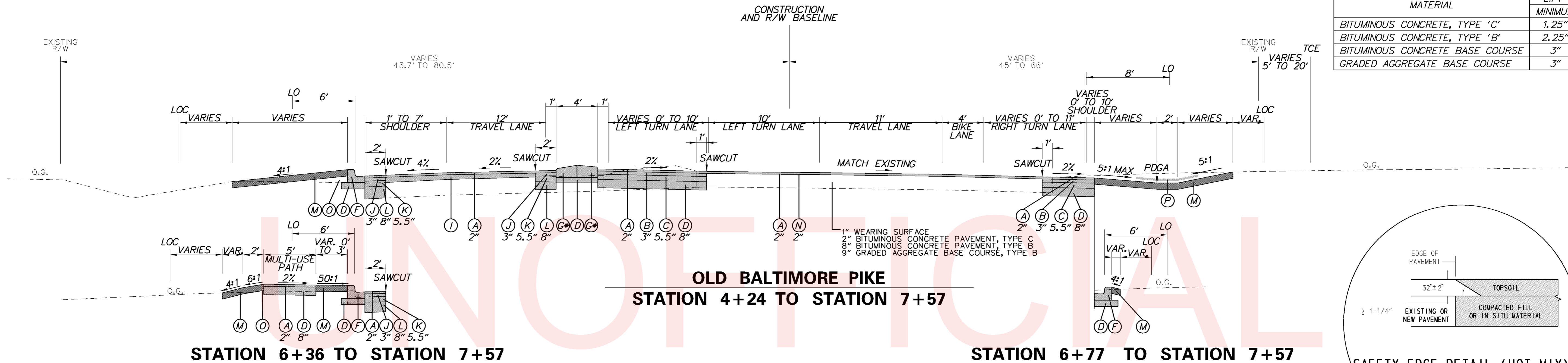
- NOTES:
- THE VALUES LISTED IN THE EARTHWORK SUMMARY ARE APPROXIMATE AND ARE NOT TO BE USED AS A BASIS OF PAYMENT. THE EARTHWORK SUMMARY IS CONSIDERED FOR INFORMATIONAL PURPOSES ONLY.
 - OTHER SOURCES OF EXCAVATION MAY INCLUDE PIPE TRENCH EXCAVATION, STRUCTURE EXCAVATION, UNDERCUT EXCAVATION, STORMWATER MANAGEMENT POND EXCAVATION, ENVIRONMENTAL SITE EXCAVATION, MAINTENANCE OF TRAFFIC EXCAVATION, ETC.
 - UNSUITABLE MATERIALS INCLUDE UNDERCUT SOILS, BITUMINOUS PAVEMENT, ETC.

UNOFFICIAL WEBSITE COPY

LAST REVISED: 08/11/2016 Y:\NEWCASTLE\356\ROAD\T201200108\PLANS\PLNDGN

 DELAWARE DEPARTMENT OF TRANSPORTATION	ADDENDUMS / REVISIONS		NOT TO SCALE	HEP NCC, SR72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS	CONTRACT	BRIDGE NO.	N/A	NOTES	SHEET NO.
					T201200108	DESIGNED BY: BCD	5		
					COUNTY	CHECKED BY: MCN	TOTAL SHTS.		
					NEW CASTLE		52		

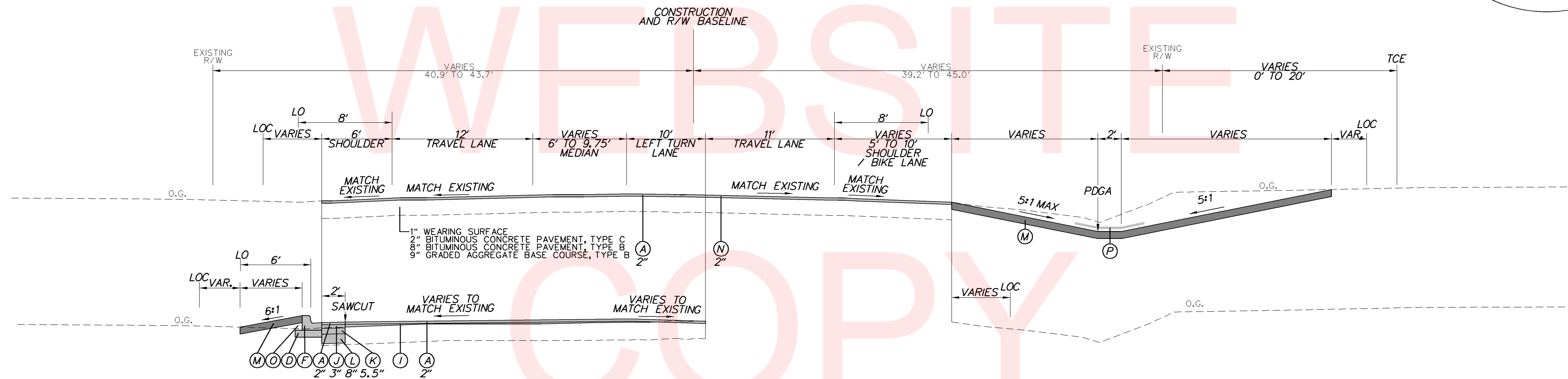
MATERIAL	LIFT THICKNESS	
	MINIMUM	MAXIMUM
BITUMINOUS CONCRETE, TYPE 'C'	1.25"	2"
BITUMINOUS CONCRETE, TYPE 'B'	2.25"	4"
BITUMINOUS CONCRETE BASE COURSE	3"	6"
GRADED AGGREGATE BASE COURSE	3"	8"



STATION 6+36 TO STATION 7+57

OLD BALTIMORE PIKE
STATION 4+24 TO STATION 7+57

STATION 6+77 TO STATION 7+57



STATION 3+69 TO STATION 4+24

OLD BALTIMORE PIKE
STATION 2+15 TO STATION 4+24

STATION 1+25 TO STATION 3+50

*FULL DEPTH TYPE 2 CURB AND MEDIAN CONCRETE TO BE CONSTRUCTED AND PAID PER CONSTRUCTION DETAIL ON SHEET 23.

LEGEND

- | | | |
|--|--|---|
| (A) ITEM 401833 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 76-22, (NON-CARBONATE STONE) | (G) ITEM 701011 - PORTLAND CEMENT CONCRETE CURB, TYPE 2 | (M) ITEM 908004 - TOPSOIL, 6" DEPTH |
| (B) ITEM 401816 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 76-22 | (H) ITEM 705002 - P.C.C. SIDEWALK, 6" | ITEM 908014 - PERMANENT GRASS SEEDING, DRY GROUND |
| (C) ITEM 401819 - BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22 | (I) ITEM 401824 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22, WEDGE | (N) ITEM 760000 - PAVEMENT - MILLING, HOT-MIX |
| (D) ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B | (J) ITEM 401822 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 64-22, PATCHING | (O) ITEM 209006 - BORROW, TYPE F |
| (E) ITEM 705001 - P.C.C. SIDEWALK, 4" | (K) ITEM 401823 - BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING | (P) ITEM 908020 - EROSION CONTROL BLANKET MULCH |
| (F) ITEM 701022 - INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 3-8 | (L) ITEM 302008 - GRADED AGGREGATE BASE COURSE, TYPE B, PATCHING | |

ADDENDUMS / REVISIONS

NOT TO SCALE

HEP NCC,
SR 72 AND OLD BALTIMORE PIKE
INTERSECTION IMPROVEMENTS

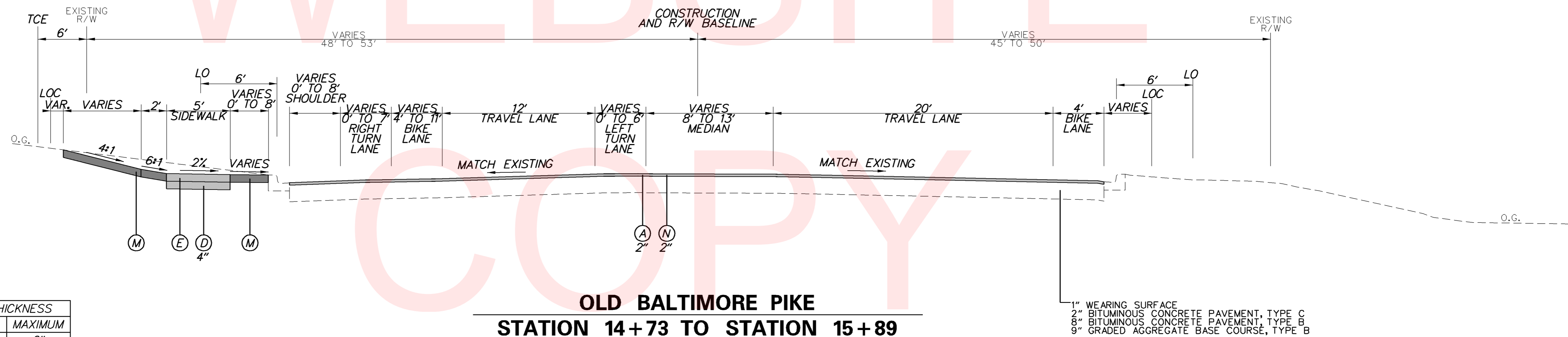
CONTRACT T201200108	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: BCD CHECKED BY: MCN

TYPICAL SECTIONS

SHEET NO. 6
TOTAL SHTS. 52

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**OLD BALTIMORE PIKE
STATION 14+73 TO STATION 15+89**

MATERIAL	LIFT THICKNESS	
	MINIMUM	MAXIMUM
BITUMINOUS CONCRETE, TYPE 'C'	1.25"	2"
BITUMINOUS CONCRETE, TYPE 'B'	2.25"	4"
BITUMINOUS CONCRETE BASE COURSE	3"	6"
GRADED AGGREGATE BASE COURSE	3"	8"

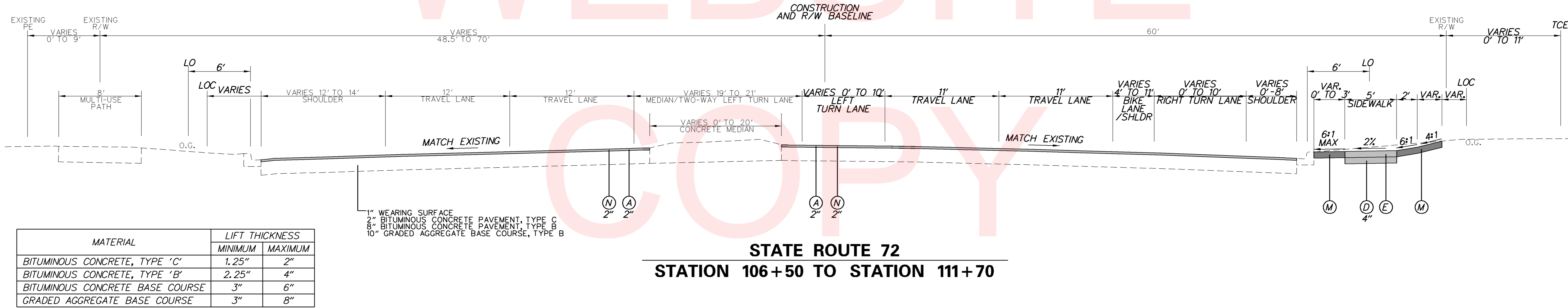
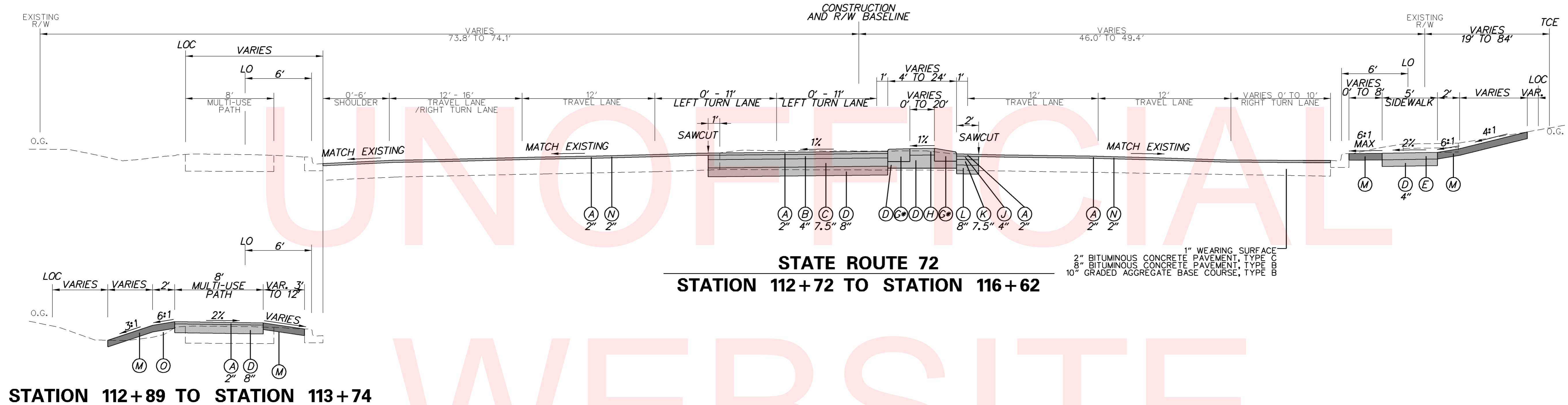
*FULL DEPTH TYPE 2 CURB AND MEDIAN CONCRETE TO BE CONSTRUCTED AND PAID PER CONSTRUCTION DETAIL ON SHEET 23.

LEGEND

- | | | |
|---|---|--|
| <ul style="list-style-type: none"> (A) ITEM 401833 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 76-22, (NON-CARBONATE STONE) (B) ITEM 401816 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 76-22 (C) ITEM 401819 - BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22 (D) ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B (E) ITEM 705001 - P.C.C. SIDEWALK, 4" (F) ITEM 701022 - INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 3-8 | <ul style="list-style-type: none"> (G) ITEM 701011 - PORTLAND CEMENT CONCRETE CURB, TYPE 2 (H) ITEM 705002 - P.C.C. SIDEWALK, 6" (I) ITEM 401824 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22, WEDGE (J) ITEM 401822 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 64-22, PATCHING (K) ITEM 401823 - BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING (L) ITEM 302008 - GRADED AGGREGATE BASE COURSE, TYPE B, PATCHING | <ul style="list-style-type: none"> (M) ITEM 908004 - TOPSOIL, 6" DEPTH ITEM 908014 - PERMANENT GRASS SEEDING, DRY GROUND (N) ITEM 760000 - PAVEMENT - MILLING, HOT-MIX (O) ITEM 209006 - BORROW, TYPE F (P) ITEM 908020 - EROSION CONTROL BLANKET MULCH |
|---|---|--|

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<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>	ADDENDUMS / REVISIONS	<p>NOT TO SCALE</p>	<p>HEP NCC, SR 72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS</p>	CONTRACT	BRIDGE NO.	<p>TYPICAL SECTIONS</p>	SHEET NO.	
					T201200108		N/A	8
					COUNTY		DESIGNED BY: BCD	TOTAL SHTS.
					NEW CASTLE		CHECKED BY: MCN	52

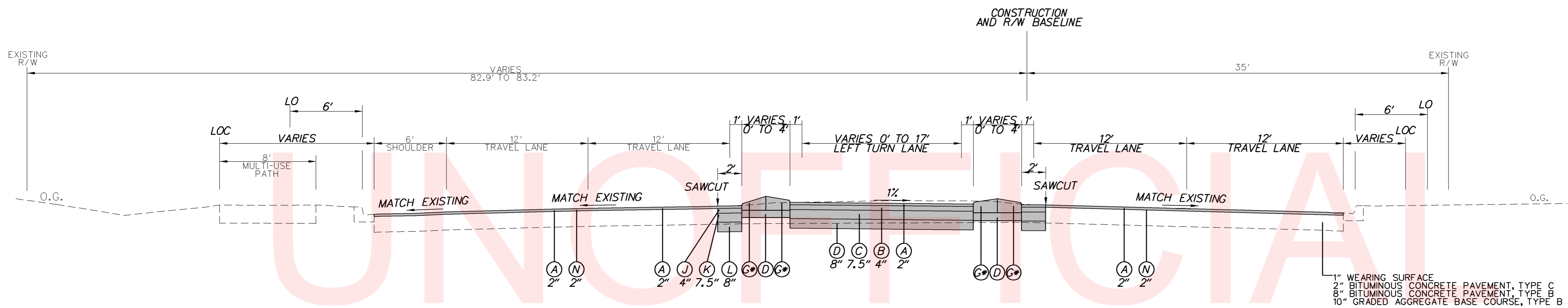


*FULL DEPTH TYPE 2 CURB AND MEDIAN CONCRETE TO BE CONSTRUCTED AND PAID PER CONSTRUCTION DETAIL ON SHEET 23.

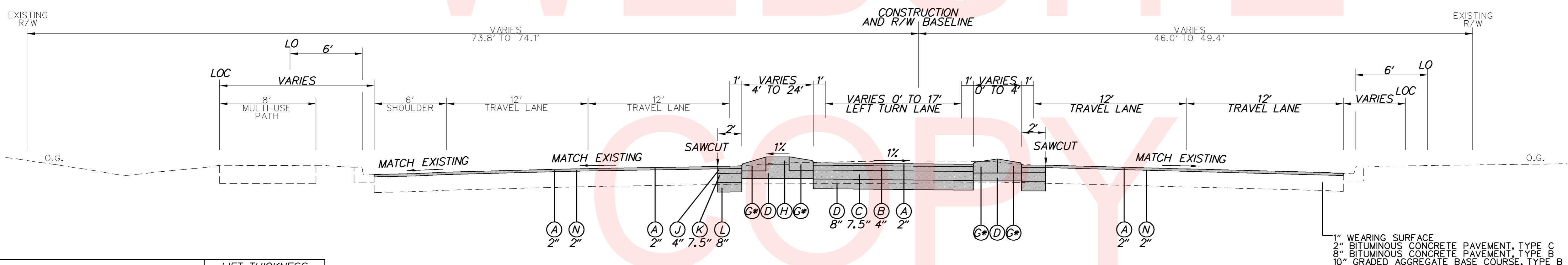
LEGEND

- | | | |
|---|---|--|
| <ul style="list-style-type: none"> (A) ITEM 401833 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 76-22, (NON-CARBONATE STONE) (B) ITEM 401816 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 76-22 (C) ITEM 401819 - BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22 (D) ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B (E) ITEM 705001 - P.C.C. SIDEWALK, 4" (F) ITEM 701022 - INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 3-8 | <ul style="list-style-type: none"> (G) ITEM 701011 - PORTLAND CEMENT CONCRETE CURB, TYPE 2 (H) ITEM 705002 - P.C.C. SIDEWALK, 6" (I) ITEM 401824 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22, WEDGE (J) ITEM 401822 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 64-22, PATCHING (K) ITEM 401823 - BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING (L) ITEM 302008 - GRADED AGGREGATE BASE COURSE, TYPE B, PATCHING | <ul style="list-style-type: none"> (M) ITEM 908004 - TOPSOIL, 6" DEPTH ITEM 908014 - PERMANENT GRASS SEEDING, DRY GROUND (N) ITEM 760000 - PAVEMENT - MILLING, HOT-MIX (O) ITEM 209006 - BORROW, TYPE F (P) ITEM 908020 - EROSION CONTROL BLANKET MULCH |
|---|---|--|

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STATE ROUTE 72
STATION 117+31.38 TO STATION 117+62



STATE ROUTE 72
STATION 116+62 TO STATION 117+22.38

MATERIAL	LIFT THICKNESS	
	MINIMUM	MAXIMUM
BITUMINOUS CONCRETE, TYPE 'C'	1.25"	2"
BITUMINOUS CONCRETE, TYPE 'B'	2.25"	4"
BITUMINOUS CONCRETE BASE COURSE	3"	6"
GRADED AGGREGATE BASE COURSE	3"	8"

*FULL DEPTH TYPE 2 CURB AND MEDIAN CONCRETE TO BE CONSTRUCTED AND PAID PER CONSTRUCTION DETAIL ON SHEET 23.

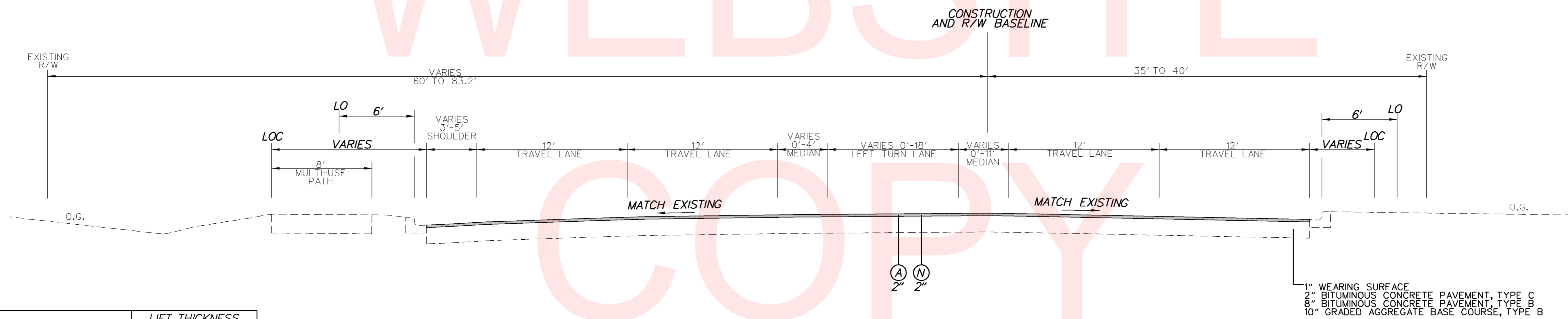
LEGEND

- | | | |
|--|--|---|
| (A) ITEM 401833 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 76-22, (NON-CARBONATE STONE) | (G) ITEM 701011 - PORTLAND CEMENT CONCRETE CURB, TYPE 2 | (M) ITEM 908004 - TOPSOIL, 6" DEPTH |
| (B) ITEM 401816 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 76-22 | (H) ITEM 705002 - P.C.C. SIDEWALK, 6" | ITEM 908014 - PERMANENT GRASS SEEDING, DRY GROUND |
| (C) ITEM 401819 - BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22 | (I) ITEM 401824 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22, WEDGE | (N) ITEM 760000 - PAVEMENT - MILLING, HOT-MIX |
| (D) ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B | (J) ITEM 401822 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 64-22, PATCHING | (O) ITEM 209006 - BORROW, TYPE F |
| (E) ITEM 705001 - P.C.C. SIDEWALK, 4" | (K) ITEM 401823 - BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING | (P) ITEM 908020 - EROSION CONTROL BLANKET MULCH |
| (F) ITEM 701022 - INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 3-8 | (L) ITEM 302008 - GRADED AGGREGATE BASE COURSE, TYPE B, PATCHING | |

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UNOFFICIAL

WEBSITE



MATERIAL	LIFT THICKNESS	
	MINIMUM	MAXIMUM
BITUMINOUS CONCRETE, TYPE 'C'	1.25"	2"
BITUMINOUS CONCRETE, TYPE 'B'	2.25"	4"
BITUMINOUS CONCRETE BASE COURSE	3"	6"
GRADED AGGREGATE BASE COURSE	3"	8"

**STATE ROUTE 72
STATION 117+62 TO STATION 121+21**

1" WEARING SURFACE
2" BITUMINOUS CONCRETE PAVEMENT, TYPE C
8" BITUMINOUS CONCRETE PAVEMENT, TYPE B
10" GRADED AGGREGATE BASE COURSE, TYPE B

*FULL DEPTH TYPE 2 CURB AND MEDIAN CONCRETE TO BE CONSTRUCTED AND PAID PER CONSTRUCTION DETAIL ON SHEET 23.

LEGEND

- | | | |
|--|--|---|
| (A) ITEM 401833 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 76-22, (NON-CARBONATE STONE) | (G) ITEM 701011 - PORTLAND CEMENT CONCRETE CURB, TYPE 2 | (M) ITEM 908004 - TOPSOIL, 6" DEPTH |
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| (F) ITEM 701022 - INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 3-8 | (L) ITEM 302008 - GRADED AGGREGATE BASE COURSE, TYPE B, PATCHING | |

ADDENDUMS / REVISIONS

NOT TO SCALE

**HEP NCC,
SR 72 AND OLD BALTIMORE PIKE
INTERSECTION IMPROVEMENTS**

CONTRACT T201200108	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: BCD CHECKED BY: MCN

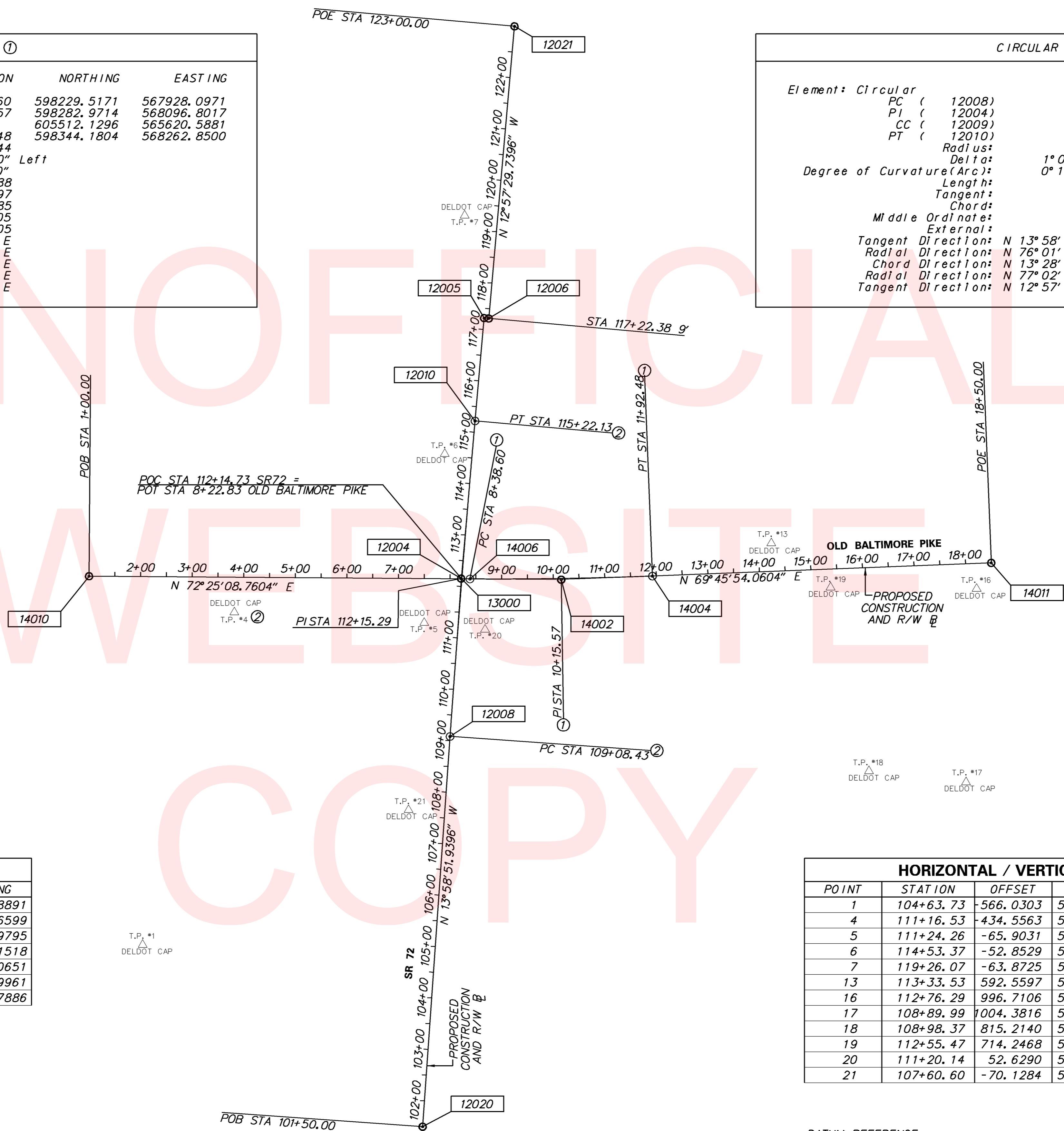
TYPICAL SECTIONS

SHEET NO. 11
TOTAL SHTS. 52

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CIRCULAR CURVE NO. ①				
Element: Circular	STATION	NORTHING	EASTING	
PC (14006)	8+38.60	598229.5171	567928.0971	
PI (14002)	10+15.57	598282.9714	568096.8017	
CC (14005)		605512.1296	565620.5881	
PT (14004)	11+92.48	598344.1804	568262.8500	
Radius:	7639.44			
Delta:	2° 39' 14.7000" Left			
Degree of Curvature (Arc):	0° 44' 59.9990"			
Length:	353.88			
Tangent:	176.97			
Chord:	353.85			
Middle Ordinate:	2.05			
External:	2.05			
Tangent Direction:	N 72° 25' 08.7604" E			
Radial Direction:	S 17° 34' 51.2396" E			
Chord Direction:	N 71° 05' 31.4104" E			
Radial Direction:	S 20° 14' 05.9396" E			
Tangent Direction:	N 69° 45' 54.0604" E			

CIRCULAR CURVE NO. ②				
Element: Circular	STATION	NORTHING	EASTING	
PC (12008)	109+08.43	597927.2070	567985.7415	
PI (12004)	112+15.29	598224.9746	567911.6040	
CC (12009)		606232.8628	601344.7961	
PT (12010)	115+22.13	598524.0183	567842.7938	
Radius:	34377.47			
Delta:	1° 01' 22.2000" Right			
Degree of Curvature (Arc):	0° 10' 00.0000"			
Length:	613.70			
Tangent:	306.86			
Chord:	613.69			
Middle Ordinate:	1.37			
External:	1.37			
Tangent Direction:	N 13° 58' 51.9396" W			
Radial Direction:	N 76° 01' 08.0604" E			
Chord Direction:	N 13° 28' 10.8396" E			
Radial Direction:	N 77° 02' 30.2604" E			
Tangent Direction:	N 12° 57' 29.7396" W			



CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
12005	117+22.38	0.0000	598719.1704	567797.8891
12006	117+31.38	0.0000	598721.1886	567806.6599
12020	101+50.00	0.0000	597191.2451	568168.9795
12021	123+00.00	0.0000	599275.3279	567679.1518
13000	112+14.73	0.0000	598224.7541	567913.0651
14010	1+00.00	0.0000	598006.4214	567223.9961
14011	18+50.00	0.0000	598571.5974	568879.7886

HORIZONTAL / VERTICAL CONTROL DATA					
POINT	STATION	OFFSET	NORTHING	EASTING	ELEVATION
1	104+63.73	-566.0303	597358.9234	567543.9207	50.8792
4	111+16.53	-434.5563	598026.8545	567513.7659	55.4000
5	111+24.26	-65.9031	598121.2857	567870.2047	57.5700
6	114+53.37	-52.8529	598445.0727	567806.7958	57.7600
7	119+26.07	-63.8725	598896.5991	567700.7561	61.0600
13	113+33.53	592.5597	598476.3813	568462.3211	61.3800
16	112+76.29	996.7106	598515.0847	568868.4817	65.6800
17	108+89.99	1004.3816	598151.9736	568964.8239	60.8900
18	108+98.37	815.2140	598114.4041	568779.2352	60.8200
19	112+55.47	714.2468	598429.9385	568598.3922	61.2300
20	111+20.14	52.6290	598145.2000	567986.3726	57.8300
21	107+60.60	-70.1284	597766.8160	567953.4059	58.2000

DATUM REFERENCE:
 HORIZONTAL - THIS PROJECT IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).
 VERTICAL - THIS PROJECT IS REFERENCED TO NAVD 88.

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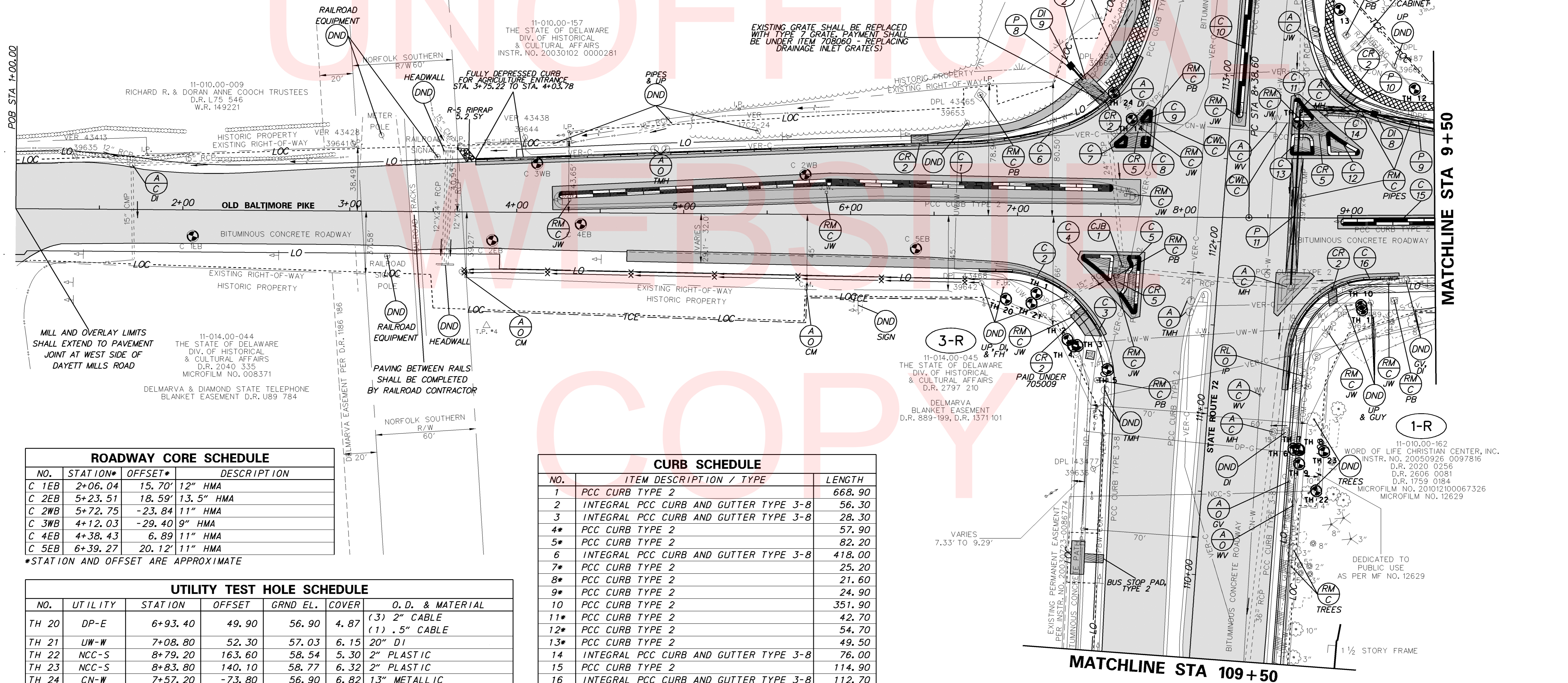
 DELAWARE DEPARTMENT OF TRANSPORTATION	ADDENDUMS / REVISIONS	SCALE FEET	HEP NCC, SR 72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS	CONTRACT T201200108 COUNTY NEW CASTLE	BRIDGE NO. N/A DESIGNED BY: BCD CHECKED BY: MCN	HORIZONTAL AND VERTICAL CONTROL	SHEET NO. 12 TOTAL SHTS. 52

UTILITY TEST HOLE SCHEDULE						
NO.	UTILITY	STATION	OFFSET	GRND EL.	COVER	O. D. & MATERIAL
TH 1	DP-G	7+11.70	46.40	57.06	3.79	6.75" PLASTIC
TH 2	UW-W	7+30.50	73.90	57.59	8.03	20" DI
TH 3	DP-G	7+36.40	77.40	57.73	3.53	6.75" PLASTIC
TH 4	DP-E	7+33.80	77.70	57.83	5.97	(3) 2" CABLE (1) .5" CABLE
TH 5	VER-C	7+54.60	91.80	57.57	8.48	(4) 4.5" PLASTIC
TH 6	DP-G	8+65.80	138.70	57.75	5.95	6.75" PLASTIC
TH 7	NCC-S	8+68.70	138.80	57.87	4.96	2.5" PLASTIC
TH 8	UW-W	8+69.20	139.00	57.86	5.09	12.75" PLASTIC
TH 9	COM-C	8+69.20	147.40	58.01	1.71	1" CATV CABLE
TH 10	COM-C	9+05.80	52.50	57.52	1.46	1" CATV CABLE 3" STEEL TC CONDUIT
TH 11	DP-G	9+06.30	55.10	57.57	3.84	6.75" PLASTIC
TH 12	DP-G	9+38.40	-77.10	59.93	7.16	7" PLASTIC
TH 13	DP-G	8+92.80	-125.00	59.36	3.54	7" PLASTIC
TH 14	VER-C	112+69.00	-58.90	57.11	2.22	RPC TELE DUCT (8) 4.5" PASTIC CONDUIT

DRAINAGE PIPE SCHEDULE						
NO.	SIZE / TYPE	CLASS	LENGTH	SLOPE	INT. EL.	DIS. EL.
8	18" RCP	III	11	.01	51.31	51.20
9	30" RCP	III	77	.012	51.08	50.13
10	30" RCP	III	42	.0026	50.08	49.96
11	36" RCP	III	105	.0191	49.30	47.31

DRAINAGE INLET SCHEDULE						
NO.	STATION	OFFSET	BOX SIZE	GRATE	T.G. EL.	INV. EL.
8	9+13.40	-61.12	48" x 48"	4	56.11	50.13
9	7+31.22	-90.29	34" x 24"	1	54.56	51.32

CONVERT TO JUNCTION BOX SCHEDULE				
NO.	STATION	OFFSET	T.C. EL.	INV. EL.
1	7+63.23	31.50	55.07	49.43
2	7+39.07	-80.73	54.33	51.01



ROADWAY CORE SCHEDULE			
NO.	STATION*	OFFSET*	DESCRIPTION
C 1EB	2+06.04	15.70'	12" HMA
C 2EB	5+23.51	18.59'	13.5" HMA
C 2WB	5+72.75	-23.84'	11" HMA
C 3WB	4+12.03	-29.40'	9" HMA
C 4EB	4+38.43	6.89'	11" HMA
C 5EB	6+39.27	20.12'	11" HMA

*STATION AND OFFSET ARE APPROXIMATE

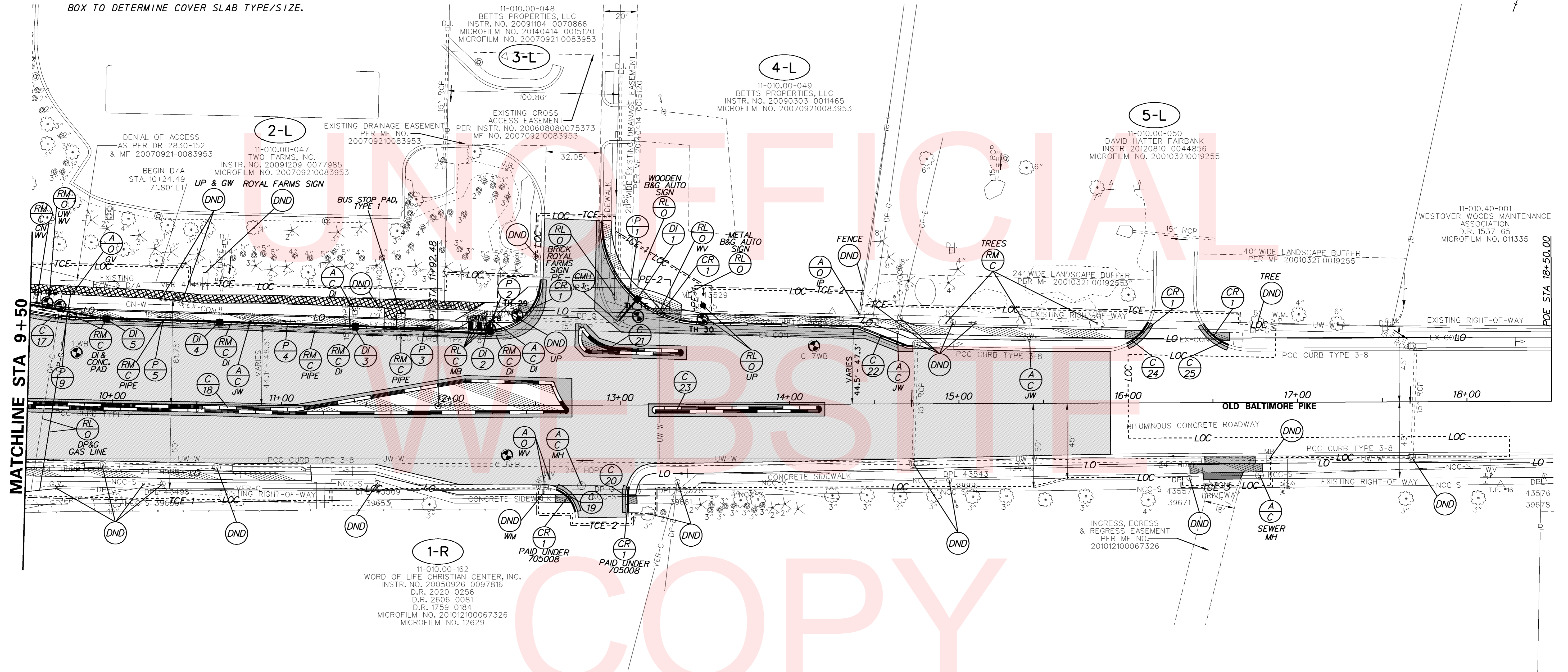
UTILITY TEST HOLE SCHEDULE						
NO.	UTILITY	STATION	OFFSET	GRND EL.	COVER	O. D. & MATERIAL
TH 20	DP-E	6+93.40	49.90	56.90	4.87	(3) 2" CABLE (1) .5" CABLE
TH 21	UW-W	7+08.80	52.30	57.03	6.15	20" DI
TH 22	NCC-S	8+79.20	163.60	58.54	5.30	2" PLASTIC
TH 23	NCC-S	8+83.80	140.10	58.77	6.32	2" PLASTIC
TH 24	CN-W	7+57.20	-73.80	56.90	6.82	13" METALLIC
TH 25	CN-W	8+67.90	-56.80	57.49	4.75	13" METALLIC

CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	PCC CURB TYPE 2	668.90
2	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	56.30
3	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	28.30
4*	PCC CURB TYPE 2	57.90
5*	PCC CURB TYPE 2	82.20
6	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	418.00
7*	PCC CURB TYPE 2	25.20
8*	PCC CURB TYPE 2	21.60
9*	PCC CURB TYPE 2	24.90
10	PCC CURB TYPE 2	351.90
11*	PCC CURB TYPE 2	42.70
12*	PCC CURB TYPE 2	54.70
13*	PCC CURB TYPE 2	49.50
14	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	76.00
15	PCC CURB TYPE 2	114.90
16	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	112.70

*THE COST FOR CONSTRUCTING THIS CURB IS INCIDENTAL TO ITEM 705530 - TRIANGULAR CHANNELIZING ISLANDS.

CONVERT TO MANHOLE SCHEDULE						
NO.	STATION	OFFSET	TYPE/SIZE	T.G. EL.	INV. EL.	
1	12+98.79	-49.15	**	58.45	53.40	

** CONTRACTOR SHALL FIELD VERIFY DIMENSIONS OF EXISTING INLET BOX TO DETERMINE COVER SLAB TYPE/SIZE.



CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
17	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	320.70
18	PCC CURB TYPE 2	652.20
19	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	19.40
20	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	15.60
21	PCC CURB TYPE 2	141.20
22	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	222.90
23	PCC CURB TYPE 2	204.60
24	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	19.30
25	INTEGRAL PCC CURB AND GUTTER TYPE 3-8	20.90

DRAINAGE INLET SCHEDULE						
NO.	STATION	OFFSET	BOX SIZE	GRATE	T.G. EL.	INV. EL.
1	13+09.55	-61.61	34" x 24"	4	58.23	55.67
2	12+23.16	-43.19	48" x 48"	4	57.67	52.94
3	11+43.36	-45.74	48" x 48"	4	56.69	52.27
4	10+62.81	-47.56	48" x 48"	4	56.01	51.64
5	9+95.46	-48.43	48" x 48"	4	55.72	51.05

DRAINAGE PIPE SCHEDULE						
NO.	SIZE / TYPE	CLASS	LENGTH	SLOPE	INT. EL.	DIS. EL.
1	15" RCP	111	15	.01	55.67	55.52
2	15" RCP	111	6	.007	52.94	52.89
3	24" RCP	111	76	.0081	52.89	52.28
4	24" RCP	111	77	.0081	52.27	51.65
5	24" RCP	111	63	.0081	51.64	51.13

ROADWAY CORE SCHEDULE			
NO.	STATION*	OFFSET*	DESCRIPTION
C 1WB	9+78.29	-29.27'	12" HMA
C 6EB	12+33.05	29.36'	11.5" HMA
C 7WB	14+14.66	-34.46'	6" HMA

*STATION AND OFFSET ARE APPROXIMATE

UTILITY TEST HOLE SCHEDULE						
NO.	UTILITY	STATION	OFFSET	GRND EL.	COVER	O.D. & MATERIAL
TH 16	DP-G	13+10.80	-52.30	59.09	3.01	6.75" PLASTIC
TH 26	CN-W	9+60.20	-58.50	57.75	4.72	13" METALLIC
TH 27	DP-G	9+67.90	-56.80	57.40	6.47	12.75" METALLIC CASING
TH 28	EX-CON	12+23.30	-45.50	58.28	2.98	(2) 3" STEEL CONDUITS
TH 29	DP-G	12+39.60	-53.40	58.54	3.44	6.75" PLASTIC
TH 30	DP-G	13+48.90	-50.50	60.43	4.87	6.75" PLASTIC

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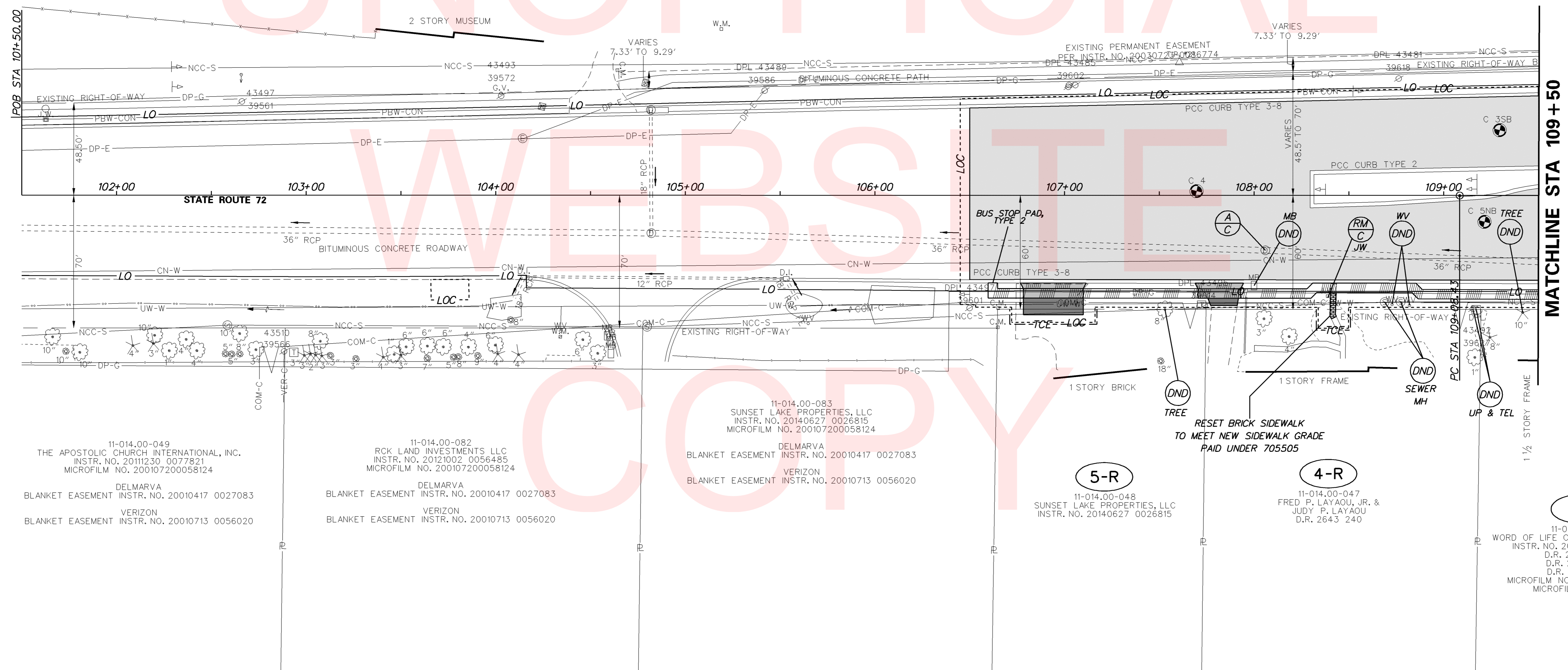
ROADWAY CORE SCHEDULE			
NO.	STATION*	OFFSET*	DESCRIPTION
C 3SB	109+29.52	-34.43'	10.5" HMA
C 4	107+69.63	-2.24'	12.5" HMA
C 5NB	109+21.54	13.87'	10.5" HMA

*STATION AND OFFSET ARE APPROXIMATE

3-R

11-014.00-045
THE STATE OF DELAWARE
DIV. OF HISTORICAL
& CULTURAL AFFAIRS
D.R. 2797 210

DELMARVA
BLANKET EASEMENT D.R. 889-199, D.R. 1371 101



11-014.00-049
THE APOSTOLIC CHURCH INTERNATIONAL, INC.
INSTR. NO. 20111230 0077821
MICROFILM NO. 200107200058124

DELMARVA
BLANKET EASEMENT INSTR. NO. 20010417 0027083

VERIZON
BLANKET EASEMENT INSTR. NO. 20010713 0056020

11-014.00-082
RCK LAND INVESTMENTS LLC
INSTR. NO. 20121002 0056485
MICROFILM NO. 200107200058124

DELMARVA
BLANKET EASEMENT INSTR. NO. 20010417 0027083

VERIZON
BLANKET EASEMENT INSTR. NO. 20010713 0056020

11-014.00-083
SUNSET LAKE PROPERTIES, LLC
INSTR. NO. 20140627 0026815
MICROFILM NO. 200107200058124

DELMARVA
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VERIZON
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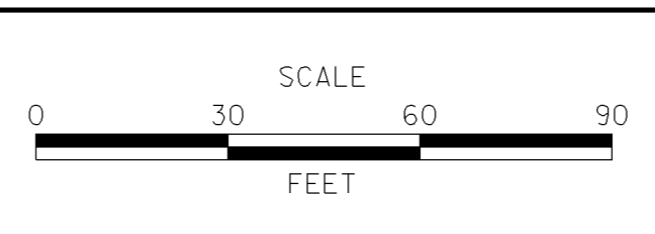
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SUNSET LAKE PROPERTIES, LLC
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4-R
11-014.00-047
FRED P. LAYAOU, JR. &
JUDY P. LAYAOU
D.R. 2643 240

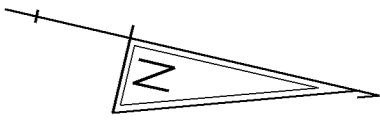
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11-010.00-162
WORD OF LIFE CHRISTIAN CENTER, INC.
INSTR. NO. 20050926 0097816
D.R. 2020 0256
D.R. 2606 0081
D.R. 1759 0184
MICROFILM NO. 201012100067326
MICROFILM NO. 12629

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ADDENDUMS / REVISIONS



CONTRACT	BRIDGE NO.	N/A
T201200108	DESIGNED BY:	BCD
COUNTY	CHECKED BY:	MCN
NEW CASTLE		



CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
26	INTEGRAL CURB AND GUTTER TYPE 3-8	81.10
27	INTEGRAL CURB AND GUTTER TYPE 3-8	22.90
28	PCC CURB TYPE 2	619.60
29	PCC CURB TYPE 2	104.80

DRAINAGE PIPE SCHEDULE						
NO.	SIZE / TYPE	CLASS	LENGTH	SLOPE	INT. EL.	DIS. EL.
6	18" RCP	111	34	.01	55.50	55.17
7	15" RCP	111	6	.012	58.10	58.03
12	15" RCP	111	8	.01	57.83	57.75

DRAINAGE INLET SCHEDULE						
NO.	STATION	OFFSET	BOX SIZE	GRATE	T.G. EL.	INV. EL.
6	116+25.41	43.93	48" x 30"	4	58.60	55.50
7	116+40.06	88.43	34" x 24"	4	60.39	57.80

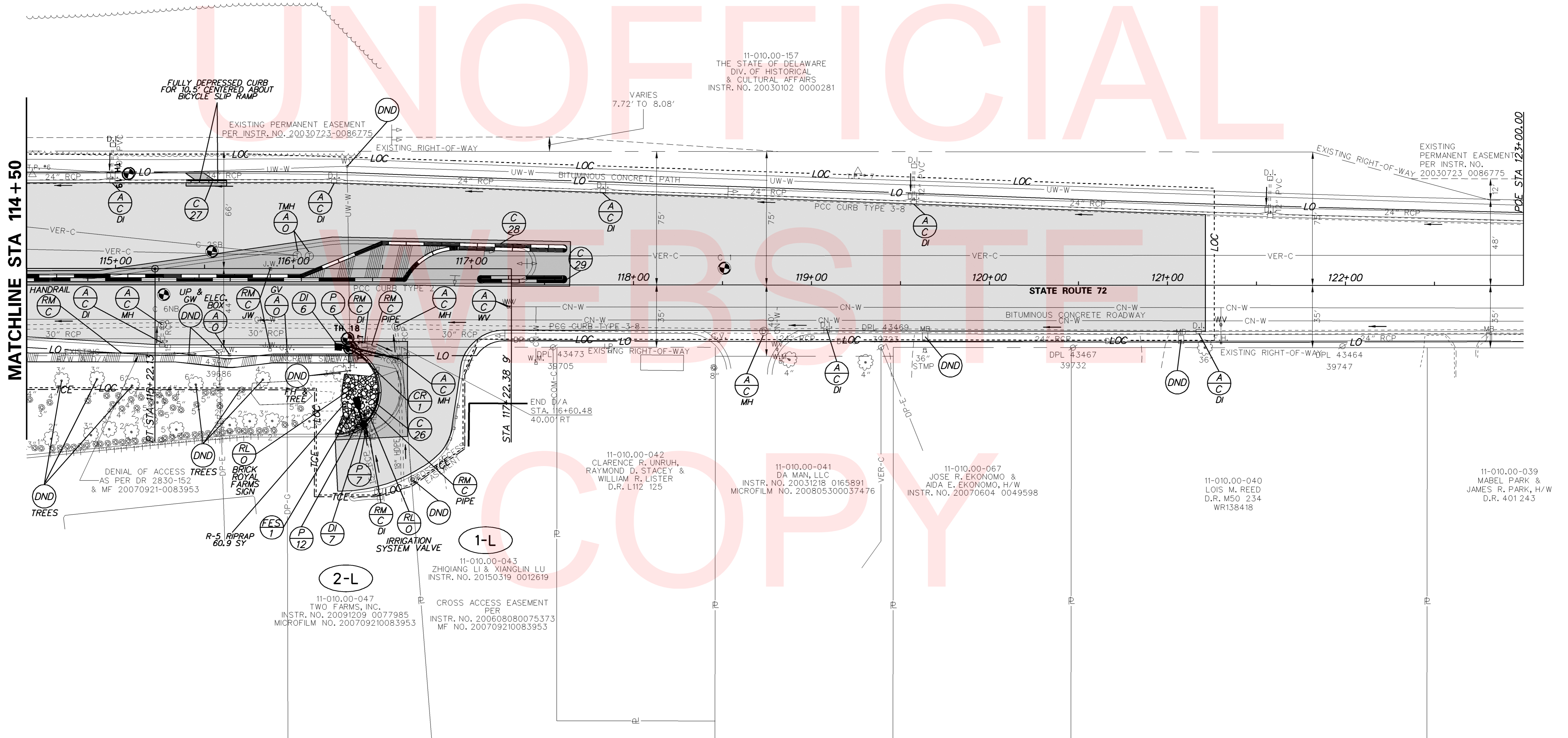
FLARED END SECTION SCHEDULE			
NO.	SIZE / TYPE	SLOPE	SAFETY GRATE
1	15" RCP	.01	NO

UTILITY TEST HOLE SCHEDULE						
NO.	UTILITY	STATION	OFFSET	GRND EL.	COVER	O. D. & MATERIAL
TH 17	UW-W	116+31.70	44.50	59.37	11.54	6" METAL*
TH 18	DP-G	116+30.40	39.20	58.73	4.37	6.75" PLASTIC
TH 19	UW-W	115+07.30	-53.30	58.30	5.28	12.75" PLASTIC

ROADWAY CORE SCHEDULE			
NO.	STATION*	OFFSET*	DESCRIPTION
C 1	118+51.06	-9.46'	9.5" HMA
C 2SB	115+54.18	-9.62'	11" HMA
C 6NB	115+27.09	14.61'	12" HMA

*STATION AND OFFSET ARE APPROXIMATE

* ACCURATE O. D. AND MATERIAL IN TH 17 COULD NOT BE DETERMINED DUE TO WATER IN BOTTOM OF THE TEST HOLE



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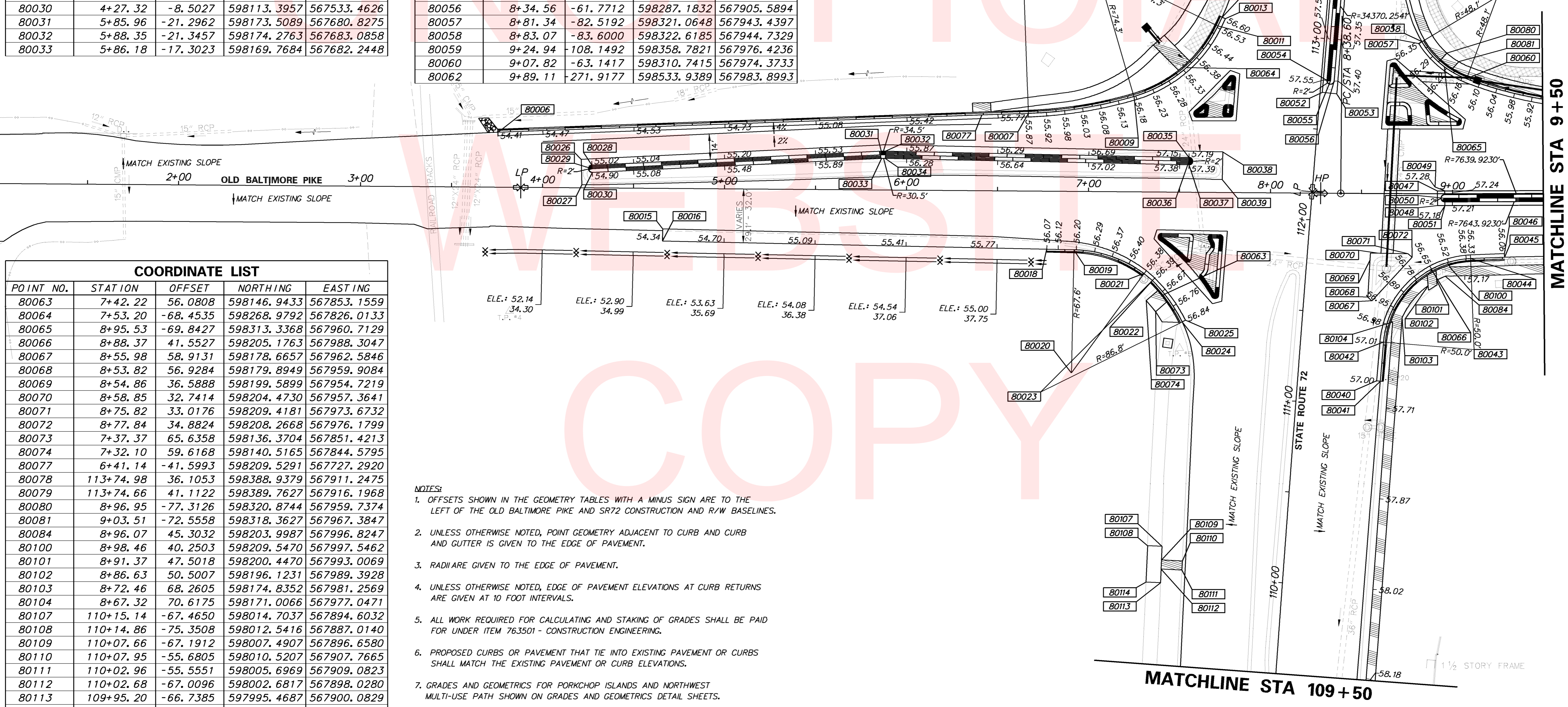
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
80006	3+75.22	-30.2342	598118.3743	567477.2290
80007	6+66.65	-42.6896	598218.2739	567751.2814
80008	6+56.44	281.5938	598442.9351	567669.3862
80009	7+24.61	-52.3959	598245.0351	567803.6065
80010	7+03.44	123.5872	598306.5049	567761.9163
80011	7+70.41	-91.4725	598296.1194	567835.4605
80012	7+72.74	-96.3397	598301.4642	567836.2152
80013	7+74.54	-95.4749	598301.1845	567838.1956
80015	4+66.21	22.2402	598095.8340	567579.8172
80016	4+66.10	28.9718	598089.3837	567581.7460
80018	6+77.27	31.8751	598150.3994	567783.9266
80019	6+92.26	32.0814	598154.7332	567798.2869
80020	6+91.34	99.6582	598090.0322	567817.8129
80021	7+30.85	44.8338	598154.2326	567838.9266
80022	7+35.44	48.5667	598152.0600	567844.4280
80023	6+74.49	110.3295	598074.7725	567804.9820
80024	7+51.98	71.2837	598135.4007	567867.0591
80025	7+53.78	70.3921	598136.7929	567868.5014
80026	4+21.21	-15.6523	598118.3637	567525.4716
80027	4+21.62	-1.6220	598105.1139	567530.1048
80028	4+27.10	-12.4966	598117.1362	567532.0453
80029	4+27.21	-10.4997	598115.2660	567532.7540
80030	4+27.32	-8.5027	598113.3957	567533.4626
80031	5+85.96	-21.2962	598173.5089	567680.8275
80032	5+88.35	-21.3457	598174.2763	567683.0858
80033	5+86.18	-17.3023	598169.7684	567682.2448

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
80034	5+88.29	-17.3461	598170.4469	567684.2412
80035	7+55.13	-19.0419	598222.4564	567842.7715
80036	7+55.10	-17.0421	598220.5416	567843.3492
80037	7+55.07	-15.0423	598218.6269	567843.9269
80038	7+73.64	-22.2647	598231.1192	567859.4423
80039	7+73.49	-7.0599	598216.5796	567863.8928
80040	8+58.81	102.9400	598137.5984	567978.7100
80041	8+60.78	103.0971	598138.0563	567980.6568
80042	8+62.46	81.5401	598159.1062	567975.7060
80043	9+11.75	85.6453	598170.5537	568024.3779
80044	9+09.93	35.6793	598217.4683	568007.0860
80045	9+28.43	35.0145	598223.9018	568024.5405
80046	9+28.36	33.0157	598225.7784	568023.8489
80047	8+91.70	-1.5170	598247.1746	567978.1921
80048	8+91.79	6.4830	598239.5922	567980.7444
80049	8+95.74	0.4830	598246.5212	567982.6546
80050	8+95.74	2.4830	598244.6192	567983.2729
80051	8+95.74	4.4830	598242.7172	567983.8913
80052	8+28.08	-55.3816	598279.1329	567901.3359
80053	8+36.05	-54.7072	598280.8979	567909.1393
80054	8+30.57	-62.0651	598286.2585	567901.6978
80055	8+32.57	-61.9181	598286.7209	567903.6436
80056	8+34.56	-61.7712	598287.1832	567905.5894
80057	8+81.34	-82.5192	598321.0648	567943.4397
80058	8+83.07	-83.6000	598322.6185	567944.7329
80059	9+24.94	-108.1492	598358.7821	567976.4236
80060	9+07.82	-63.1417	598310.7415	567974.3733
80062	9+89.11	-271.9177	598533.9389	567983.8993

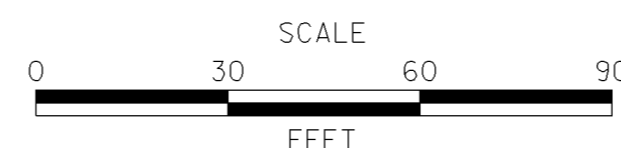
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
80063	7+42.22	56.0808	598146.9433	567853.1559
80064	7+53.20	-68.4535	598268.9792	567826.0133
80065	8+95.53	-69.8427	598313.3368	567960.7129
80066	8+88.37	41.5527	598205.1763	567988.3047
80067	8+55.98	58.9131	598178.6657	567962.5846
80068	8+53.82	56.9284	598179.8949	567959.9084
80069	8+54.86	36.5888	598199.5899	567954.7219
80070	8+58.85	32.7414	598204.4730	567957.3641
80071	8+75.82	33.0176	598209.4181	567973.6732
80072	8+77.84	34.8824	598208.2668	567976.1799
80073	7+37.37	65.6358	598136.3704	567851.4213
80074	7+32.10	59.6168	598140.5165	567844.5795
80077	6+41.14	-41.5993	598209.5291	567727.2920
80078	113+74.98	36.1053	598388.9379	567911.2475
80079	113+74.66	41.1122	598389.7627	567916.1968
80080	8+96.95	-77.3126	598320.8744	567959.7374
80081	9+03.51	-72.5558	598318.3627	567967.3847
80084	8+96.07	45.3032	598203.9987	567996.8247
80100	8+98.46	40.2503	598209.5470	567997.5462
80101	8+91.37	47.5018	598200.4470	567993.0069
80102	8+86.63	50.5007	598196.1231	567989.3928
80103	8+72.46	68.2605	598174.8352	567981.2569
80104	8+67.32	70.6175	598171.0066	567977.0471
80107	110+15.14	-67.4650	598014.7037	567894.6032
80108	110+14.86	-75.3508	598012.5416	567887.0140
80109	110+07.66	-67.1912	598007.4907	567896.6580
80110	110+07.95	-55.6805	598010.5207	567907.7665
80111	110+02.96	-55.5551	598005.6969	567909.0823
80112	110+02.68	-67.0096	598002.6817	567898.0280
80113	109+95.20	-66.7385	597995.4687	567900.0829
80114	109+94.91	-74.7190	597993.2807	567892.4028

- NOTES:
- OFFSETS SHOWN IN THE GEOMETRY TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE OLD BALTIMORE PIKE AND SR72 CONSTRUCTION AND R/W BASELINES.
 - UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND CURB AND GUTTER IS GIVEN TO THE EDGE OF PAVEMENT.
 - RADIARE GIVEN TO THE EDGE OF PAVEMENT.
 - UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT CURB RETURNS ARE GIVEN AT 10 FOOT INTERVALS.
 - ALL WORK REQUIRED FOR CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501 - CONSTRUCTION ENGINEERING.
 - PROPOSED CURBS OR PAVEMENT THAT TIE INTO EXISTING PAVEMENT OR CURBS SHALL MATCH THE EXISTING PAVEMENT OR CURB ELEVATIONS.
 - GRADES AND GEOMETRICS FOR PORKCHOP ISLANDS AND NORTHWEST MULTI-USE PATH SHOWN ON GRADES AND GEOMETRICS DETAIL SHEETS.

POB STA 1+00.00



ADDENDUMS / REVISIONS



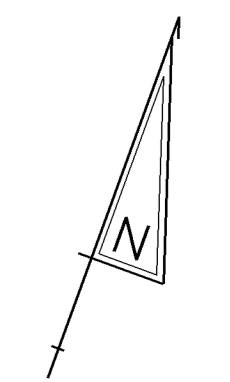
HEP NCC,
SR 72 AND OLD BALTIMORE PIKE
INTERSECTION IMPROVEMENTS

CONTRACT	BRIDGE NO.	N/A
T201200108	DESIGNED BY:	BCD
COUNTY	CHECKED BY:	MCN
NEW CASTLE		

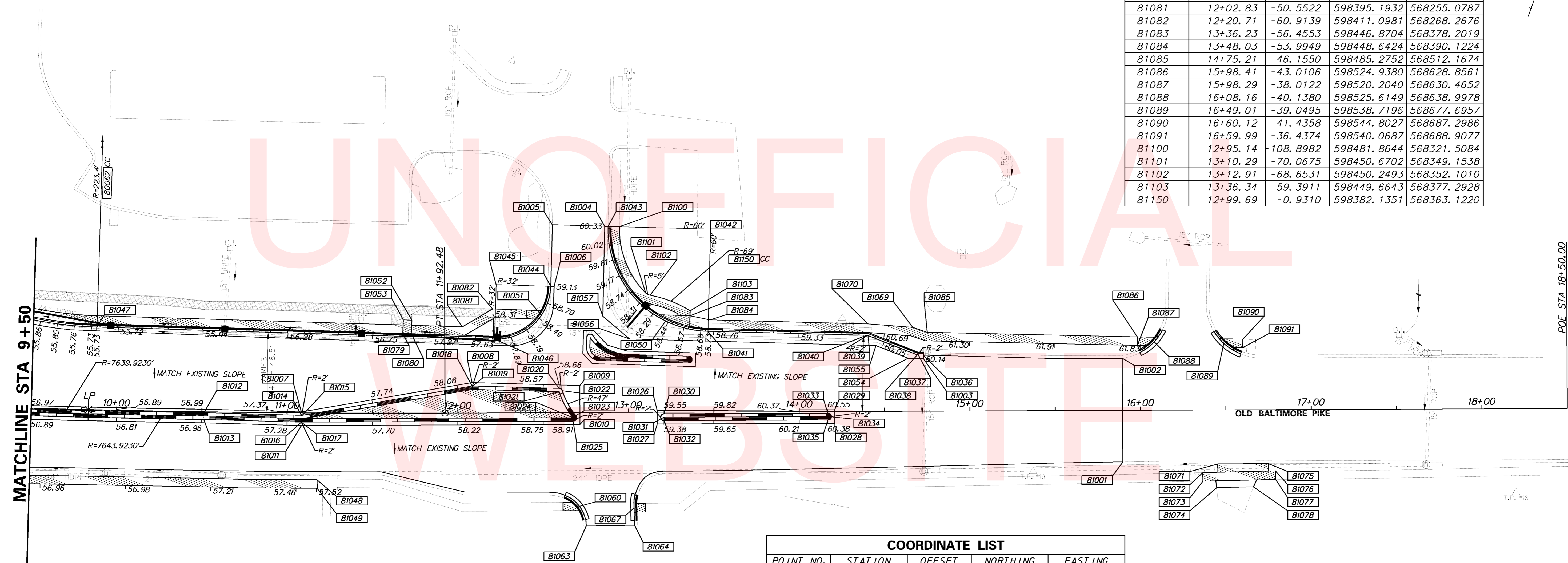
GRADES AND
GEOMETRICS

SHEET NO.	17
TOTAL SHTS.	52

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COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
81080	11+72.81	-46.5542	598381.1250	568228.4011
81081	12+02.83	-50.5522	598395.1932	568255.0787
81082	12+20.71	-60.9139	598411.0981	568268.2676
81083	13+36.23	-56.4553	598446.8704	568378.2019
81084	13+48.03	-53.9949	598448.6424	568390.1224
81085	14+75.21	-46.1550	598485.2752	568512.1674
81086	15+98.41	-43.0106	598524.9380	568628.8561
81087	15+98.29	-38.0122	598520.2040	568630.4652
81088	16+08.16	-40.1380	598525.6149	568638.9978
81089	16+49.01	-39.0495	598538.7196	568677.6957
81090	16+60.12	-41.4358	598544.8027	568687.2986
81091	16+59.99	-36.4374	598540.0687	568688.9077
81100	12+95.14	-108.8982	598481.8644	568321.5084
81101	13+10.29	-70.0675	598450.6702	568349.1538
81102	13+12.91	-68.6531	598450.2493	568352.1010
81103	13+36.34	-59.3911	598449.6643	568377.2928
81150	12+99.69	-0.9310	598382.1351	568363.1220



COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
80062	9+89.11	-271.9177	598533.9389	567983.8993
81001	15+89.48	30.2719	598453.0881	568645.8193
81002	15+89.47	-30.5443	598510.1488	568624.7792
81003	14+72.92	-30.7909	598470.0680	568515.3346
81004	12+86.48	-109.1596	598479.1150	568313.2938
81005	12+55.88	-110.1577	598469.4669	568284.2348
81006	12+56.68	-74.0908	598435.9051	568297.4664
81007	11+08.36	-0.5393	598316.0295	568183.5838
81008	12+08.43	-17.8228	598366.4220	568271.6569
81009	12+71.51	-15.8123	598386.3506	568331.5325
81010	12+71.51	6.9973	598364.9489	568339.4216
81011	11+08.36	7.4609	598308.4919	568186.2646
81012	10+51.20	0.4830	598296.0903	568130.0103
81013	10+51.18	4.4830	598292.3034	568131.2989
81014	11+08.32	1.4603	598314.1318	568184.2154
81015	11+08.71	1.4293	598314.2935	568184.5770
81016	11+08.29	5.4605	598310.3528	568185.5277
81017	11+08.35	5.4608	598310.3739	568185.5878
81018	12+08.37	-15.7844	598364.4883	568272.3046
81019	12+08.77	-15.8111	598364.6500	568272.6663
81020	12+59.45	-14.1957	598380.6622	568320.7748
81021	12+59.38	-12.1967	598378.7646	568321.4064
81022	12+61.28	-12.8152	598380.0027	568322.9771

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
81023	12+69.08	1.7589	598369.0237	568335.3302
81024	12+67.51	2.9973	598367.3185	568334.2850
81025	12+67.51	4.9973	598365.4419	568334.9768
81026	13+16.83	-1.0027	598388.1307	568379.1795
81027	13+16.83	6.9973	598380.6244	568381.9465
81028	14+20.83	6.9973	598416.5950	568479.5278
81029	14+20.83	-1.0027	598424.1013	568476.7608
81030	13+20.83	0.9973	598387.6376	568383.6244
81031	13+20.83	2.9973	598385.7611	568384.3161
81032	13+20.83	4.9973	598383.8845	568385.0079
81033	14+16.83	0.9973	598420.8412	568473.6995
81034	14+16.83	2.9973	598418.9647	568474.3912
81035	14+16.83	4.9973	598417.0881	568475.0829
81036	14+72.98	-32.7901	598471.9645	568514.6993
81037	14+69.93	-32.8812	598470.9966	568511.8100
81038	14+69.25	-33.0229	598470.8936	568511.1211
81039	14+40.52	-44.4867	598471.7116	568480.1955
81040	14+39.84	-44.6282	598471.6083	568479.5061
81041	13+46.68	-47.3527	598441.9444	568391.1564
81042	13+48.45	-107.3266	598498.8284	568372.0720
81043	12+88.47	-109.0946	598479.7439	568315.1880
81044	12+54.69	-74.1569	598435.2764	568295.5696
81045	12+22.70	-75.2159	598425.2082	568265.1948

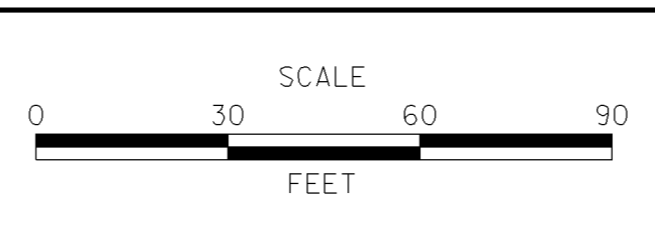
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
81046	12+21.60	-43.2349	598394.8198	568275.2222
81047	9+87.36	-48.5237	598321.7898	568053.9036
81048	11+17.72	39.1813	598281.7745	568205.7749
81049	11+17.84	44.1815	598277.1070	568207.5723
81050	13+21.91	-53.5146	598439.1590	568365.7845
81051	12+49.18	-57.2411	598417.4996	568296.2524
81052	11+73.07	-54.5501	598388.7221	568225.8942
81053	11+68.04	-54.7079	598387.1553	568221.1461
81054	14+63.97	-31.0203	598467.1877	568506.8578
81055	14+45.08	-38.4890	598467.6633	568486.5540
81056	13+00.92	-43.0386	598422.0686	568349.7103
81057	12+85.13	-56.4556	598429.1964	568330.2547
81060	12+71.07	54.9710	598319.7838	568355.6016
81063	12+74.94	66.5645	598310.2461	568363.2475
81064	13+03.30	66.1491	598320.4432	568389.7090
81067	13+03.00	55.9977	598329.8650	568385.9183
81069	14+66.80	-49.1187	598485.1481	568503.2537
81070	14+40.67	-51.2826	598478.1394	568477.9841
81071	16+45.08	31.6633	598471.0130	568698.4691
81072	16+36.25	36.7139	598463.2191	568691.9280
81073	16+36.34	41.7131	598458.5592	568693.7405
81074	16+44.42	45.5929	598457.7158	568702.6700
81075	16+74.81	31.5979	598481.3584	568726.3450
81076	16+79.00	35.9540	598478.7207	568731.7838
81077	16+79.09	40.9532	598474.0608	568733.5963
81078	16+66.03	45.1926	598465.5661	568722.8093
81079	11+67.79	-46.7118	598379.5582	568223.6529

- NOTES:
- OFFSETS SHOWN IN THE GEOMETRY TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE OLD BALTIMORE PIKE CONSTRUCTION AND R/W BASELINE.
 - UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND CURB AND GUTTER IS GIVEN TO THE EDGE OF PAVEMENT.
 - RADIARE GIVEN TO THE EDGE OF PAVEMENT.
 - UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT CURB RETURNS ARE GIVEN AT 10 FOOT INTERVALS.
 - ALL WORK REQUIRED FOR CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501 - CONSTRUCTION ENGINEERING.
 - PROPOSED CURBS OR PAVEMENT THAT TIE INTO EXISTING PAVEMENT OR CURBS SHALL MATCH THE EXISTING PAVEMENT OR CURB ELEVATIONS.
 - GRADES AND GEOMETRICS FOR ROYAL FARMS ENTRANCE MEDIAN SHOWN ON GRADES AND GEOMETRICS DETAIL SHEET.
 - AT LOCATIONS WHERE CURB OR CURB AND GUTTER IS REMOVED FOR CURB RAMP REPLACEMENT, EXISTING CURB OR CURB AND GUTTER SHALL BE REMOVED FOR A MINIMUM OF 10' OR TO THE NEAREST JOINT IN THE EXISTING CURB.

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ADDENDUMS / REVISIONS	



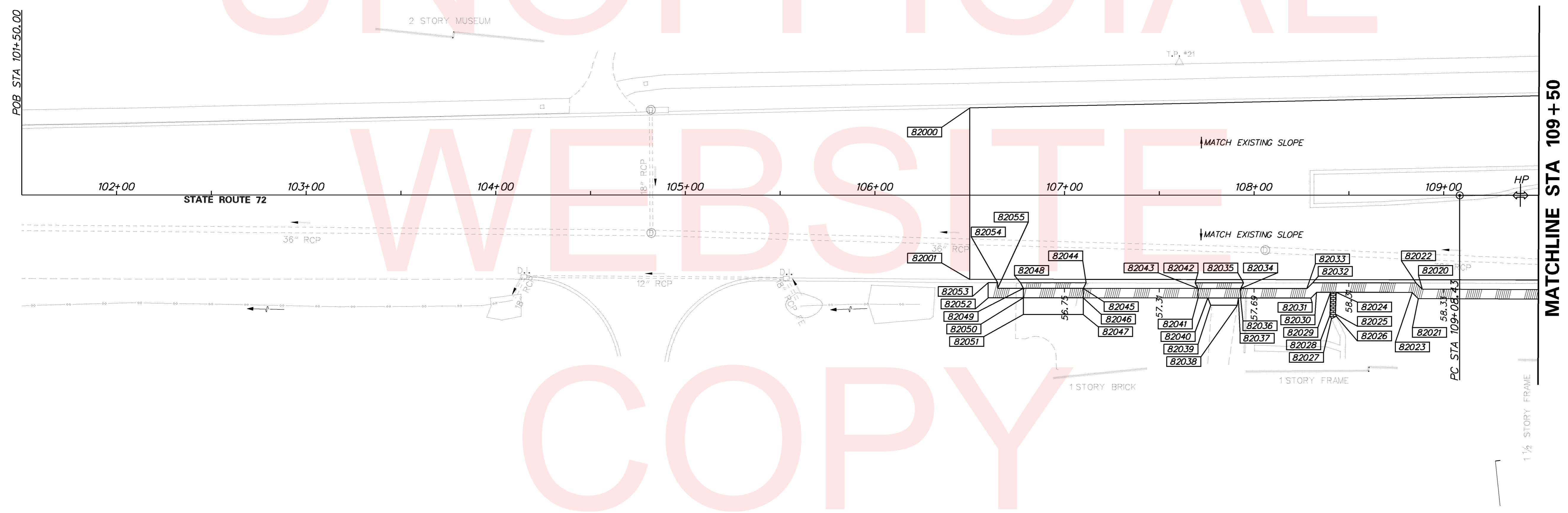
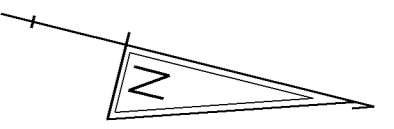
**HEP NCC,
SR 72 AND OLD BALTIMORE PIKE
INTERSECTION IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T201200108	DESIGNED BY:	BCD
COUNTY	CHECKED BY:	MCN
NEW CASTLE		

GRADES AND GEOMETRICS	SHEET NO.	18
	TOTAL SHTS.	52

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
82000	106+50.00	-46.0547	597665.3060	568003.4883
82001	106+50.00	44.4284	597687.1668	568091.2908
82020	108+88.65	49.4967	597919.9725	568038.5505
82021	108+86.56	54.4889	597919.1507	568043.8997
82022	108+85.46	46.2768	597916.0933	568036.1981
82023	108+83.37	51.2710	597915.2740	568041.5488
82024	108+43.27	51.2392	597876.3585	568051.2051
82025	108+42.93	62.7255	597878.8009	568062.4339
82026	108+43.45	63.8261	597879.5717	568063.3762
82027	108+40.81	65.0648	597877.3051	568065.2171
82028	108+40.04	63.4277	597876.1654	568063.8138
82029	108+40.34	51.2394	597873.5171	568051.9127
82030	108+32.88	51.2398	597866.2793	568053.7152
82031	108+29.86	54.2375	597864.0728	568057.3538
82032	108+30.82	46.2400	597863.0721	568049.3612
82033	108+27.81	49.2363	597860.8666	568052.9982
82034	107+92.93	49.2160	597827.0215	568061.4040
82035	107+94.42	46.2398	597827.7485	568058.1558
82036	107+92.93	54.2160	597828.2267	568066.2566

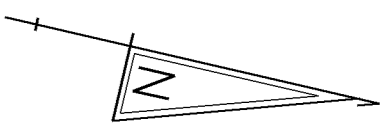
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
82037	107+91.69	54.2153	597827.0209	568066.5560
82038	107+91.13	58.0000	597827.3975	568070.3625
82039	107+77.09	58.0000	597813.7732	568073.7546
82040	107+75.63	54.2060	597811.4395	568070.4258
82041	107+70.35	54.2029	597806.3165	568071.6982
82042	107+68.90	46.2753	597802.9948	568064.3556
82043	107+70.36	49.2029	597805.1114	568066.8456
82044	107+11.40	46.2389	597747.1863	568078.2131
82045	107+09.93	49.1678	597746.4708	568081.4096
82046	107+09.93	54.1678	597747.6760	568086.2622
82047	107+09.93	63.0000	597749.8049	568094.8339
82048	106+76.83	46.1319	597713.6153	568086.4613
82049	106+78.34	49.1495	597715.8063	568089.0254
82050	106+78.34	54.1495	597717.0115	568093.8780
82051	106+78.33	63.0000	597719.1448	568102.4676
82052	106+59.73	54.1387	597698.9566	568098.3621
82053	106+59.74	46.1387	597697.0283	568090.5980
82054	106+64.74	46.1416	597701.8808	568089.3928
82055	106+64.73	49.1416	597702.6040	568092.3044



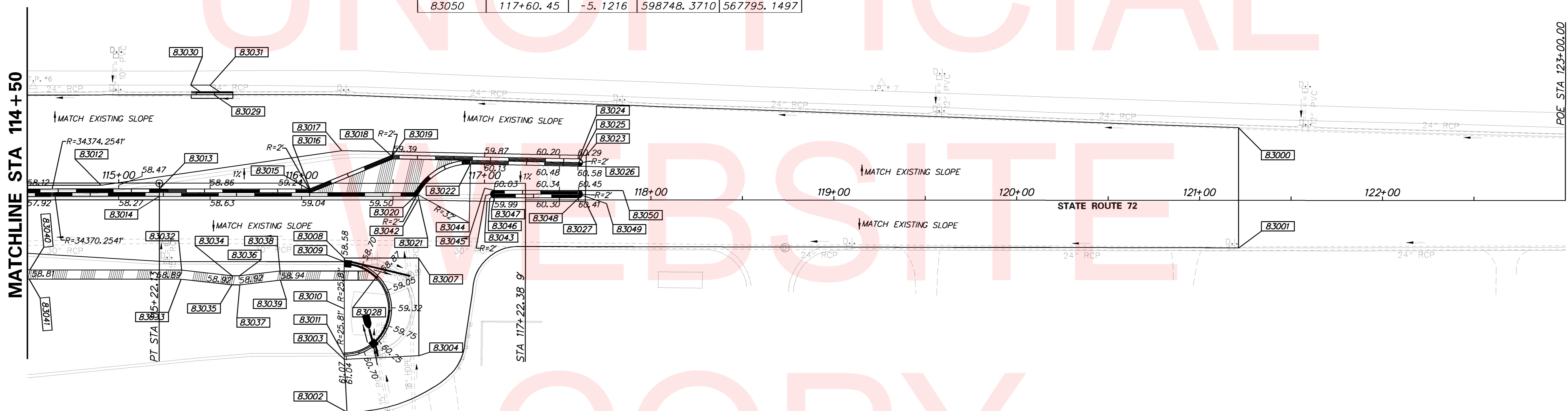
- NOTES:**
1. OFFSETS SHOWN IN THE GEOMETRY TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE STATE ROUTE 72 CONSTRUCTION AND R/W BASELINE.
 2. UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND CURB AND GUTTER IS GIVEN TO THE EDGE OF PAVEMENT.
 3. RADIARE GIVEN TO THE EDGE OF PAVEMENT.
 4. UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT CURB RETURNS ARE GIVEN AT 10 FOOT INTERVALS.
 5. ALL WORK REQUIRED FOR CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501 - CONSTRUCTION ENGINEERING.
 6. PROPOSED CURBS OR PAVEMENT THAT TIE INTO EXISTING PAVEMENT OR CURBS SHALL MATCH THE EXISTING PAVEMENT OR CURB ELEVATIONS.

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<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>	ADDENDUMS / REVISIONS	<p>SCALE 0 30 60 90 FEET</p>	<p>HEP NCC, SR 72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr><td>CONTRACT</td><td>BRIDGE NO.</td><td style="text-align: center;">N/A</td></tr> <tr><td>T201200108</td><td>DESIGNED BY: BCD</td><td></td></tr> <tr><td>COUNTY</td><td>CHECKED BY: MCN</td><td></td></tr> <tr><td>NEW CASTLE</td><td></td><td></td></tr> </table>	CONTRACT	BRIDGE NO.	N/A	T201200108	DESIGNED BY: BCD		COUNTY	CHECKED BY: MCN		NEW CASTLE			<p>GRADES AND GEOMETRICS</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr><td>SHEET NO.</td><td style="text-align: center;">19</td></tr> <tr><td>TOTAL SHTS.</td><td style="text-align: center;">52</td></tr> </table>	SHEET NO.	19	TOTAL SHTS.	52
	CONTRACT	BRIDGE NO.	N/A																			
T201200108	DESIGNED BY: BCD																					
COUNTY	CHECKED BY: MCN																					
NEW CASTLE																						
SHEET NO.	19																					
TOTAL SHTS.	52																					



COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
83032	115+34.40	47.5504	598546.6416	567886.3811
83033	115+34.12	52.5504	598547.4908	567891.3163
83034	115+62.12	50.6495	598574.3460	567883.1864
83035	115+61.84	55.6498	598575.1986	567888.1212
83036	115+65.88	50.6450	598578.0108	567882.3385
83037	115+66.18	55.6447	598579.4238	567887.1437
83038	115+88.16	47.9920	598599.1325	567874.7560
83039	115+88.46	52.9922	598600.5412	567879.5627
83040	114+51.28	47.4639	598465.7304	567904.9849
83041	114+50.91	52.4631	598466.4978	567909.9389
83042	116+63.53	9.2159	598663.8800	567820.0681
83043	117+01.35	9.8784	598700.8921	567812.2314
83044	116+91.62	21.4898	598694.0131	567825.7291
83045	117+05.35	3.8784	598703.4448	567805.4873
83046	117+05.35	5.8784	598703.8933	567807.4363
83047	117+05.35	7.8784	598704.3417	567809.3854
83048	117+60.46	-1.1216	598749.2716	567799.0470
83049	117+60.46	-3.1216	598748.8232	567797.0979
83050	117+60.45	-5.1216	598748.3710	567795.1497



COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
83000	121+21.27	-39.7515	599092.2338	567680.4917
83001	121+21.27	25.9963	599106.9771	567744.5652
83002	116+24.83	125.1386	598652.1652	567941.7157
83003	116+23.42	96.2759	598644.3132	567913.9055
83004	116+64.10	94.2802	598683.5123	567902.8379
83007	116+64.02	40.8405	598671.4507	567850.7772
83008	116+23.35	40.8739	598631.8215	567859.9302
83009	116+23.35	42.8739	598632.2729	567861.8785
83010	116+23.39	68.6874	598638.0992	567887.0260
83011	116+23.41	94.5009	598643.9126	567912.1763
83012	114+90.35	1.2159	598493.3224	567851.1196
83013	115+22.13	3.2159	598524.7394	567845.9278
83014	115+22.13	7.2159	598525.6364	567849.8259
83015	116+03.73	3.2159	598604.2606	567827.6299

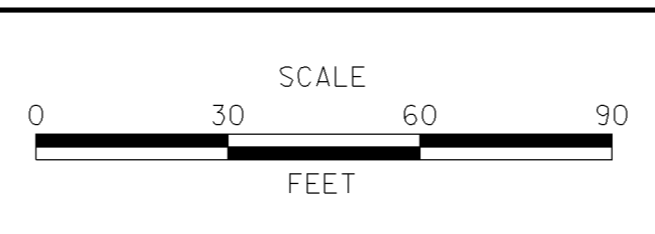
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
83016	116+04.49	3.0667	598604.9658	567827.3145
83017	116+21.99	-17.8307	598617.3374	567803.0243
83018	116+48.99	-15.1572	598644.2447	567799.5763
83019	116+49.78	-15.3060	598644.9868	567799.2528
83020	116+61.77	7.2159	598661.7203	567818.5128
83021	116+63.53	6.1732	598663.1977	567817.1029
83022	116+92.23	-10.5046	598687.4269	567794.4142
83023	117+64.57	-24.3106	598748.0795	567775.5264
83024	117+60.53	-22.3864	598744.5767	567778.3068
83025	117+60.49	-20.3867	598744.9884	567780.2640
83026	117+60.46	-18.3871	598745.4000	567782.2212
83027	117+64.11	0.2159	598753.1284	567799.5320
83028	116+41.52	50.3182	598651.6541	567865.0578
83029	115+52.14	-48.3539	598542.4258	567788.9407
83030	115+39.30	-53.6068	598528.7313	567786.7016
83031	115+49.87	-53.5795	598539.0395	567784.3578

- NOTES:
- OFFSETS SHOWN IN THE GEOMETRY TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE STATE ROUTE 72 CONSTRUCTION AND R/W BASELINE.
 - UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND CURB AND GUTTER IS GIVEN TO THE EDGE OF PAVEMENT.
 - RADIARE GIVEN TO THE EDGE OF PAVEMENT.
 - UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT CURB RETURNS ARE GIVEN AT 10 FOOT INTERVALS.
 - ALL WORK REQUIRED FOR CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501 - CONSTRUCTION ENGINEERING.
 - PROPOSED CURBS OR PAVEMENT THAT TIE INTO EXISTING PAVEMENT OR CURBS SHALL MATCH THE EXISTING PAVEMENT OR CURB ELEVATIONS.

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ADDENDUMS / REVISIONS	



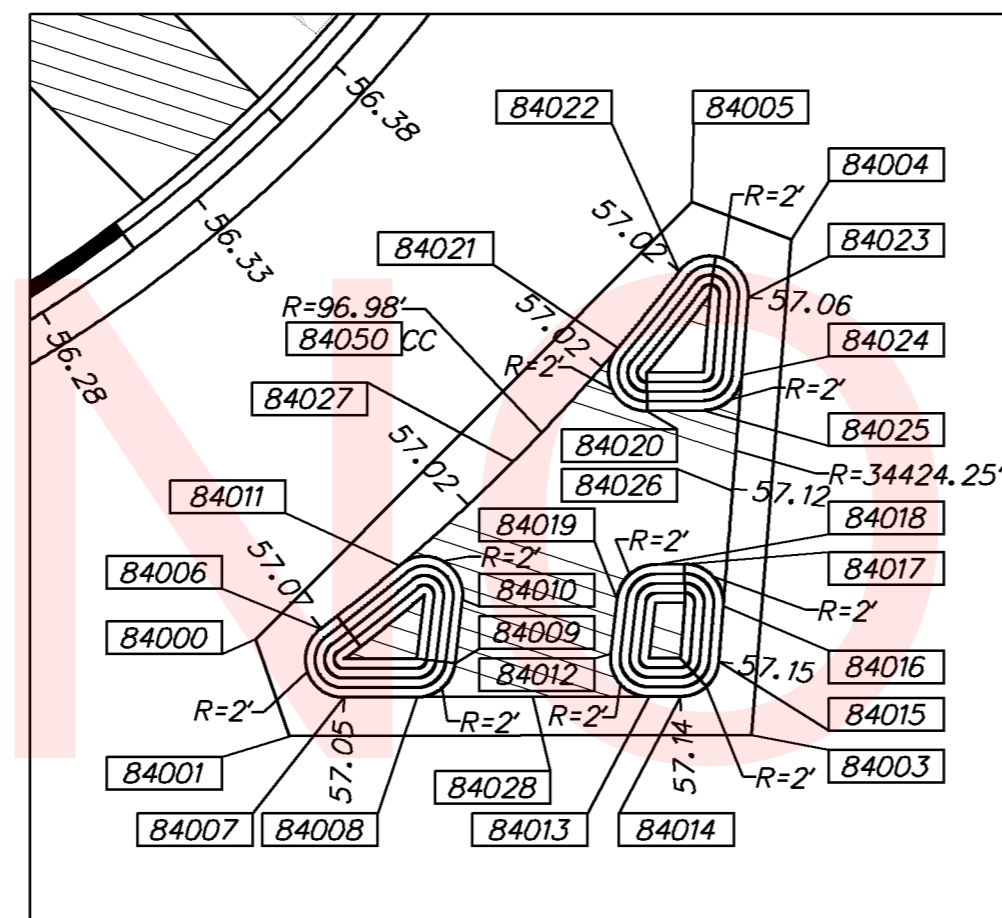
**HEP NCC,
SR 72 AND OLD BALTIMORE PIKE
INTERSECTION IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T201200108	DESIGNED BY:	BCD
COUNTY	CHECKED BY:	MCN
NEW CASTLE		

GRADES AND GEOMETRICS	SHEET NO.	20
	TOTAL SHTS.	52

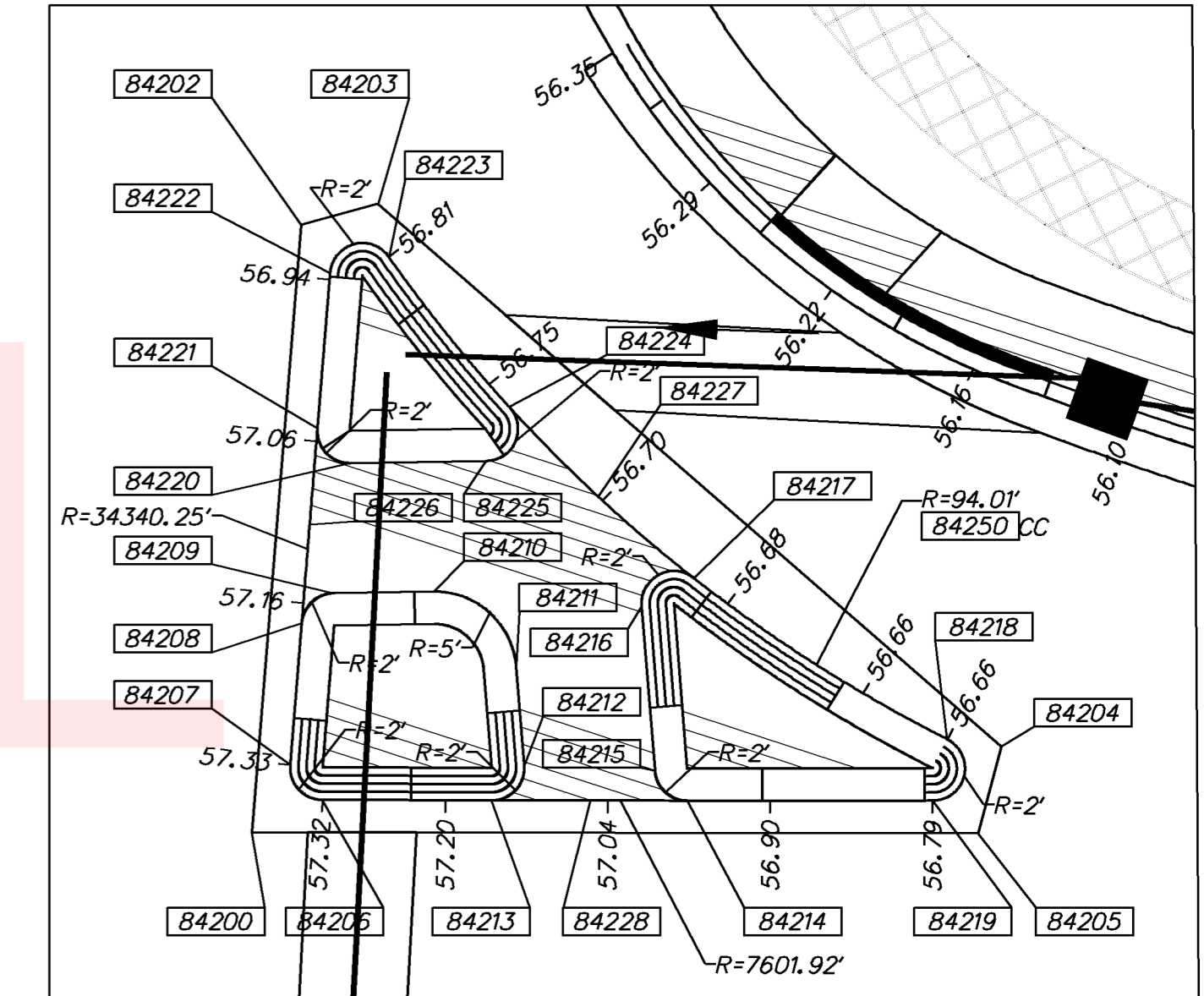
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
84000	7+54.92	-44.2083	598246.3860	567834.9765
84001	7+56.72	-39.1808	598242.1365	567838.2074
84003	7+80.78	-39.4271	598249.6389	567861.0717
84004	7+82.67	-65.2231	598274.8012	567855.0827
84005	7+77.44	-67.1484	598275.0561	567849.5132
84006	7+58.31	-44.7963	598247.9688	567838.0252
84007	7+59.55	-41.2098	598244.9238	567840.2888
84008	7+63.35	-41.2487	598246.1094	567843.9017
84009	7+65.32	-43.0294	598248.4012	567845.2394
84010	7+65.69	-46.3756	598251.7025	567844.5805
84011	7+62.40	-48.1130	598252.3651	567840.9195
84012	7+73.43	-43.5916	598251.3869	567852.8013
84013	7+75.44	-41.3725	598249.8781	567855.3862
84014	7+77.08	-41.3894	598250.3919	567856.9520
84015	7+79.06	-43.2436	598252.7559	567858.2740
84016	7+79.27	-46.1419	598255.5828	567857.6004
84017	7+77.23	-48.2873	598257.0117	567855.0075
84018	7+75.69	-48.2522	598256.5125	567853.5482
84019	7+73.74	-46.4719	598254.2286	567852.2341
84020	7+75.25	-56.2443	598263.9982	567850.7143
84021	7+73.67	-59.5266	598266.6500	567848.2168
84022	7+76.85	-63.5486	598271.4465	567850.0388
84023	7+80.45	-62.2020	598271.2485	567853.8724
84024	7+80.15	-58.1644	598267.3098	567854.8090
84025	7+78.20	-56.3114	598264.9547	567853.5105
84026	7+79.71	-52.1532	598261.4462	567856.2042
84027	7+68.27	-53.5864	598259.3564	567844.8640
84028	7+69.39	-41.3106	598247.9937	567849.6439
84050	6+99.26	-121.7319	598303.4757	567758.4981

NORTH WEST PORK CHOP ISLAND



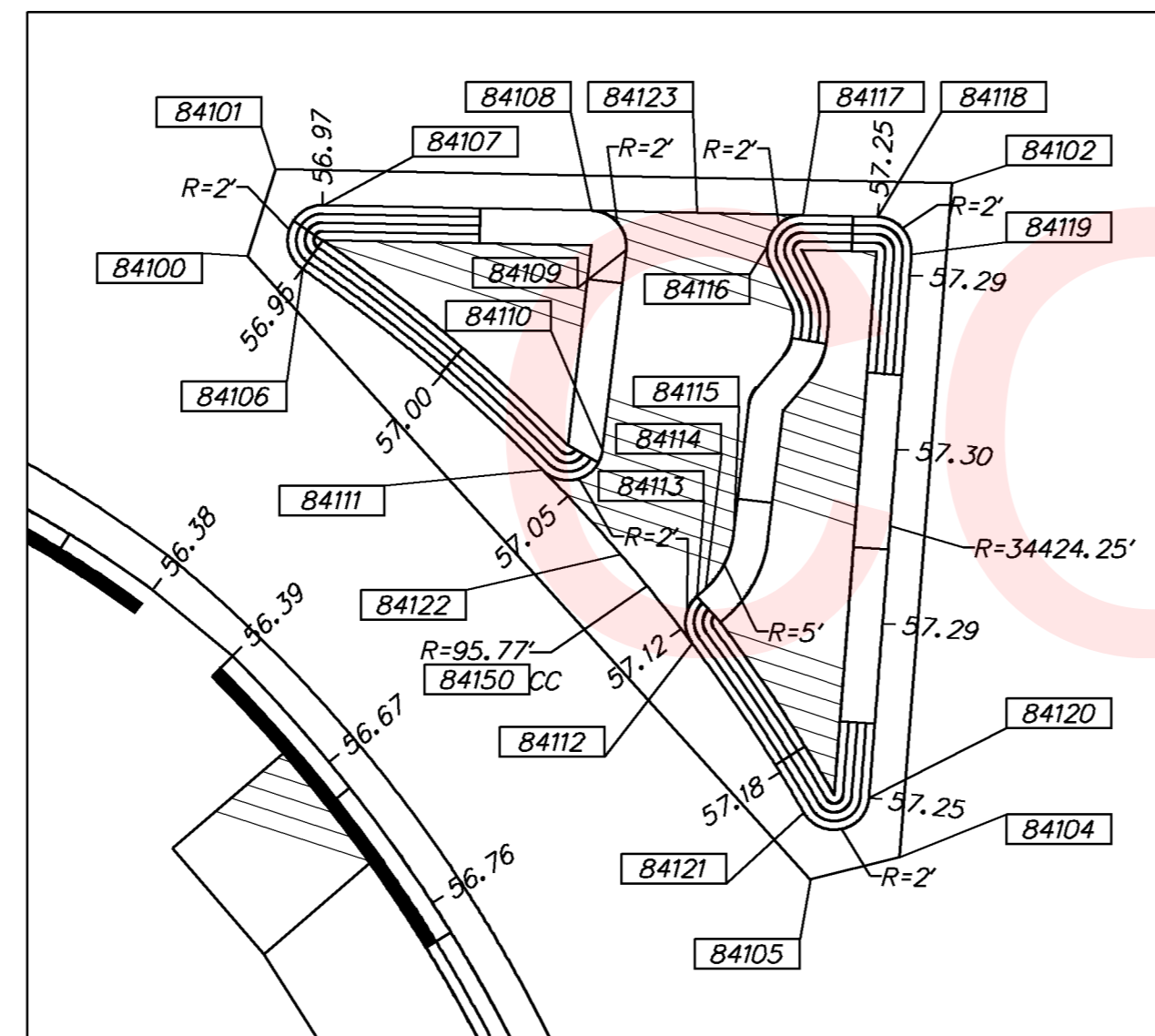
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
84200	8+60.82	-35.5052	598270.0734	567938.4419
84202	8+63.71	-72.9397	598306.5996	567929.7579
84203	8+68.47	-74.2443	598309.2818	567933.8477
84204	9+07.20	-40.8495	598289.3616	567980.7151
84205	9+05.76	-35.5088	598283.8380	567981.0054
84206	8+65.16	-37.5170	598273.3071	567941.9417
84207	8+63.15	-39.6695	598274.7481	567939.3854
84208	8+63.82	-48.4027	598283.2687	567937.3567
84209	8+65.79	-50.2492	598285.6240	567938.6551
84210	8+72.02	-50.3653	598287.6257	567944.5069
84211	8+77.12	-45.7835	598284.8195	567950.7399
84212	8+77.63	-39.6834	598279.1699	567953.0962
84213	8+75.63	-37.5170	598276.4964	567951.8636
84214	8+87.77	-37.5170	598280.2093	567963.3560
84215	8+85.73	-39.3532	598281.3319	567960.8624
84216	8+84.89	-49.5043	598290.7325	567956.9417
84217	8+88.14	-51.2423	598293.3800	567959.4764
84218	9+03.83	-41.3003	598288.7487	567977.3857
84219	9+02.92	-37.5170	598284.8711	567977.6981
84220	8+66.80	-58.2698	598293.5680	567937.1609
84221	8+64.75	-60.4236	598293.9981	567934.5678
84222	8+65.50	-69.9881	598304.3316	567932.3516
84223	8+69.16	-70.9907	598306.3941	567935.4965
84224	8+76.67	-61.7760	598299.9035	567945.4076
84225	8+75.22	-58.4244	598296.2723	567945.0668
84226	8+64.29	-54.4700	598289.1887	567935.9485
84227	8+82.19	-56.2762	598296.3511	567952.3072
84228	8+81.71	-37.5170	598278.3557	567957.6264
84250	9+47.10	-124.9981	598381.6359	567991.8526

NORTH EAST PORK CHOP ISLAND

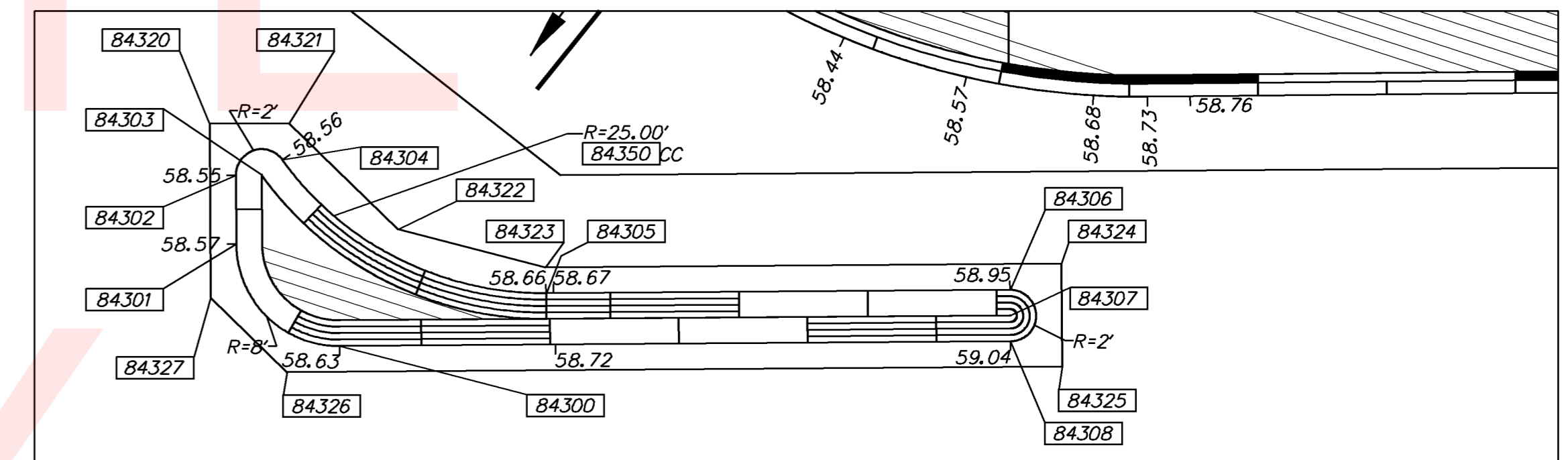


COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
84100	7+36.15	25.7033	598174.0678	567838.1922
84101	7+37.71	20.7213	598179.2899	567838.1796
84102	7+76.41	21.2559	598190.4696	567875.2332
84104	7+73.69	59.8409	598152.8645	567884.2924
84105	7+68.60	61.1311	598150.0980	567879.8324
84106	7+39.29	26.4145	598174.3405	567841.4074
84107	7+40.44	22.7593	598178.1724	567841.3995
84108	7+55.83	22.9718	598182.6180	567856.1340
84109	7+57.79	25.1909	598181.0947	567858.6731
84110	7+56.53	36.6272	598169.8116	567860.9251
84111	7+53.13	37.8251	598167.6429	567858.0465
84112	7+61.75	47.6679	598160.8630	567869.2348
84113	7+61.98	44.9894	598163.4863	567868.6466
84114	7+62.48	44.5260	598164.0791	567868.9832
84115	7+64.05	41.4069	598167.5270	567869.5388
84116	7+65.87	24.9190	598183.7941	567866.2921
84117	7+67.89	23.1383	598186.1004	567867.6758
84118	7+72.15	23.1972	598187.3321	567871.7581
84119	7+74.12	25.3387	598185.8849	567874.2804
84120	7+71.92	56.4301	598155.5830	567881.5801
84121	7+68.20	57.3007	598153.6292	567878.2958
84122	7+57.61	42.5992	598164.4442	567863.7565
84123	7+61.86	23.0551	598184.3592	567861.9049
84150	6+85.55	105.6888	598082.5365	567814.1217

SOUTH WEST PORK CHOP ISLAND

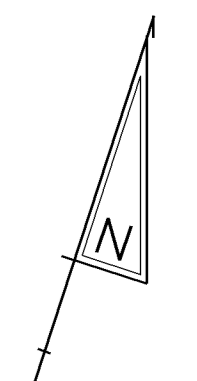


MEDIAN AT ROYAL FARMS ENTRANCE FROM OLD BALTIMORE PIKE



- NOTES:
1. OFFSETS SHOWN IN THE GEOMETRY TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE OLD BALTIMORE PIKE CONSTRUCTION AND R/W BASELINE.
 2. UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND CURB AND GUTTER IS GIVEN TO THE EDGE OF PAVEMENT.
 3. RADIARE GIVEN TO THE EDGE OF PAVEMENT.
 4. UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT CURB RETURNS ARE GIVEN AT 10 FOOT INTERVALS.
 5. ALL WORK REQUIRED FOR CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501 - CONSTRUCTION ENGINEERING.
 6. PROPOSED CURBS OR PAVEMENT THAT TIE INTO EXISTING PAVEMENT OR CURBS SHALL MATCH THE EXISTING PAVEMENT OR CURB ELEVATIONS.

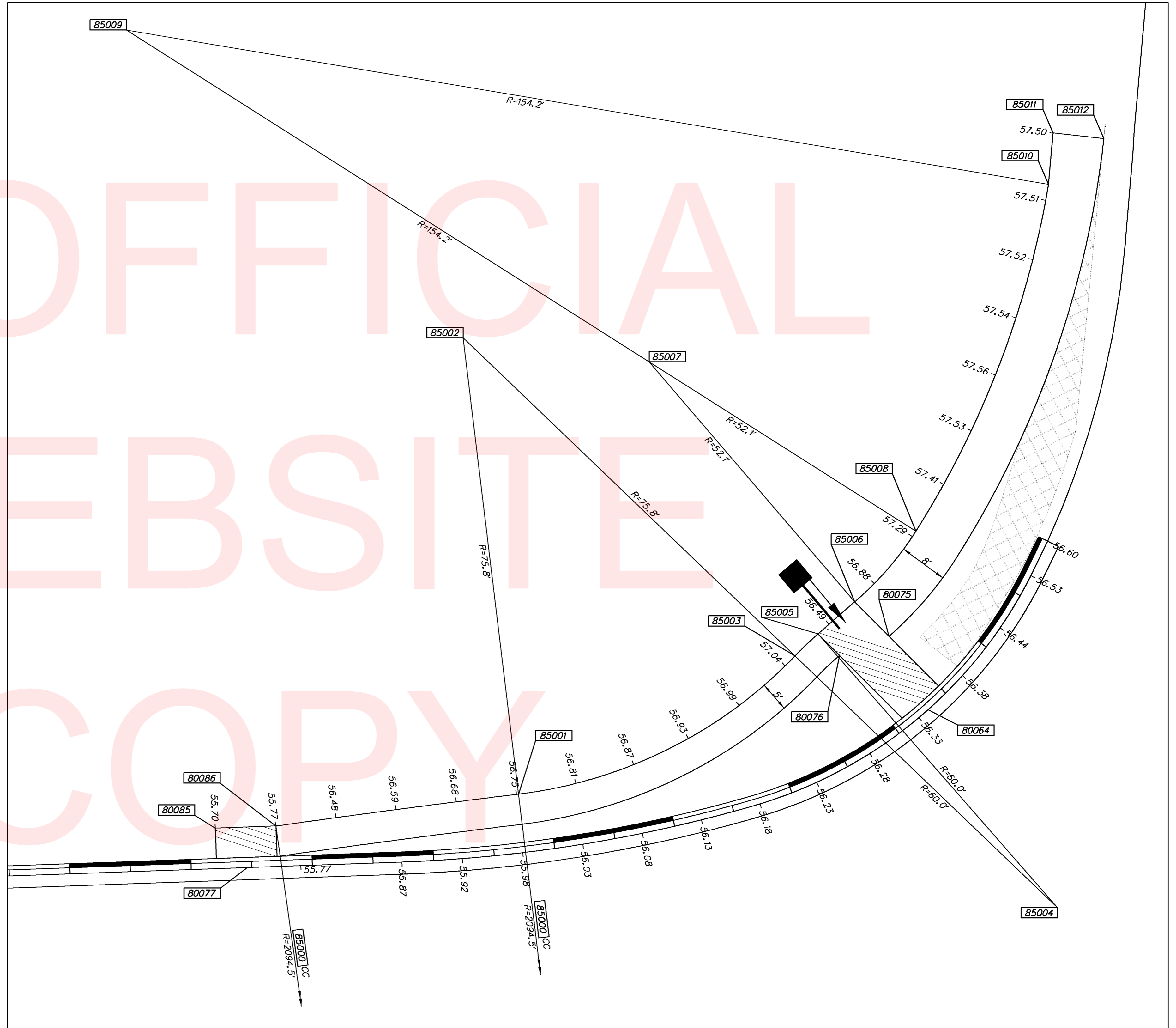
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
84300	12+83.26	-30.4453	598404.1454	568337.4985
84301	12+75.52	-38.7062	598409.2192	568327.3784
84302	12+75.70	-44.0462	598414.2908	568325.6974
84303	12+77.69	-43.9799	598414.9200	568327.5959
84304	12+79.36	-45.0866	598416.5345	568328.7762
84305	12+99.41	-33.9324	598413.0040	568351.4480
84306	13+35.43	-32.7842	598424.3851	568385.6426
84307	13+35.37	-30.7852	598422.4875	568386.2742
84308	13+35.30	-28.7862	598420.5898	568386.9058
84320	12+73.83	-48.1102	598417.4585	568322.5406
84321	12+79.99	-47.9059	598419.3986	568328.3946
84322	12+88.12	-39.3382	598414.1707	568338.9836
84323	12+99.43	-35.9329	598414.8866	568350.7712
84324	13+39.49	-34.6558	598427.5460	568388.8062
84325	13+39.24	-26.6598	598419.9554	568391.3327
84326	12+79.05	-28.5784	598400.9384	568334.1964
84327	12+73.38	-34.6289	598404.6546	568326.7841
84350	13+00.18	-58.9205	598436.7169	568343.5299



NORTH WEST SHARED-USE PATH

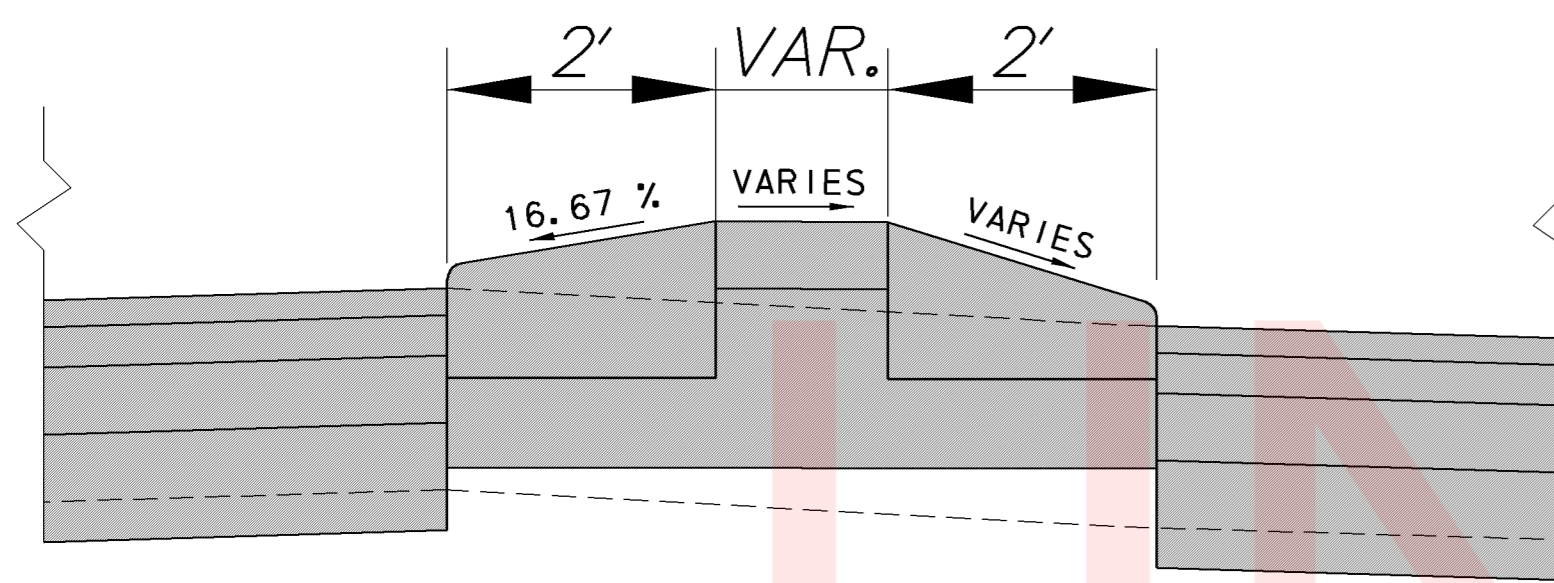
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
80064	7+53.20	-68.4535	598268.9792	567826.0133
80075	7+46.69	-80.5124	598278.5085	567816.1654
80076	7+38.56	-77.3031	598272.9918	567809.3793
80077	6+41.14	-41.5993	598209.5291	567727.2920
80085	6+35.86	-48.0464	598214.0802	567720.3111
80086	6+45.85	-48.4734	598217.5050	567729.7064
85000			596335.6821	568649.2907
85001	6+85.82	-54.0278	598234.8747	567766.1372
85002	6+76.12	-129.1649	598303.5715	567734.1922
85003	7+31.20	-77.1461	598270.6194	567802.4116
85004	7+74.82	-35.9490	598244.5225	567856.4389
85005	7+34.99	-80.8240	598275.2713	567804.9170
85006	7+40.99	-86.1477	598282.1570	567809.0233
85007	7+06.83	-125.4340	598309.2894	567764.5898
85008	7+50.99	-97.8618	598296.3442	567815.0173
85009	6+20.22	-179.5069	598334.6767	567665.6950
85010	113+65.61	-61.7068	598357.4581	567818.1664
85011	113+74.11	-61.6648	598365.7542	567816.2619
85012	113+73.88	-53.2446	598367.4569	567824.5112

- NOTES:
1. OFFSETS SHOWN IN THE GEOMETRY TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE OLD BALTIMORE PIKE AND SR72 CONSTRUCTION AND R/W BASELINES.
 2. UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND CURB AND GUTTER IS GIVEN TO THE EDGE OF PAVEMENT.
 3. RADIARE GIVEN TO THE EDGE OF PAVEMENT.
 4. UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT CURB RETURNS ARE GIVEN AT 10 FOOT INTERVALS.
 5. ALL WORK REQUIRED FOR CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501 - CONSTRUCTION ENGINEERING.
 6. PROPOSED CURBS OR PAVEMENT THAT TIE INTO EXISTING PAVEMENT OR CURBS SHALL MATCH THE EXISTING PAVEMENT OR CURB ELEVATIONS.



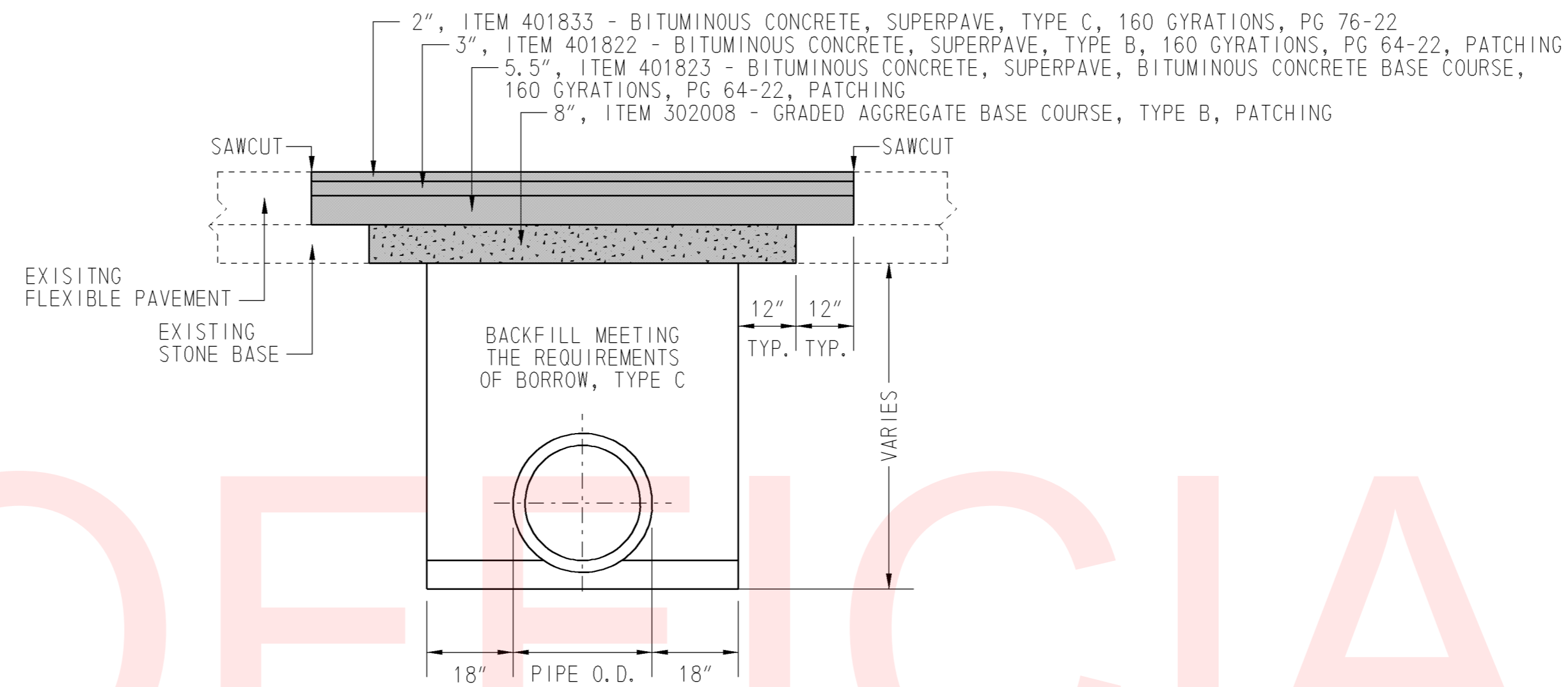
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DELAWARE DEPARTMENT OF TRANSPORTATION	ADDENDUMS / REVISIONS	SCALE FEET	HEP NCC, SR 72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS	CONTRACT T201200108 COUNTY NEW CASTLE	BRIDGE NO. N/A DESIGNED BY: BCD CHECKED BY: MCN	GRADES AND GEOMETRICS	SHEET NO. 22 TOTAL SHTS. 52



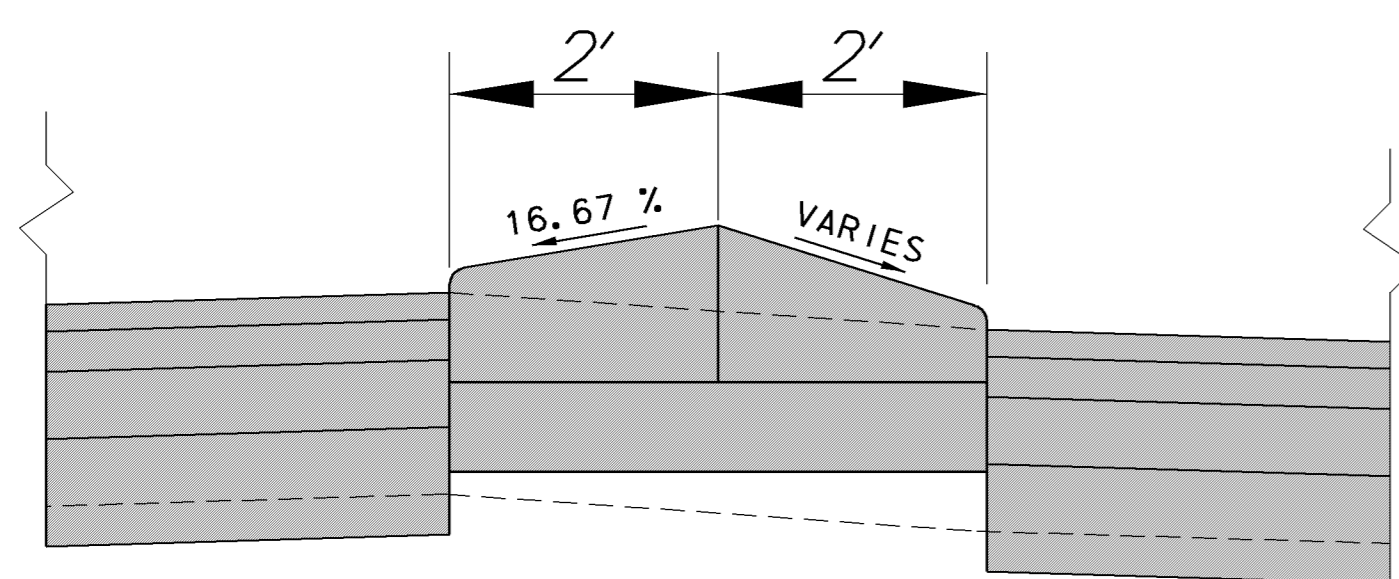
NOTES:
 1) ALL WORK NECESSARY FOR INSTALLATION OF TYPE 2 PCC CURB SHALL BE BID UNDER THE UNIT PRICE FOR ITEM 701011 - PORTLAND CEMENT CONCRETE CURB, TYPE 2.

PCC CURB, TYPE 2 - MEDIAN DETAIL
 NOT TO SCALE



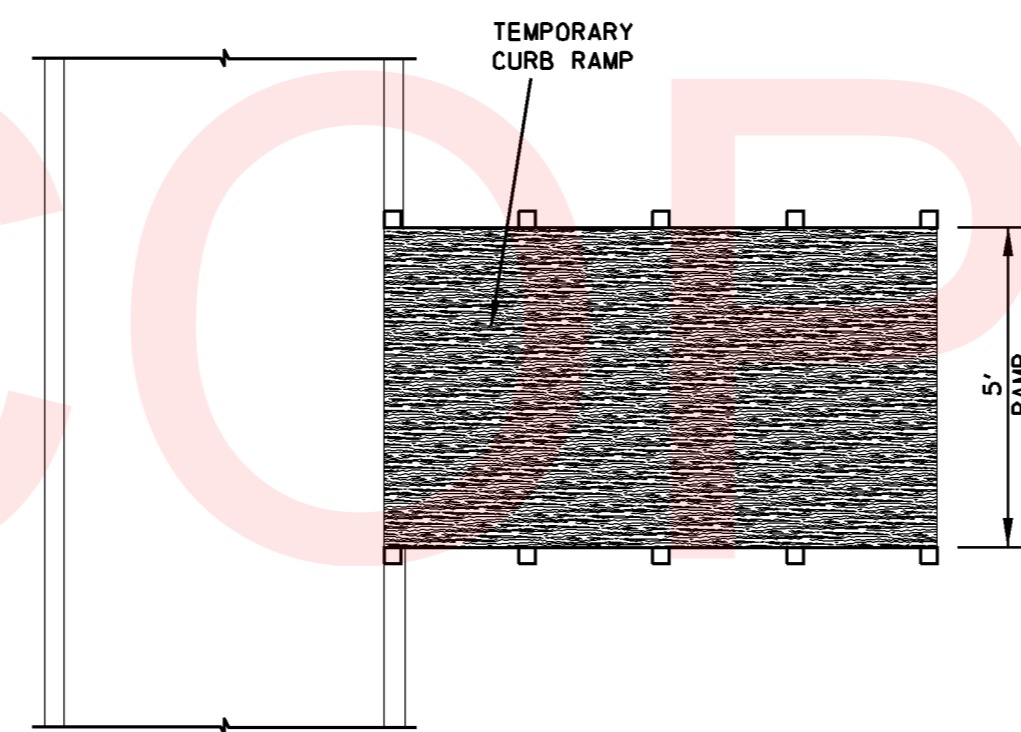
NOTES:
 1) SEE STANDARD CONSTRUCTION DETAILS FOR PIPE BEDDING REQUIREMENTS.
 2) THIS DETAIL SHOWS THE MINIMUM PAVEMENT PATCH REQUIREMENTS. IF THE EXISTING ROADWAY HAS A HEAVIER CROSS SECTION, THE PROPOSED PAVEMENT PATCH WILL MATCH THE HEAVIER CROSS SECTION, OR AS DIRECTED BY THE ENGINEER.

FLEXIBLE PAVEMENT PATCH
 NOT TO SCALE

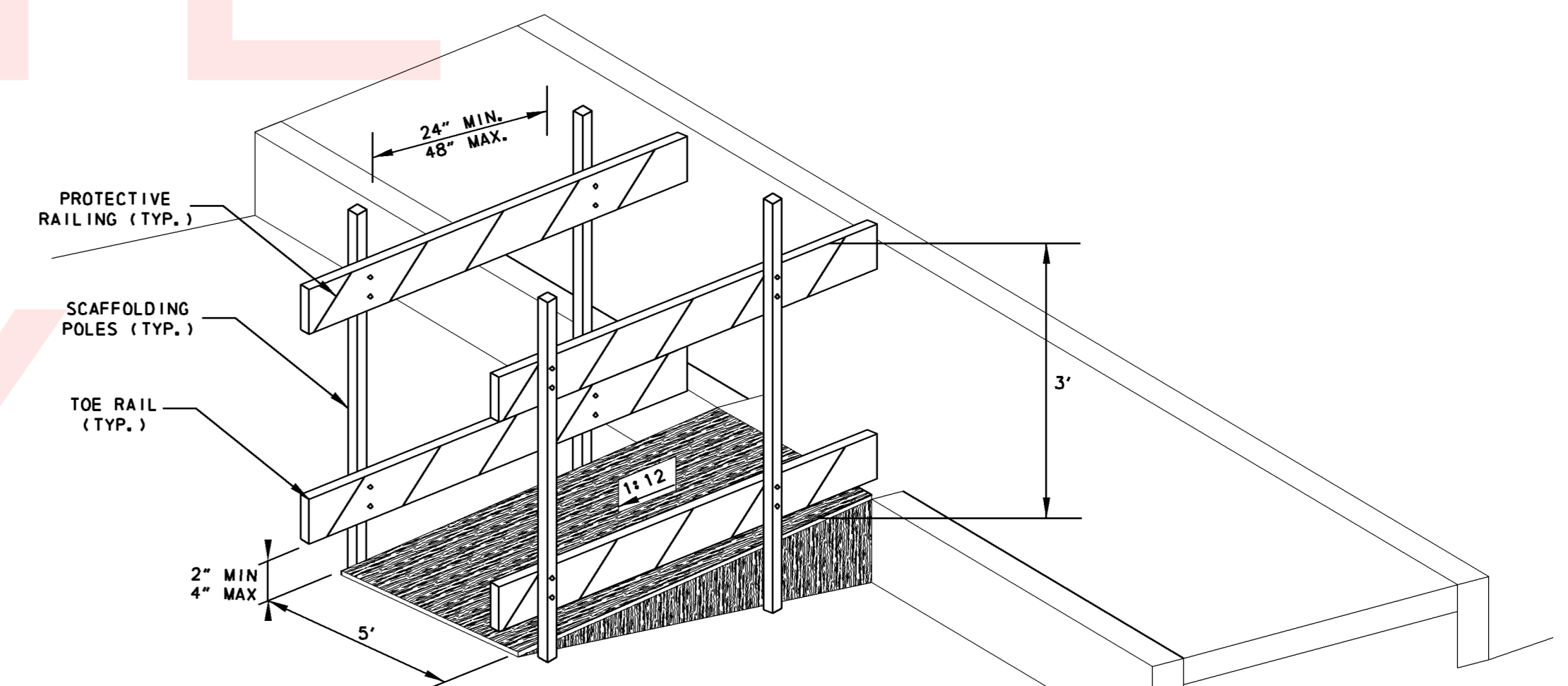


NOTES:
 1) PROPOSED BACK TO BACK PCC CURB, TYPE 2 MAY BE CONSTRUCTED MONOLITHICALLY.
 2) ALL WORK NECESSARY FOR INSTALLATION OF TYPE 2 PCC CURB SHALL BE BID UNDER THE UNIT PRICE FOR ITEM 701011 - PORTLAND CEMENT CONCRETE CURB, TYPE 2.

PCC CURB, TYPE 2 BACK-TO-BACK DETAIL
 NOT TO SCALE



PLAN VIEW



ISOMETRIC VIEW

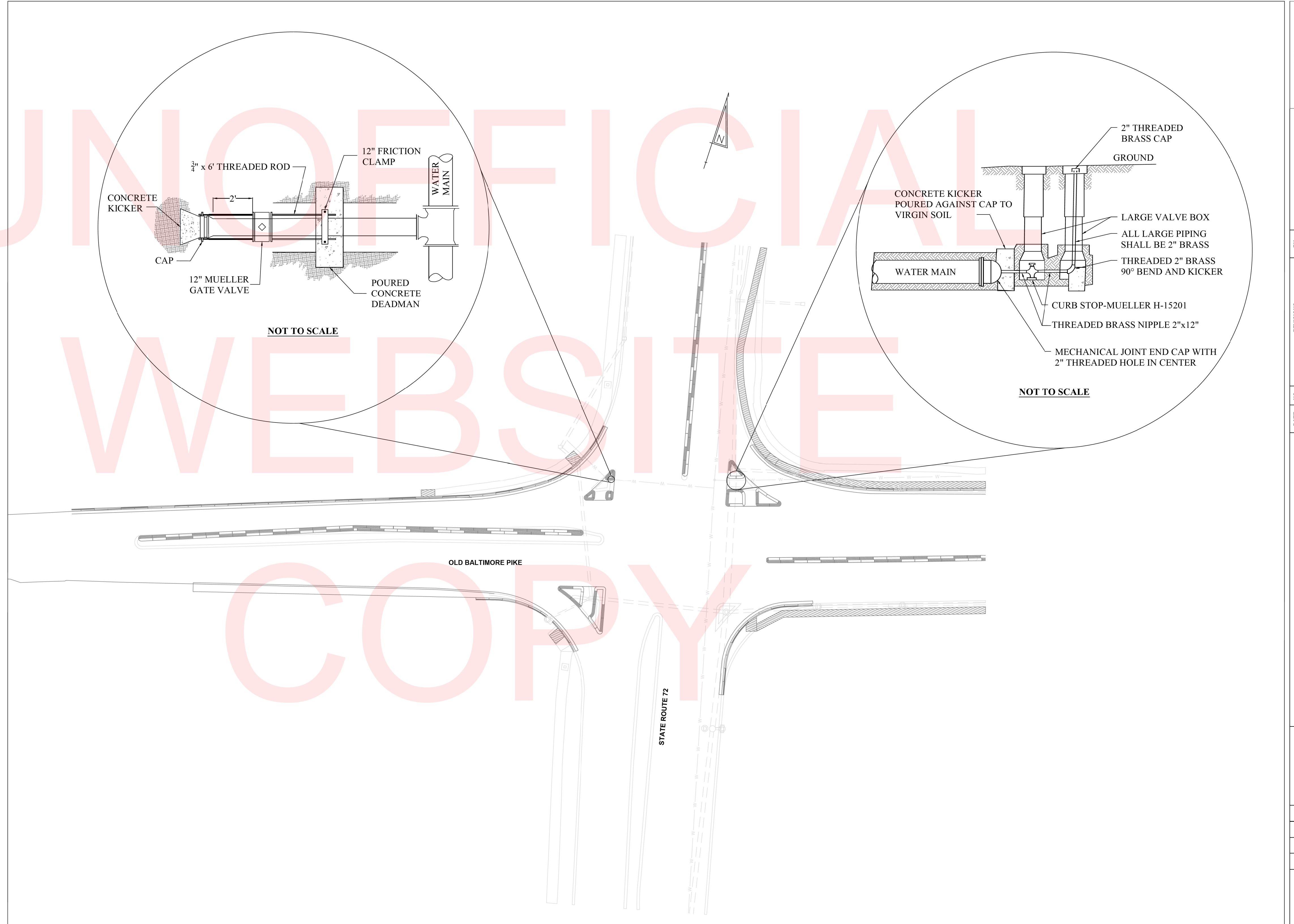
NOTES:
 1) COSTS ASSOCIATED WITH SUPPLY AND INSTALLATION OF TEMPORARY CURB RAMP IS TO BE PAID UNDER ITEM 705528.
 2) PRE-MANUFACTURED RAMP SYSTEMS MAY BE USED IF THEY MEET ALL MINIMUM DIMENSIONS DEPICTED IN THIS DETAIL AND COMPLY WITH THE DELAWARE MUTCD AND ADA GUIDELINES.

TEMPORARY CURB RAMP DETAIL
 NOT TO SCALE

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CITY OF NEWARK WATER - CAPPING NOTES

- PIPE CAPPING SHALL BE PAID AS A LUMP SUM COST UNDER ITEM 614508 - WATER MAIN AND ACCESSORIES.
- WATER LINE SHUT DOWN RESTRICTED TO MONDAY THROUGH THURSDAY, 8:00PM TO 5:00AM.
- THE CONTRACTOR SHALL CONTACT THE WATER/SEWER INSPECTOR FOR THE CITY OF NEWARK, DELAWARE, AT (302) 366-7000 TO COORDINATE INSPECTION. NO TESTING WILL BE REQUIRED.
- ALL MATERIAL MUST BE AMERICAN MADE.
ADDITIONAL MATERIAL INFORMATION:
12 INCH MUELLER GATE, MODEL NO. A-2360, OPEN LEFT
DUCTILE IRON CLASS #52 PIPE
CAST IRON VALVE BOXES, THREAD TYPE



NEWARK DELAWARE
Committed to Service Excellence

CITY OF NEWARK
PUBLIC WORKS AND WATER
RESOURCES DEPARTMENT
230 SOUTH MAIN STREET
NEWARK, DELAWARE 19711

BY	REVISIONS	NO.	DATE

STATE ROAD IMPROVEMENTS
STATE ROUTE 72 AND OLD BALTIMORE PIKE INTERSECTION
NEWARK, DELAWARE
CITY OF NEWARK WATER MAIN
IMPROVEMENTS

DATE: 3-18-2016
DRAWING SCALE: NTS
DRAWN BY: B.J.L.
APPROVED BY: P.B.

SHEET 1 OF 1

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<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>	ADDENDUMS / REVISIONS	<p>NOT TO SCALE</p>	<p>HEP NCC, SR 72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS</p>	CONTRACT	BRIDGE NO.	NA	<p>CITY OF NEWARK WATER PIPE CAPPING DETAILS</p>	SHEET NO.
	T201200108			DESIGNED BY: BCD	24			
	COUNTY			CHECKED BY: MCN	TOTAL SHTS.			
	NEW CASTLE				52			

CONSTRUCTION PHASING & M.O.T	
	BARRICADE, TYPE 3
	CONCRETE SAFETY BARRIER - PORTABLE
	CONSTRUCTION SAFETY FENCE / LENGTH
	CONSTRUCTION SAFETY FENCE
	PEDESTRIAN CHANNELIZING BARRIER
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	TEMPORARY CONSTRUCTION
	TEMPORARY PAVEMENT MARKING ARROW
	TRUCK WITH MOUNTED ATTENUATOR
	WORK AREA - ACTIVE PHASE

WARNING SIGN LEGEND			
W20-1 48" X 48"	W20-1 48" X 48"	W20-1 48" X 48"	W11-1 48" X 48"
①	②	③	④
G20-2 48" X 24"	R2-10 36" X 48"	R2-11 36" X 48"	
⑤	⑥	⑦	

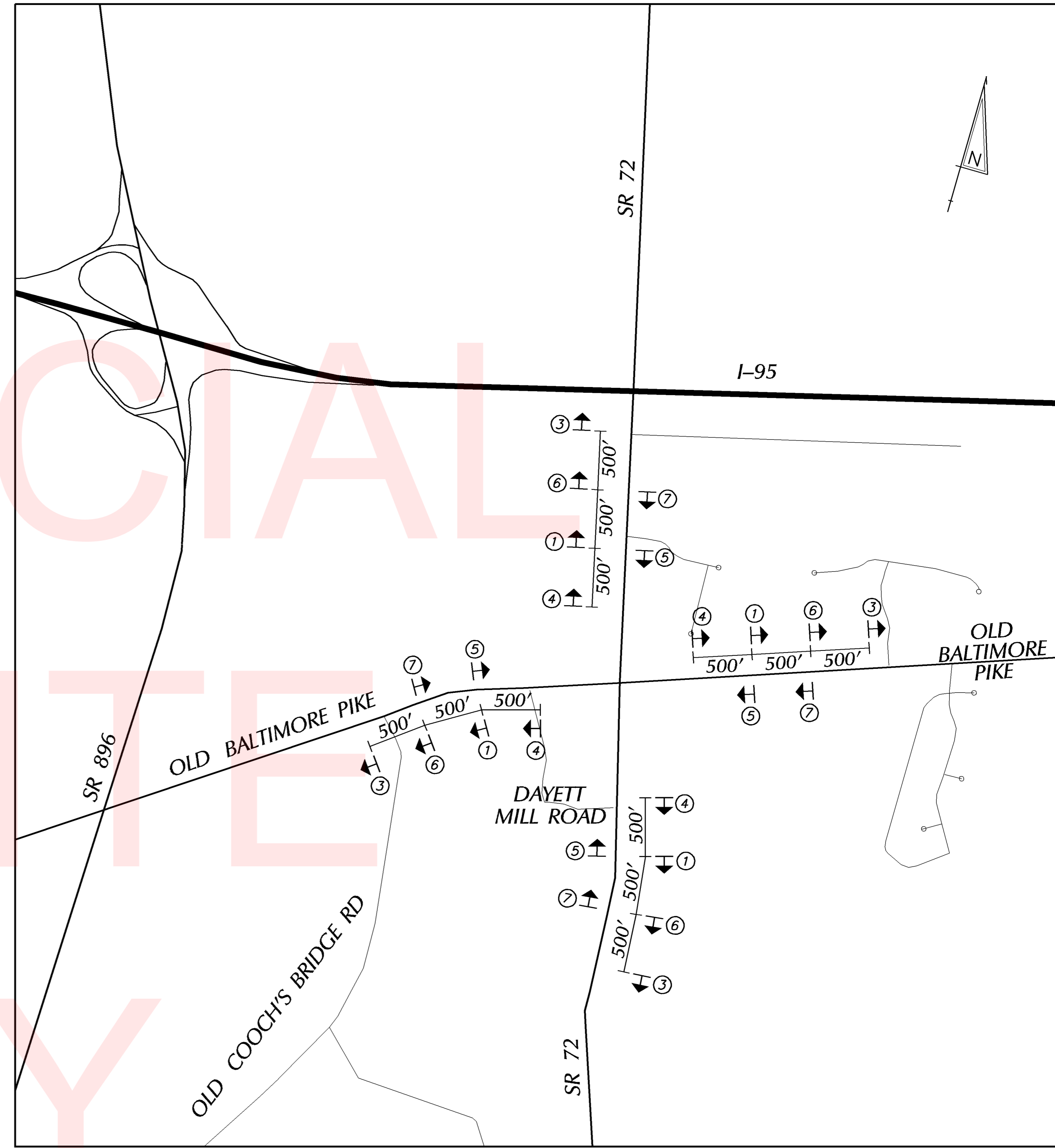
EROSION & SEDIMENT CONTROL	
	DEWATERING BAG
	DEWATERING BASIN
	EARTH DIKE
	INLET SEDIMENT CONTROL
	PERIMETER DIKE/SWALE
	PORTABLE SEDIMENT TANK
	SANDBAG DIKE
	SANDBAG DIVERSION
	STONE CHECK DAM
	STABILIZED CONSTRUCTION ENTRANCE
	SILT FENCE / LENGTH
	SILT FENCE
	SILT FENCE - REINFORCED
	COMPOST FILTER LOG
	SUMP PIT, TYPE 1
	SUMP PIT, TYPE 2
	SEDIMENT TRAP
	SEDIMENT TRAP WITH INLET AS OUTLET
	SEDIMENT TRAP PIPE OUTLET
	STILLING WELL
	TEMPORARY SWALE
	TEMPORARY SLOPE DRAIN
	TURBIDITY CURTAIN / LENGTH
	TURBIDITY CURTAIN

- THE USE OF MILLINGS AND GABC IN THE TRAVEL WAY, TEMPORARY TRAVEL WAY, HIGH VOLUME ENTRANCES AND ACCESS RAMP FOR THE PURPOSE OF PROVIDING A TEMPORARY ROADWAY SURFACE, POT HOLE REPAIR, TAPERED EDGE FOR UTILITIES, BUTT JOINTS, AND LONGITUDINAL DROP-OFFS (MILLING AND PAVING OPERATIONS) IS PROHIBITED UNLESS IT IS OTHERWISE DESIGNATED TO BE USED IN THE CONTRACT PLANS. USE COLD PATCH, BITUMINOUS CONCRETE, BITUMINOUS CONCRETE WEDGE, OR TAPER MILL, AS NOTED IN THE CONTRACT DOCUMENTS OR APPROVED BY THE ENGINEER. PAYMENT FOR COLD PATCH, BITUMINOUS CONCRETE, OR BITUMINOUS CONCRETE WEDGE SHALL BE PAID AS NOTED IN THE CONTRACT DOCUMENTS. TAPER MILL BITUMINOUS CONCRETE SHALL BE PAID UNDER THE BITUMINOUS CONCRETE MILLING ITEM.

MILLINGS OR GABC SHALL BE USED AT THE FOLLOWING LOCATIONS WHERE ACCESS TO A BUSINESS, RESIDENCE, OR EDGE DROP OFF NEEDS TO BE MAINTAINED UNLESS OTHERWISE NOTED IN THE PLANS OR DIRECTED BY THE ENGINEER TO USE BITUMINOUS CONCRETE OR COLD PATCH. ALL MILLINGS AND GABC WILL BE ROLLED AND COMPACTED TO HELP PREVENT THE MATERIAL FROM UNRAVELLING:
A. DRIVEWAYS
B. ENTRANCES
C. LOW VOLUME ACCESS RAMPS (IDENTIFIED IN THE CONTRACT DOCUMENTS)
D. EDGE DROP-OFFS ADJACENT TO LIVE ROADWAY(LANE, SHOULDER, OR TURN LANE) AND THE PROPOSED ROAD CONSTRUCTION
E. EDGE OF ROADWAY DROPOFF

GRADING AND MAINTAINING BASE COURSE THAT IS BEING USED FOR ROADWAY WEDGE/FILLET BETWEEN TRAVEL LANES AND PAVEMENT BOX, EDGE OF TRAVELWAY, DRIVEWAY OR ENTRANCE ACCESS SHALL BE INCIDENTAL TO ITEM NO. 743000 - MAINTENANCE OF TRAFFIC. THE BASE COURSE MATERIAL SHALL BE PLACED AT NO GREATER THAN THE SLOPE SPECIFIED IN TABLE 6G-1 AND SHALL BE COMPACTED. EXCESS BASE COURSE MATERIAL SHALL BE PUSHED AHEAD AND USED IN THE NEXT SEGMENT AND SHALL BE INCIDENTAL TO THE PARTICULAR BASE COURSE PAY ITEM. NO SEPARATE PAYMENT SHALL BE MADE FOR MILLINGS OR GABC TEMPORARY ROADWAY MATERIAL (TRM) USED TO PROTECT EDGE DROP-OFFS, UNLESS THE MATERIAL IS EVENTUALLY UTILIZED AS PART OF A PERMANENT ROADWAY AT WHICH TIME THE MATERIAL WOULD BE PAID FOR UNDER THE RESPECTIVE CONTRACT MATERIAL ITEM.

VERTICAL DIFFERENCES SHALL BE CORRECTED IN ACCORDANCE WITH TABLE 6G-1 OF THE DELAWARE MUTCD.
- ACCEPTABLE MATERIALS FOR TEMPORARY PEDESTRIAN PATHS SHALL INCLUDE CONCRETE, HOT-MIX, COMPACTED MILLINGS OR PLYWOOD WALKWAY STRUCTURE. PLYWOOD WALKWAY STRUCTURES SHALL ALSO INCLUDE DETECTABLE EDGING AND RAILINGS IN ACCORDANCE WITH ADA GUIDELINES AND THE DELAWARE MUTCD. TEMPORARY PEDESTRIAN PATHS SHALL BE INCIDENTAL TO ITEM 743000 UNLESS OTHERWISE SPECIFIED IN THE PLANS. TEMPORARY PEDESTRIAN PATHS SHALL BE A MINIMUM OF 5 FEET IN WIDTH. STONE OR GRADED AGGREGATE BASE COURSE SHALL NOT BE USED FOR TEMPORARY PEDESTRIAN PATHS.
- MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS SHALL CONFORM TO TYPICAL APPLICATION TA-33, TA-10 AND TA-3A OF THE DELAWARE MUTCD.
- PEDESTRIAN FLAGGERS SHALL BE PAID UNDER ITEM 743050
- TRAFFIC OFFICERS SHALL BE ON SITE DURING WORKING HOURS FOR THE FIRST TWO WORK NIGHTS FOLLOWING THE CLOSURE OF THE LEFT TURN LANE INTO ROYAL FARMS IN PHASE 5A. AFTER THE FIRST 2 NIGHTS, ADDITIONAL TRAFFIC OFFICER HOURS SHALL BE COORDINATED WITH THE ENGINEER AND DELDOT'S TRAFFIC SAFETY SECTION.

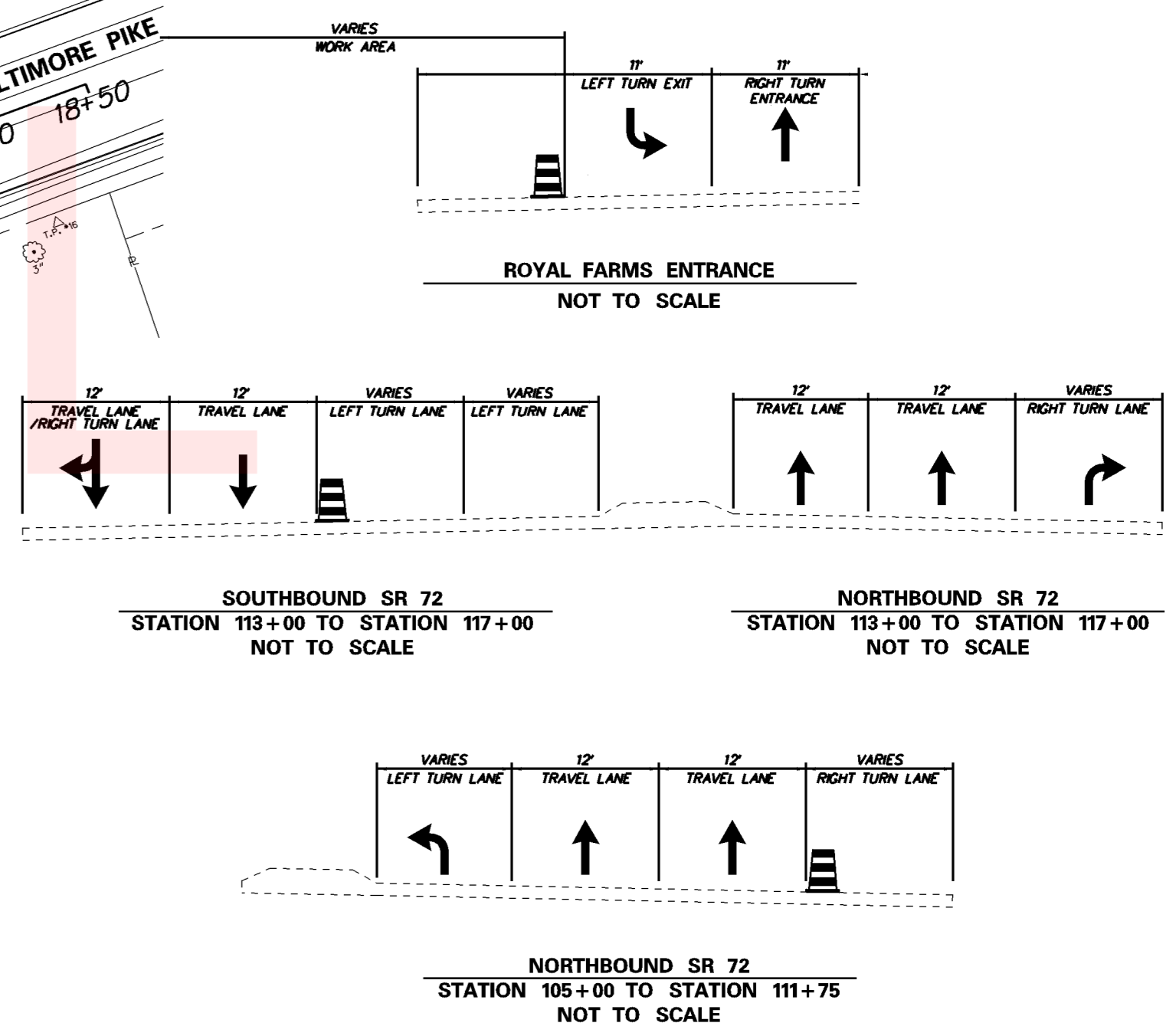
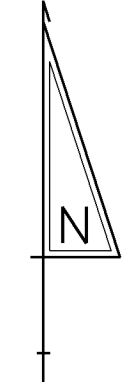
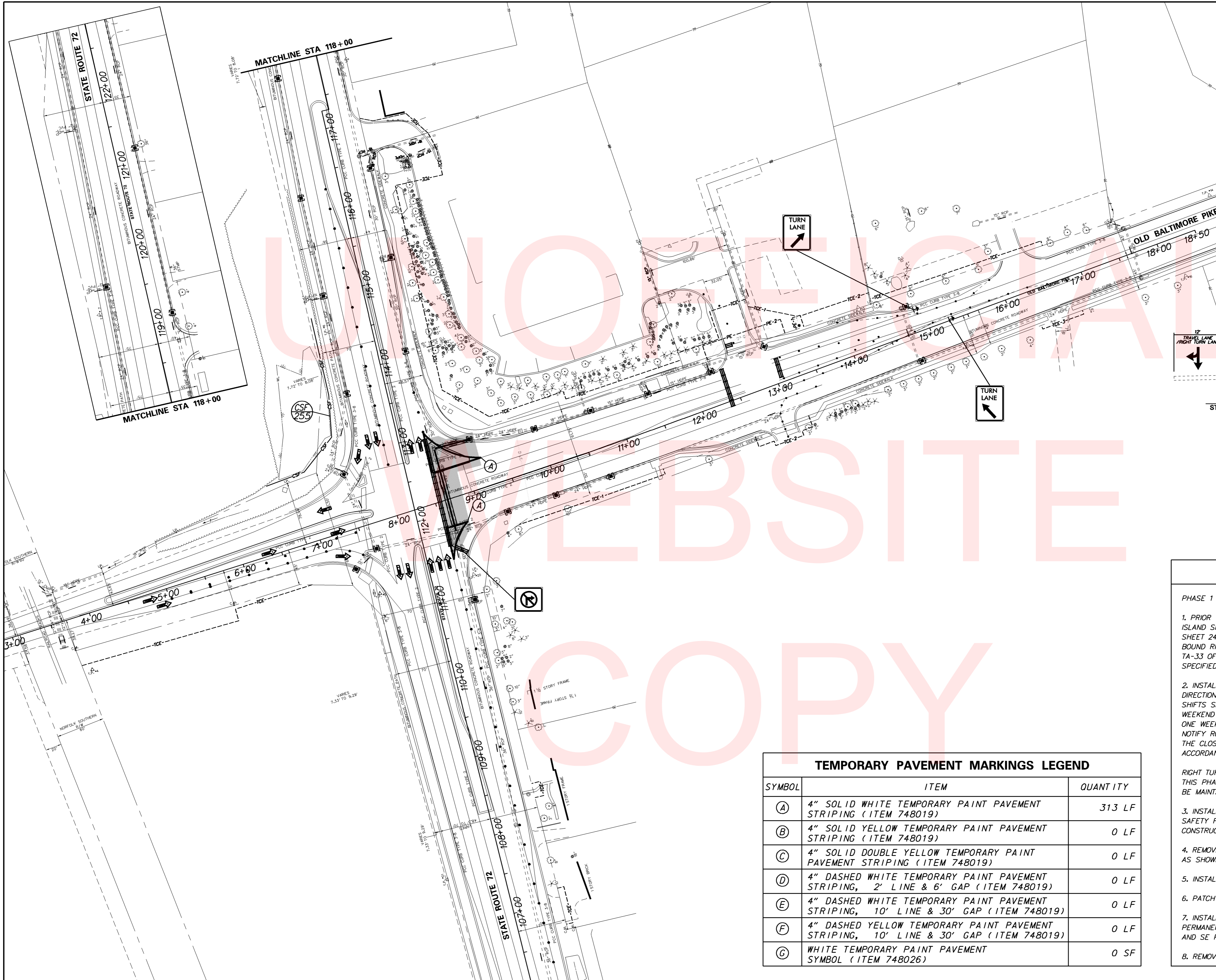


ALLOWABLE TRAVEL LANE CLOSURE TIMES																									
	12:00 AM	1:00 AM	2:00 AM	3:00 AM	4:00 AM	5:00 AM	6:00 AM	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM	8:00 PM	9:00 PM	10:00 PM	11:00 PM	
SUNDAY																									
MONDAY																									
TUESDAY																									
WEDNESDAY																									
THURSDAY																									
FRIDAY																									
SATURDAY																									

TRAVEL LANE CLOSURES ALLOWED
 NO TRAVEL LANE CLOSURES ALLOWED

*NOTE: THE ABOVE ALLOWABLE LANE CLOSURE TIMES APPLY TO ALL THROUGH LANES, AUXILIARY LANES AND TURN LANES ON SR 72 AND OLD BALTIMORE PIKE WITHIN THE PROJECT LIMITS

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TEMPORARY PAVEMENT MARKINGS LEGEND

SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	313 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	0 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	0 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	0 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	0 SF

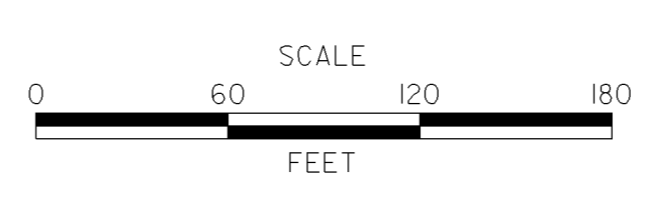
SEQUENCE OF CONSTRUCTION

PHASE 1

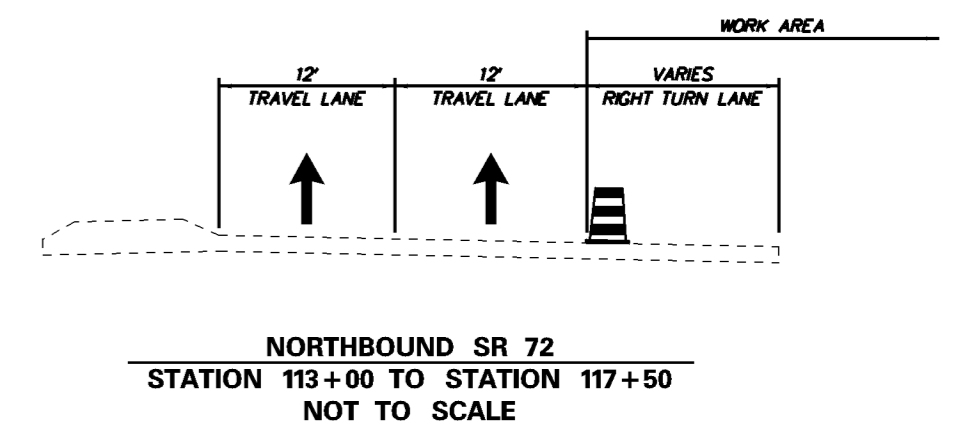
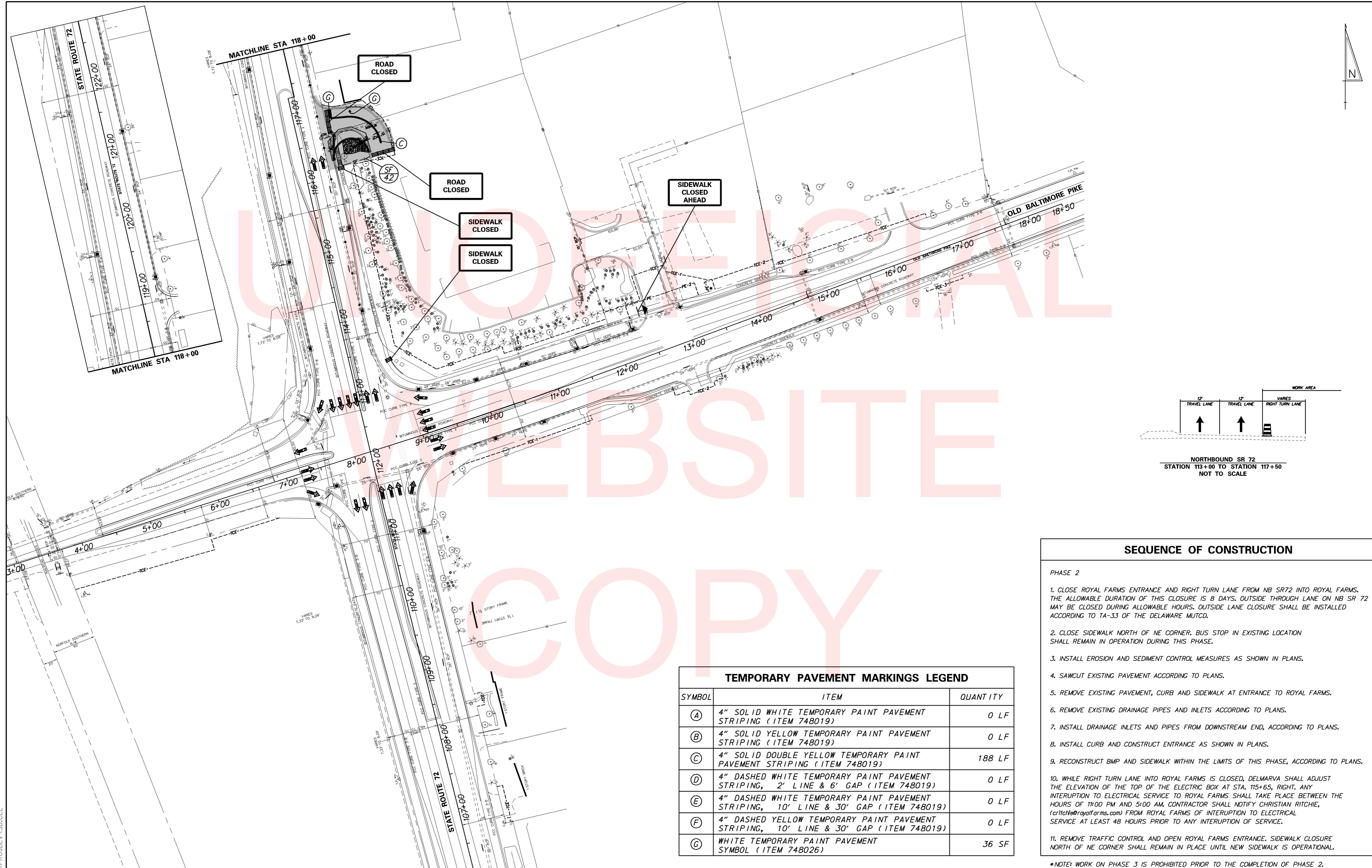
- PRIOR TO PHASE 1, THE CITY OF NEWARK WATER LINE IN THE NORTHEAST CHANNELIZING ISLAND SHALL BE CAPPED. CAPPING SHALL BE CONSTRUCTED ACCORDING TO DETAIL ON SHEET 24. MAINTENANCE OF TRAFFIC IN OUTSIDE LANE OF NORTHBOUND SR 72 AND WEST BOUND RIGHT TURN LANE ON OLD BALTIMORE PIKE SHALL BE INSTALLED ACCORDING TO TA-33 OF THE DELAWARE MUTCD. THIS WORK SHALL BE COMPLETED DURING THE HOURS SPECIFIED IN THE CITY OF NEWARK WATER PIPE CAPPING DETAILS.
- INSTALL DETOUR TO CLOSE EAST LEG OF OLD BALTIMORE PIKE IN EB AND WB DIRECTIONS. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS SHALL BE INSTALLED ACCORDING TO DETOUR PLANS (SHEET 3B). THE ALLOWABLE WEEKEND CLOSURE HOURS ARE FROM 7:00PM FRIDAY THROUGH 6:00AM MONDAY. THERE IS ONE WEEKEND CLOSURE ALLOWED FOR THE WORK IN THIS PHASE. THE CONTRACTOR SHALL NOTIFY ROYAL FARMS AND WORD OF LIFE CHRISTIAN CENTER AT LEAST 7 DAYS BEFORE THE CLOSURE IS INSTALLED. ADDITIONAL LANE CLOSURES ON SR 72, MUST BE IN ACCORDANCE WITH THE ALLOWABLE CLOSURE HOURS.
- RIGHT TURNS IN AND LEFT TURNS OUT OF ROYAL FARMS SHALL BE MAINTAINED THROUGHOUT THIS PHASE. LEFT TURNS IN AND RIGHT TURNS OUT OF WORD OF LIFE CHRISTIAN CENTER SHALL BE MAINTAINED THROUGHOUT THIS PHASE.
- INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN. INSTALL CONSTRUCTION SAFETY FENCE AROUND WETLAND AT NW CORNER OF THE INTERSECTION, ACCORDING TO PLANS. CONSTRUCTION SAFETY FENCE SHALL REMAIN IN PLACE THROUGH THE DURATION OF THE PROJECT.
- REMOVE CROSS ROAD DRAINAGE PIPES AND EXISTING NE AND SE PORKCHOP ISLANDS, AS SHOWN ON PLANS.
- INSTALL CROSS-ROAD DRAINAGE PIPES, STARTING AT THE DOWNSTREAM END.
- PATCH PIPE TRENCHES ACCORDING TO PLANS.
- INSTALL TEMPORARY WARM MIX PATCHING IN NE PORKCHOP ISLAND AREA AND PERMANENT PATCHING IN SE PORKCHOP ISLAND AREA. ADJUST MANHOLE LIDS IN NE AND SE PORKCHOPS TO PATCHING SURFACE.
- REMOVE TRAFFIC CONTROL.

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ADDENDUMS / REVISIONS



CONTRACT	BRIDGE NO.	NA
T201200108	DESIGNED BY:	BCD
COUNTY	CHECKED BY:	MCN
NEW CASTLE		



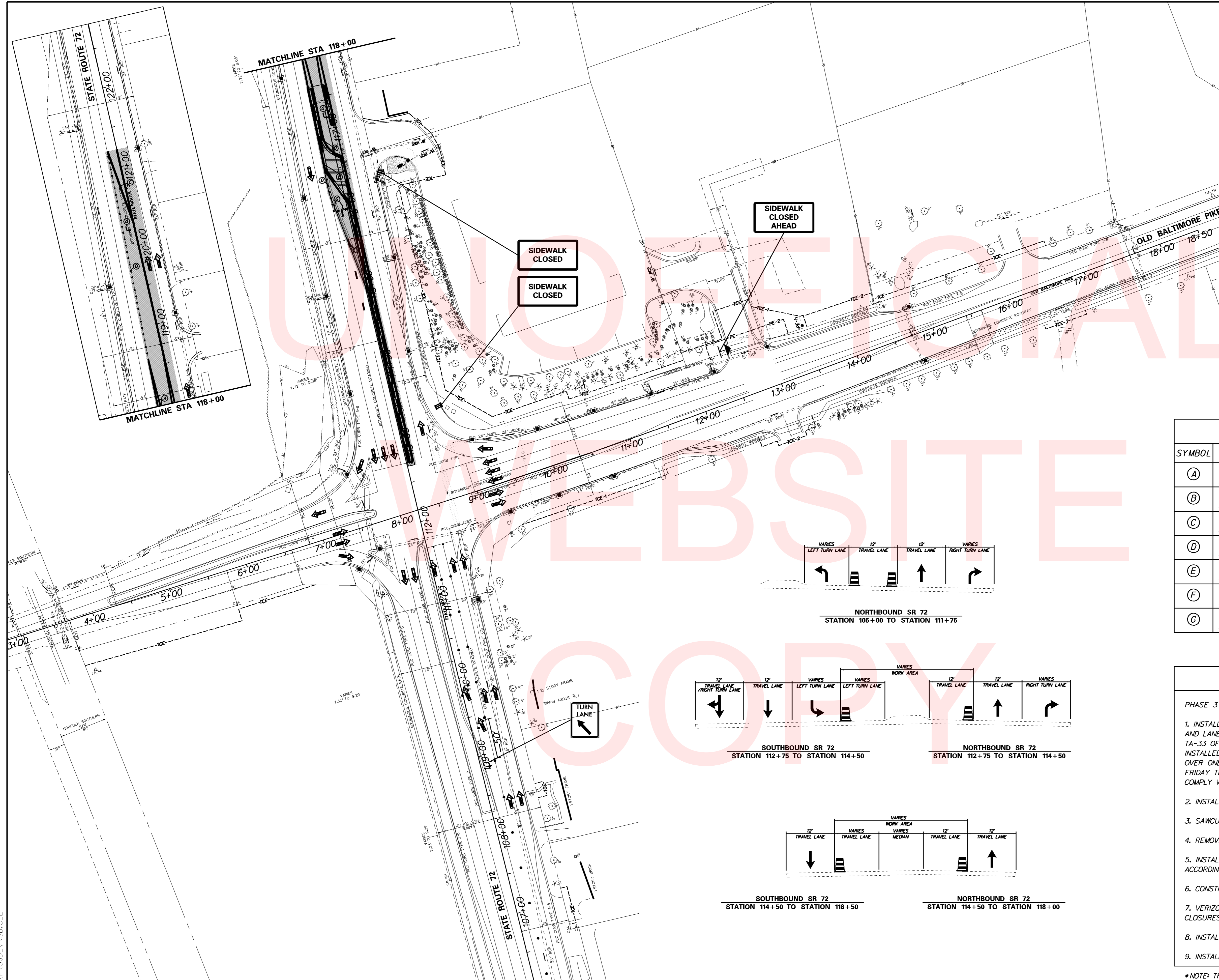
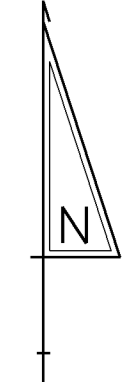
NORTHBOUND SR 72
STATION 113+00 TO STATION 117+50
NOT TO SCALE

TEMPORARY PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	0 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	0 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	188 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	0 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	36 SF

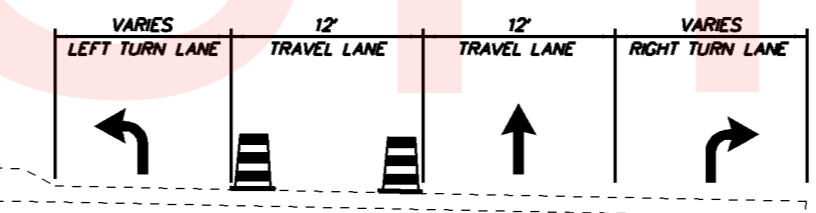
- SEQUENCE OF CONSTRUCTION**
- PHASE 2
- CLOSE ROYAL FARMS ENTRANCE AND RIGHT TURN LANE FROM NB SR72 INTO ROYAL FARMS. THE ALLOWABLE DURATION OF THIS CLOSURE IS 8 DAYS. OUTSIDE THROUGH LANE ON NB SR 72 MAY BE CLOSED DURING ALLOWABLE HOURS. OUTSIDE LANE CLOSURE SHALL BE INSTALLED ACCORDING TO TA-33 OF THE DELAWARE MUTCD.
 - CLOSE SIDEWALK NORTH OF NE CORNER. BUS STOP IN EXISTING LOCATION SHALL REMAIN IN OPERATION DURING THIS PHASE.
 - INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN IN PLANS.
 - SAWCUT EXISTING PAVEMENT ACCORDING TO PLANS.
 - REMOVE EXISTING PAVEMENT, CURB AND SIDEWALK AT ENTRANCE TO ROYAL FARMS.
 - REMOVE EXISTING DRAINAGE PIPES AND INLETS ACCORDING TO PLANS.
 - INSTALL DRAINAGE INLETS AND PIPES FROM DOWNSTREAM END, ACCORDING TO PLANS.
 - INSTALL CURB AND CONSTRUCT ENTRANCE AS SHOWN IN PLANS.
 - RECONSTRUCT BMP AND SIDEWALK WITHIN THE LIMITS OF THIS PHASE, ACCORDING TO PLANS.
 - WHILE RIGHT TURN LANE INTO ROYAL FARMS IS CLOSED, DELMARVA SHALL ADJUST THE ELEVATION OF THE TOP OF THE ELECTRIC BOX AT STA. 115+65, RIGHT. ANY INTERRUPTION TO ELECTRICAL SERVICE TO ROYAL FARMS SHALL TAKE PLACE BETWEEN THE HOURS OF 1:00 PM AND 5:00 AM. CONTRACTOR SHALL NOTIFY CHRISTIAN RITCHIE, (critch@royalfarms.com) FROM ROYAL FARMS OF INTERRUPTION TO ELECTRICAL SERVICE AT LEAST 48 HOURS PRIOR TO ANY INTERRUPTION OF SERVICE.
 - REMOVE TRAFFIC CONTROL AND OPEN ROYAL FARMS ENTRANCE. SIDEWALK CLOSURE NORTH OF NE CORNER SHALL REMAIN IN PLACE UNTIL NEW SIDEWALK IS OPERATIONAL.

*NOTE: WORK ON PHASE 3 IS PROHIBITED PRIOR TO THE COMPLETION OF PHASE 2.

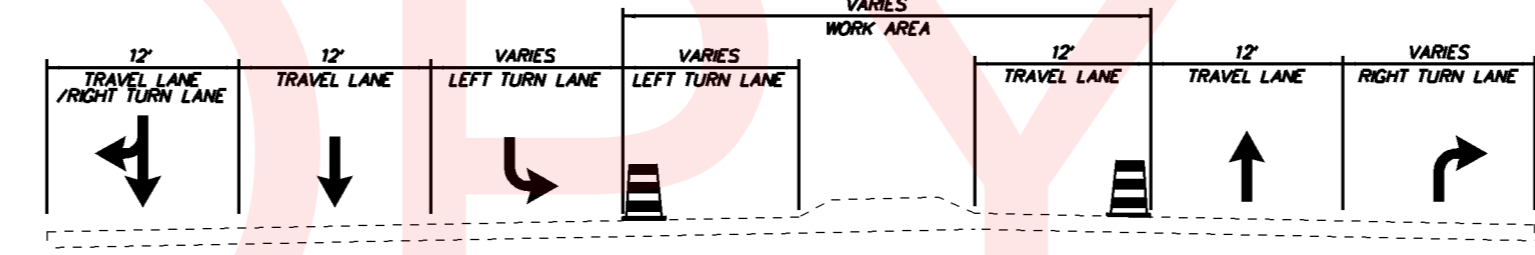
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TEMPORARY PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	291 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	874 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	1, 878 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	78 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	40 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	62 SF

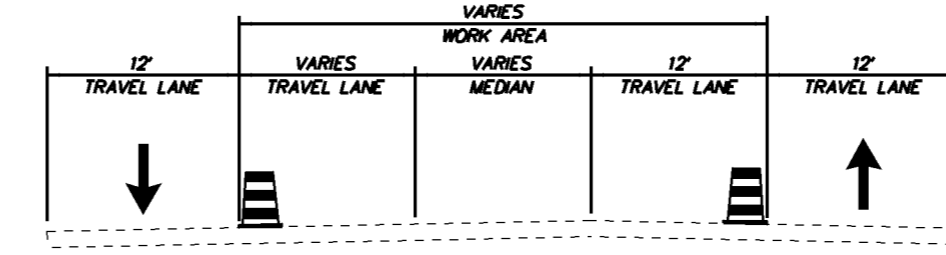


NORTHBOUND SR 72
STATION 105+00 TO STATION 111+75



SOUTHBOUND SR 72
STATION 112+75 TO STATION 114+50

NORTHBOUND SR 72
STATION 112+75 TO STATION 114+50



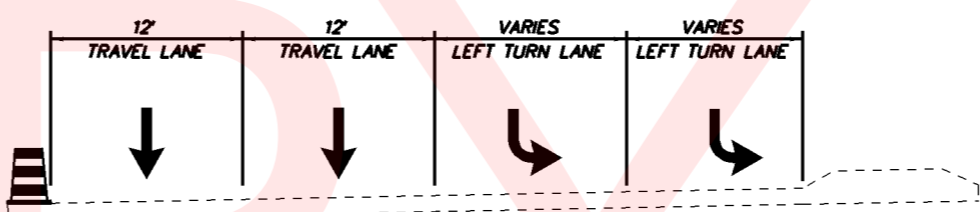
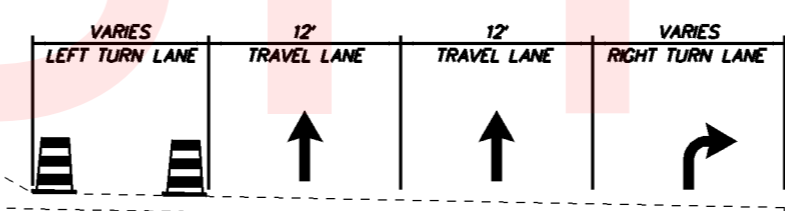
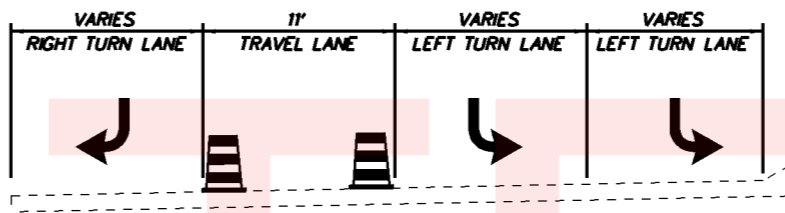
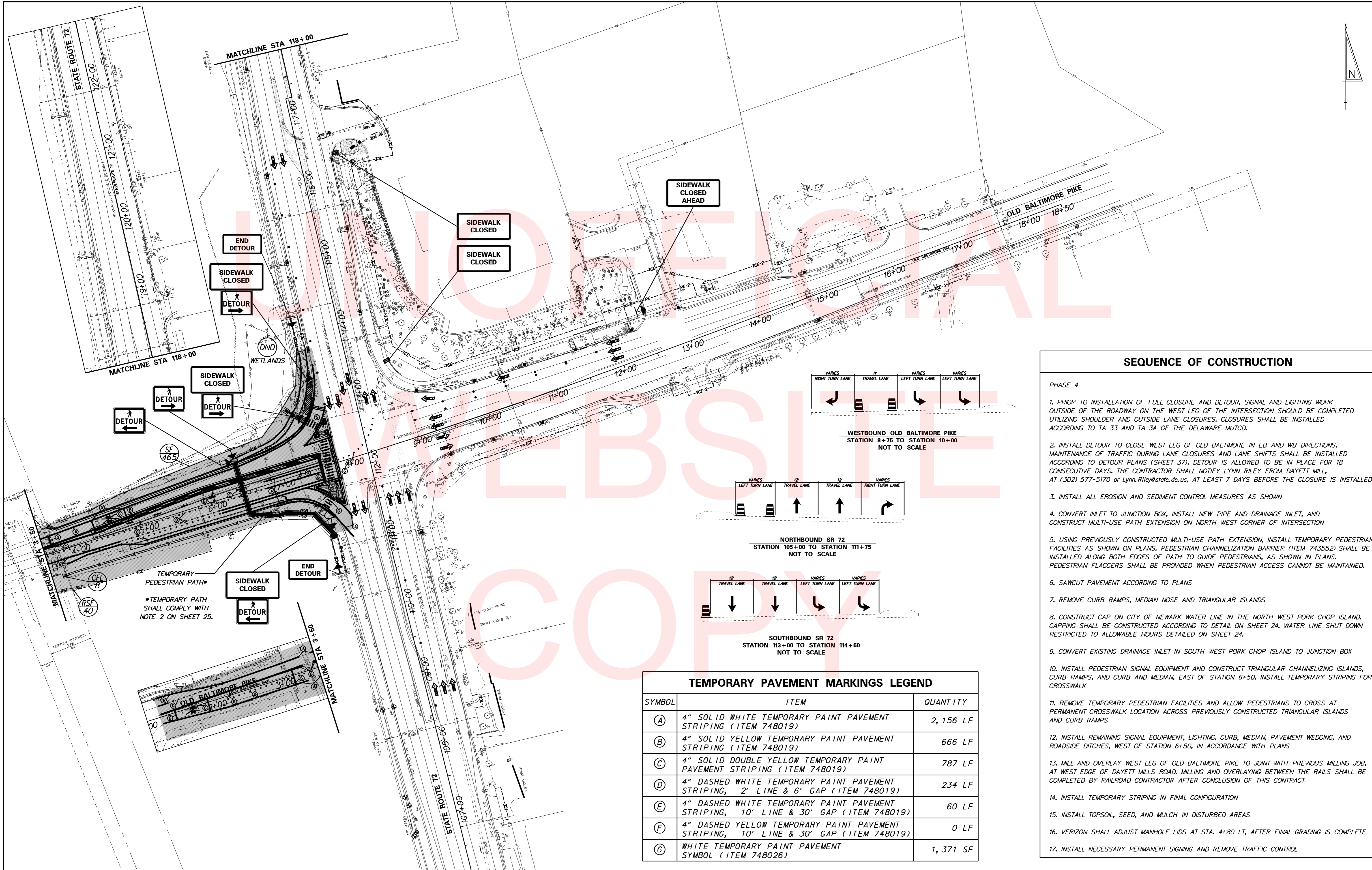
SOUTHBOUND SR 72
STATION 114+50 TO STATION 118+50

NORTHBOUND SR 72
STATION 114+50 TO STATION 118+50

- SEQUENCE OF CONSTRUCTION**
- PHASE 3
1. INSTALL TRAFFIC CONTROL. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS ON NB & SB SR 72 SHALL CONFORM TO TYPICAL APPLICATION TA-33 OF THE DELAWARE MUTCD. LEFT TURN LANE ON NB SR 72 SHALL BE INSTALLED ACCORDING TO PLANS. THE CLOSURES IN THIS PHASE MAY BE IN PLACE OVER ONE WEEKEND. THE ALLOWABLE WEEKEND CLOSURE HOURS ARE FROM 7:00PM FRIDAY THROUGH 6:00AM MONDAY. ADDITIONAL LANE CLOSURES SHALL COMPLY WITH HOURS IN TABLE ON SHEET 25.
 2. INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN
 3. SAWCUT PAVEMENT ACCORDING TO PLANS
 4. REMOVE EXISTING PAVEMENT AND MEDIAN AS SHOWN IN PLANS
 5. INSTALL SIGNAL AND LIGHTING FACILITIES IN THE MEDIAN AREA, ACCORDING TO PLANS
 6. CONSTRUCT NEW MEDIAN AND PAVEMENT ACCORDING TO PLANS
 7. VERIZON SHALL ADJUST MANHOLES TO FINAL PAVEMENT ELEVATION. LANE CLOSURES SHALL COMPLY WITH ALLOWABLE HOURS IN TABLE ON SHEET 25.
 8. INSTALL TEMPORARY STRIPING FOR FINAL LANE CONFIGURATIONS
 9. INSTALL NECESSARY PERMANENT SIGNING AND REMOVE TRAFFIC CONTROL

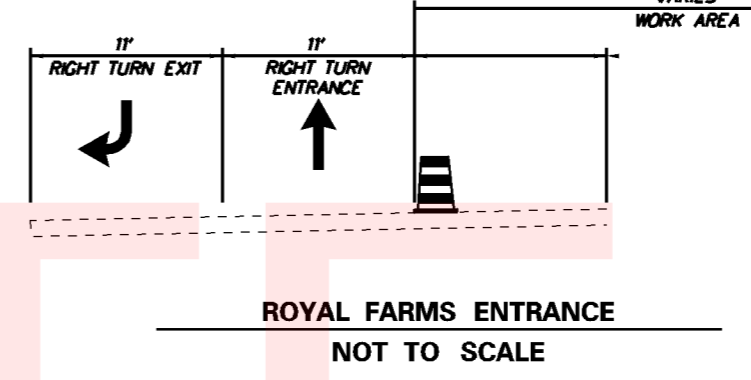
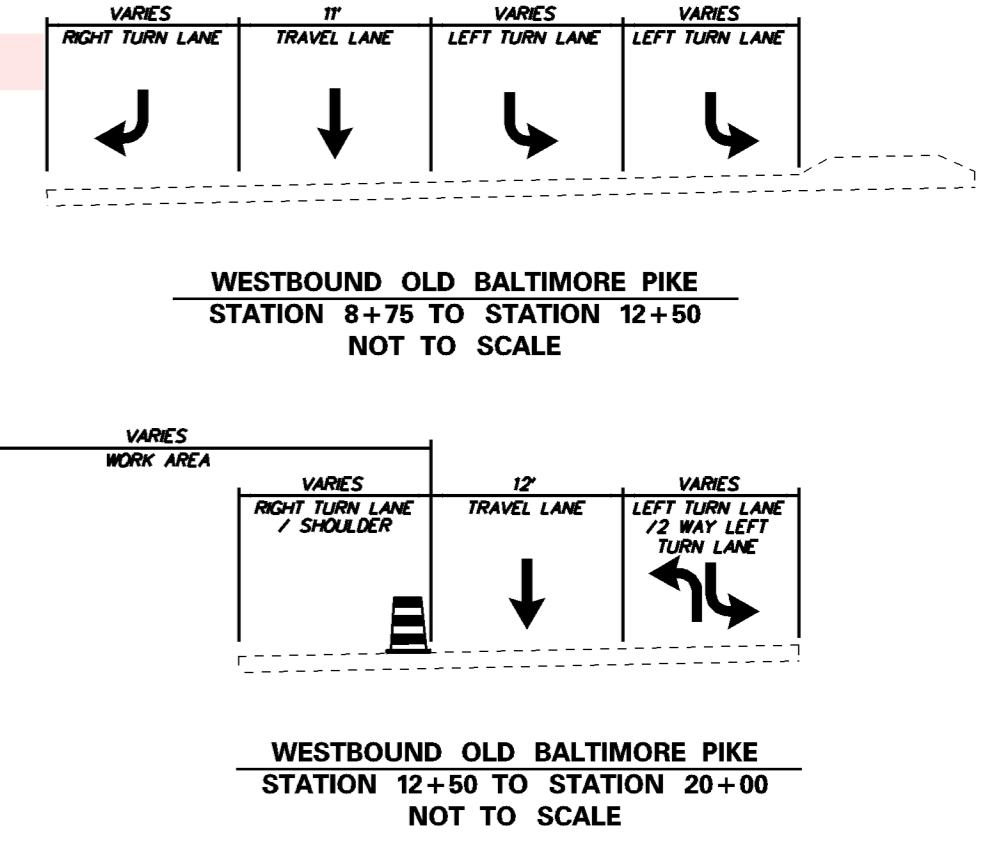
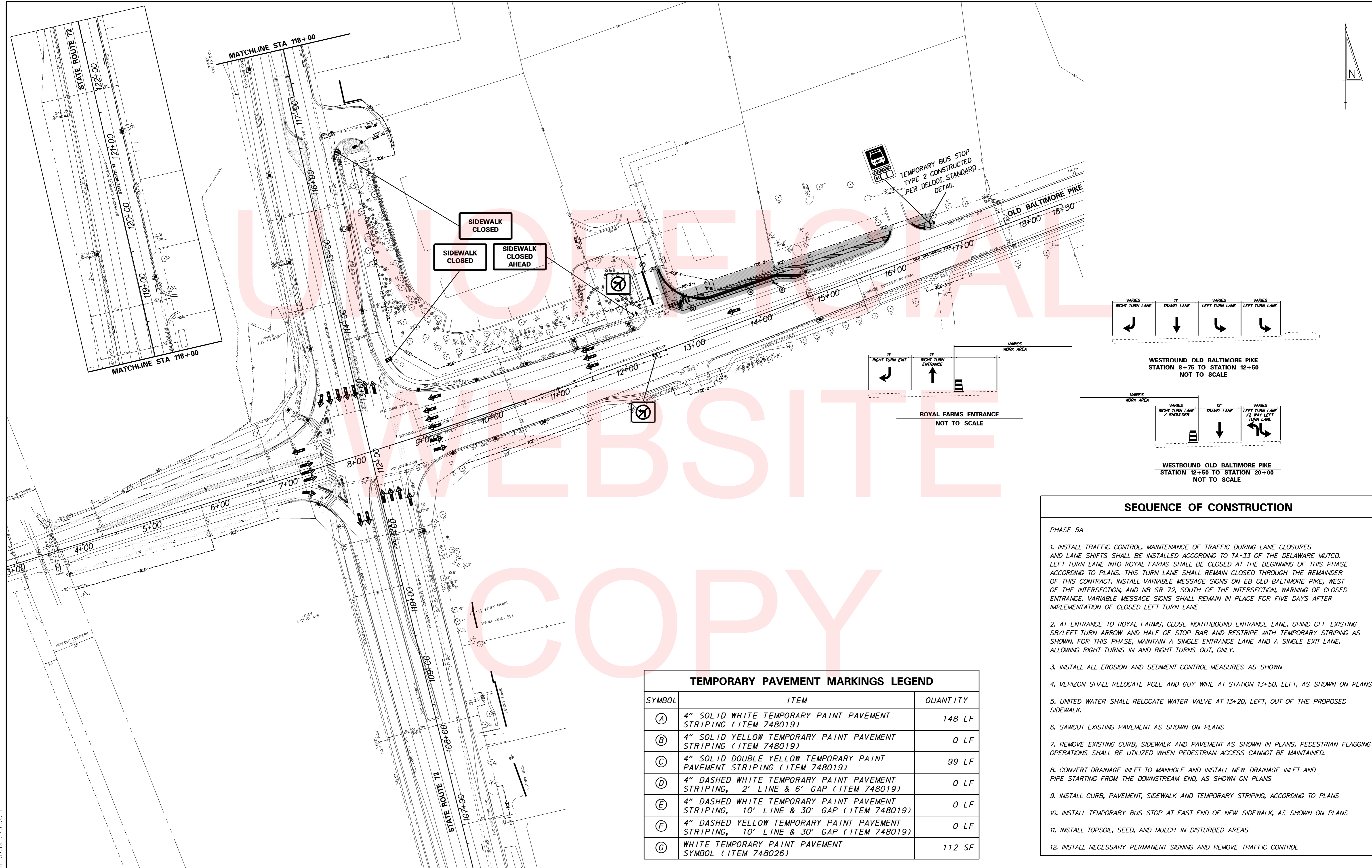
*NOTE: THE WORK IN PHASE 2 MUST BE COMPLETED PRIOR TO THE START OF PHASE 3.

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TEMPORARY PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	2,156 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	666 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	787 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	234 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	60 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	1,371 SF

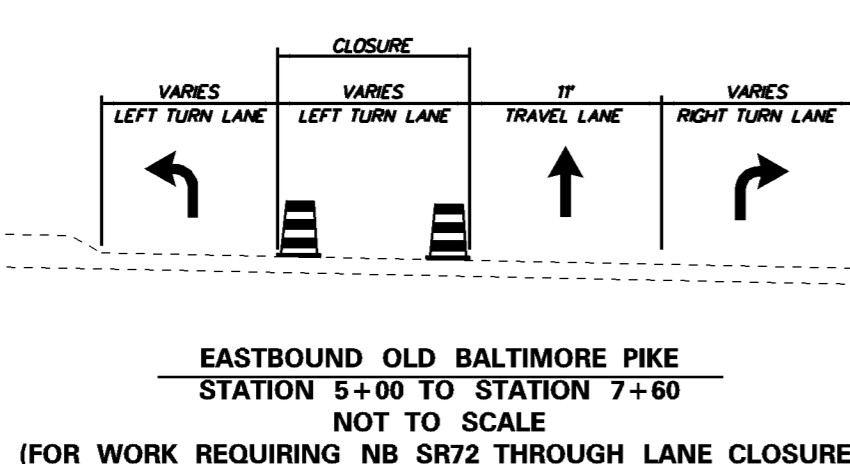
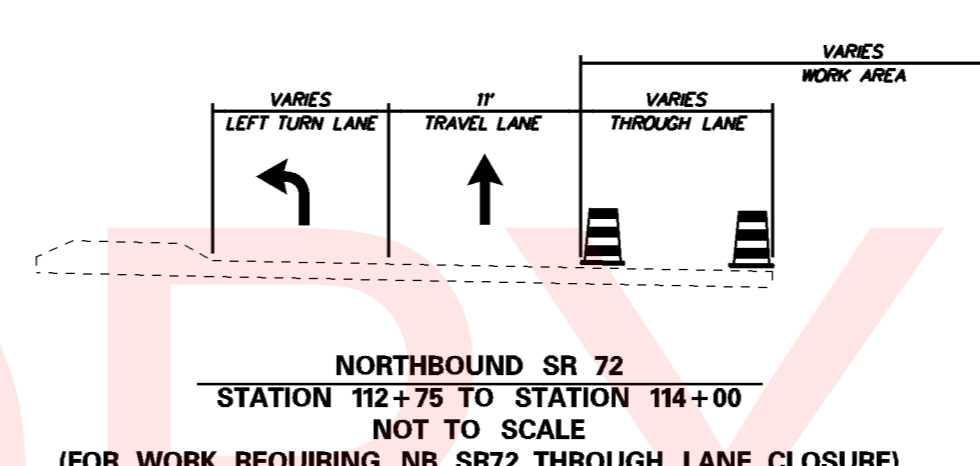
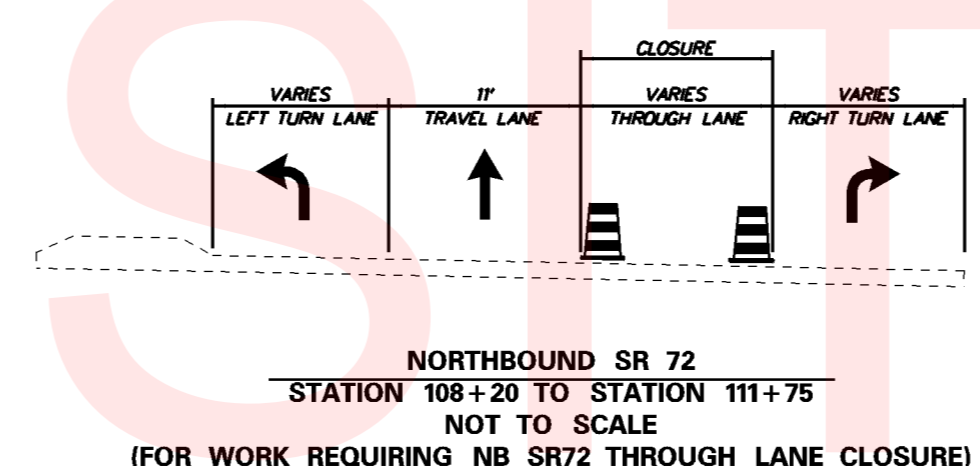
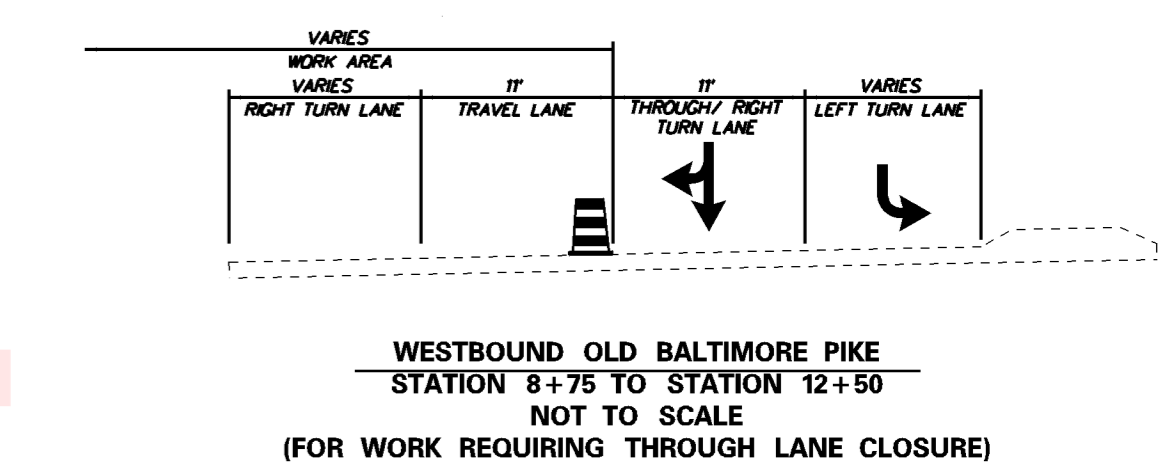
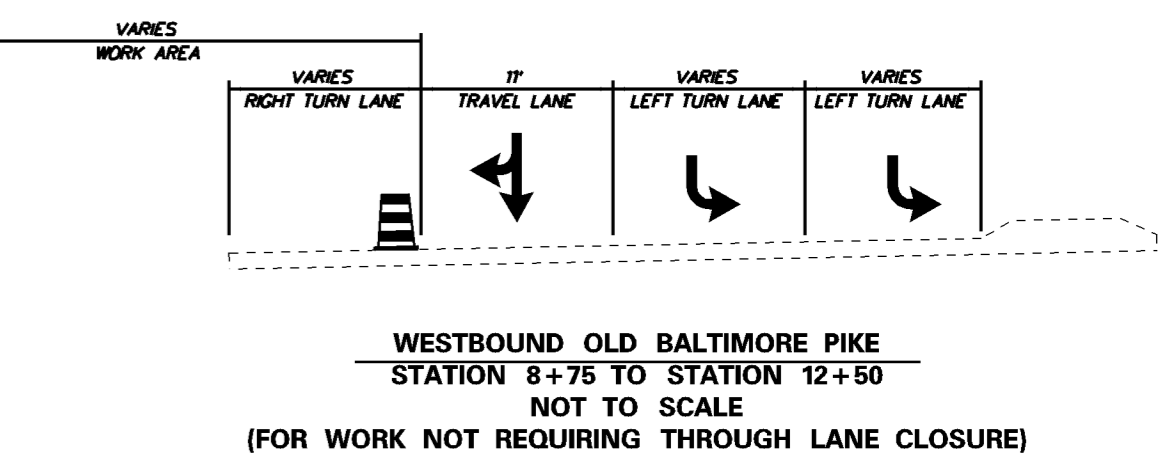
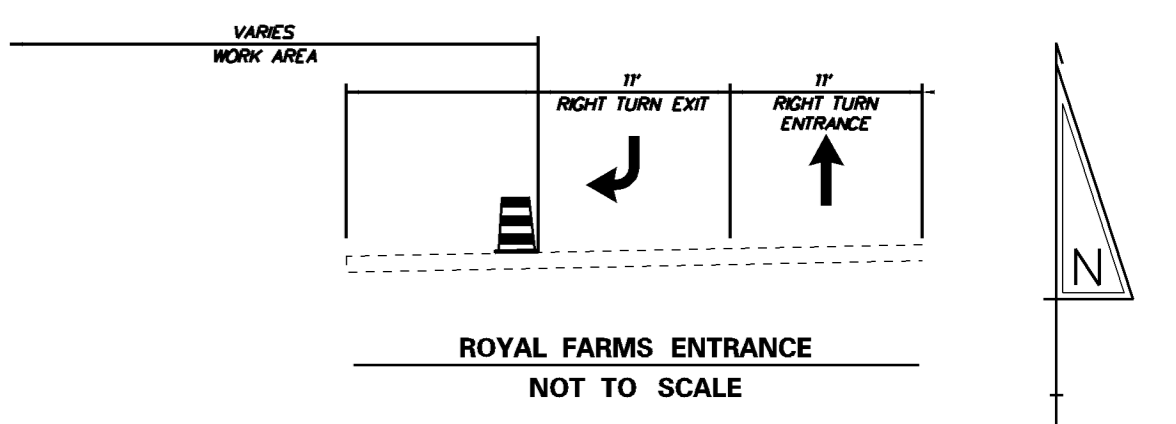
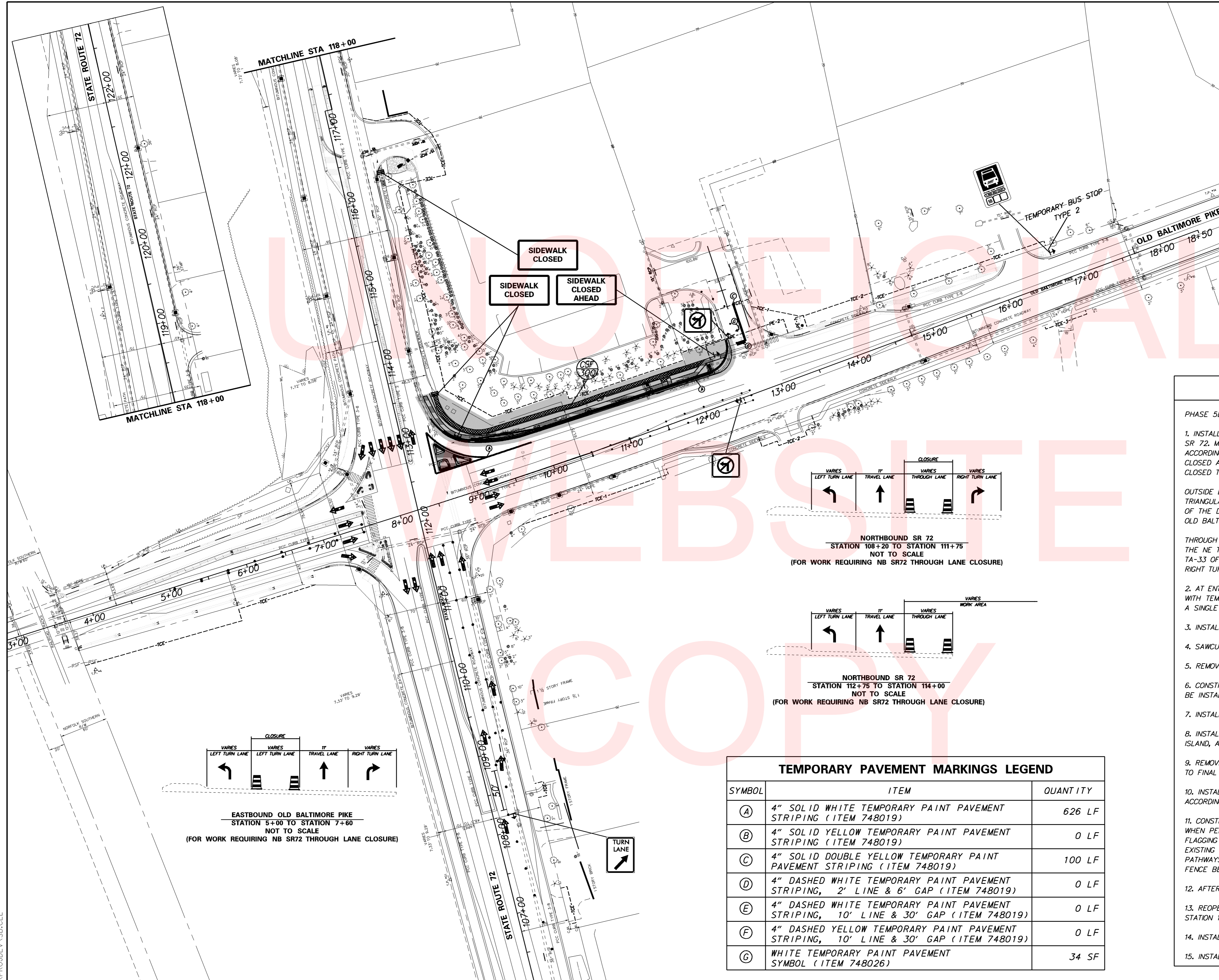
- ### SEQUENCE OF CONSTRUCTION
- PHASE 4
- PRIOR TO INSTALLATION OF FULL CLOSURE AND DETOUR, SIGNAL AND LIGHTING WORK OUTSIDE OF THE ROADWAY ON THE WEST LEG OF THE INTERSECTION SHOULD BE COMPLETED UTILIZING SHOULDER AND OUTSIDE LANE CLOSURES. CLOSURES SHALL BE INSTALLED ACCORDING TO TA-33 AND TA-3A OF THE DELAWARE MUTCD.
 - INSTALL DETOUR TO CLOSE WEST LEG OF OLD BALTIMORE IN EB AND WB DIRECTIONS. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS SHALL BE INSTALLED ACCORDING TO DETOUR PLANS (SHEET 37). DETOUR IS ALLOWED TO BE IN PLACE FOR 18 CONSECUTIVE DAYS. THE CONTRACTOR SHALL NOTIFY LYNN RILEY FROM DAYETT MILL, AT (302) 577-5170 or Lynn.Riley@state.de.us, AT LEAST 7 DAYS BEFORE THE CLOSURE IS INSTALLED.
 - INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN
 - CONVERT INLET TO JUNCTION BOX, INSTALL NEW PIPE AND DRAINAGE INLET, AND CONSTRUCT MULTI-USE PATH EXTENSION ON NORTH WEST CORNER OF INTERSECTION
 - USING PREVIOUSLY CONSTRUCTED MULTI-USE PATH EXTENSION, INSTALL TEMPORARY PEDESTRIAN FACILITIES AS SHOWN ON PLANS. PEDESTRIAN CHANNELIZATION BARRIER (ITEM 743552) SHALL BE INSTALLED ALONG BOTH EDGES OF PATH TO GUIDE PEDESTRIANS, AS SHOWN IN PLANS. PEDESTRIAN FLAGGERS SHALL BE PROVIDED WHEN PEDESTRIAN ACCESS CANNOT BE MAINTAINED.
 - SAWCUT PAVEMENT ACCORDING TO PLANS
 - REMOVE CURB RAMPS, MEDIAN NOSE AND TRIANGULAR ISLANDS
 - CONSTRUCT CAP ON CITY OF NEWARK WATER LINE IN THE NORTH WEST PORK CHOP ISLAND. CAPPING SHALL BE CONSTRUCTED ACCORDING TO DETAIL ON SHEET 24. WATER LINE SHUT DOWN RESTRICTED TO ALLOWABLE HOURS DETAILED ON SHEET 24.
 - CONVERT EXISTING DRAINAGE INLET IN SOUTH WEST PORK CHOP ISLAND TO JUNCTION BOX
 - INSTALL PEDESTRIAN SIGNAL EQUIPMENT AND CONSTRUCT TRIANGULAR CHANNELIZING ISLANDS, CURB RAMPS, AND CURB AND MEDIAN, EAST OF STATION 6+50. INSTALL TEMPORARY STRIPING FOR CROSSWALK
 - REMOVE TEMPORARY PEDESTRIAN FACILITIES AND ALLOW PEDESTRIANS TO CROSS AT PERMANENT CROSSWALK LOCATION ACROSS PREVIOUSLY CONSTRUCTED TRIANGULAR ISLANDS AND CURB RAMPS
 - INSTALL REMAINING SIGNAL EQUIPMENT, LIGHTING, CURB, MEDIAN, PAVEMENT WEDGING, AND ROADSIDE DITCHES, WEST OF STATION 6+50, IN ACCORDANCE WITH PLANS
 - MILL AND OVERLAY WEST LEG OF OLD BALTIMORE PIKE TO JOINT WITH PREVIOUS MILLING JOB, AT WEST EDGE OF DAYETT MILLS ROAD. MILLING AND OVERLAYING BETWEEN THE RAILS SHALL BE COMPLETED BY RAILROAD CONTRACTOR AFTER CONCLUSION OF THIS CONTRACT
 - INSTALL TEMPORARY STRIPING IN FINAL CONFIGURATION
 - INSTALL TOPSOIL, SEED, AND MULCH IN DISTURBED AREAS
 - VERIZON SHALL ADJUST MANHOLE LIDS AT STA. 4+80 LT, AFTER FINAL GRADING IS COMPLETE
 - INSTALL NECESSARY PERMANENT SIGNING AND REMOVE TRAFFIC CONTROL



TEMPORARY PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	148 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	0 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	99 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	0 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	112 SF

- ### SEQUENCE OF CONSTRUCTION
- PHASE 5A
- INSTALL TRAFFIC CONTROL. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS SHALL BE INSTALLED ACCORDING TO TA-33 OF THE DELAWARE MUTCD. LEFT TURN LANE INTO ROYAL FARMS SHALL BE CLOSED AT THE BEGINNING OF THIS PHASE ACCORDING TO PLANS. THIS TURN LANE SHALL REMAIN CLOSED THROUGH THE REMAINDER OF THIS CONTRACT. INSTALL VARIABLE MESSAGE SIGNS ON EB OLD BALTIMORE PIKE, WEST OF THE INTERSECTION, AND NB SR 72, SOUTH OF THE INTERSECTION, WARNING OF CLOSED ENTRANCE. VARIABLE MESSAGE SIGNS SHALL REMAIN IN PLACE FOR FIVE DAYS AFTER IMPLEMENTATION OF CLOSED LEFT TURN LANE.
 - AT ENTRANCE TO ROYAL FARMS, CLOSE NORTHBOUND ENTRANCE LANE. GRIND OFF EXISTING SB/LEFT TURN ARROW AND HALF OF STOP BAR AND RESTRIPE WITH TEMPORARY STRIPING AS SHOWN. FOR THIS PHASE, MAINTAIN A SINGLE ENTRANCE LANE AND A SINGLE EXIT LANE, ALLOWING RIGHT TURNS IN AND RIGHT TURNS OUT, ONLY.
 - INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN
 - VERIZON SHALL RELOCATE POLE AND GUY WIRE AT STATION 13+50, LEFT, AS SHOWN ON PLANS.
 - UNITED WATER SHALL RELOCATE WATER VALVE AT 13+20, LEFT, OUT OF THE PROPOSED SIDEWALK.
 - SAWCUT EXISTING PAVEMENT AS SHOWN ON PLANS
 - REMOVE EXISTING CURB, SIDEWALK AND PAVEMENT AS SHOWN IN PLANS. PEDESTRIAN FLAGGING OPERATIONS SHALL BE UTILIZED WHEN PEDESTRIAN ACCESS CANNOT BE MAINTAINED.
 - CONVERT DRAINAGE INLET TO MANHOLE AND INSTALL NEW DRAINAGE INLET AND PIPE STARTING FROM THE DOWNSTREAM END, AS SHOWN ON PLANS
 - INSTALL CURB, PAVEMENT, SIDEWALK AND TEMPORARY STRIPING, ACCORDING TO PLANS
 - INSTALL TEMPORARY BUS STOP AT EAST END OF NEW SIDEWALK, AS SHOWN ON PLANS
 - INSTALL TOPSOIL, SEED, AND MULCH IN DISTURBED AREAS
 - INSTALL NECESSARY PERMANENT SIGNING AND REMOVE TRAFFIC CONTROL

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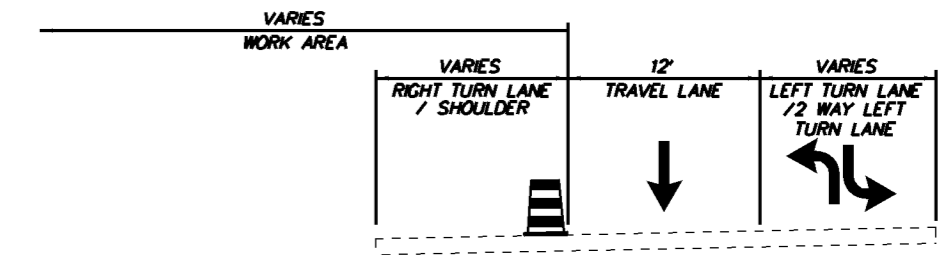
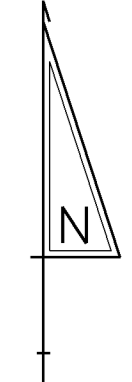
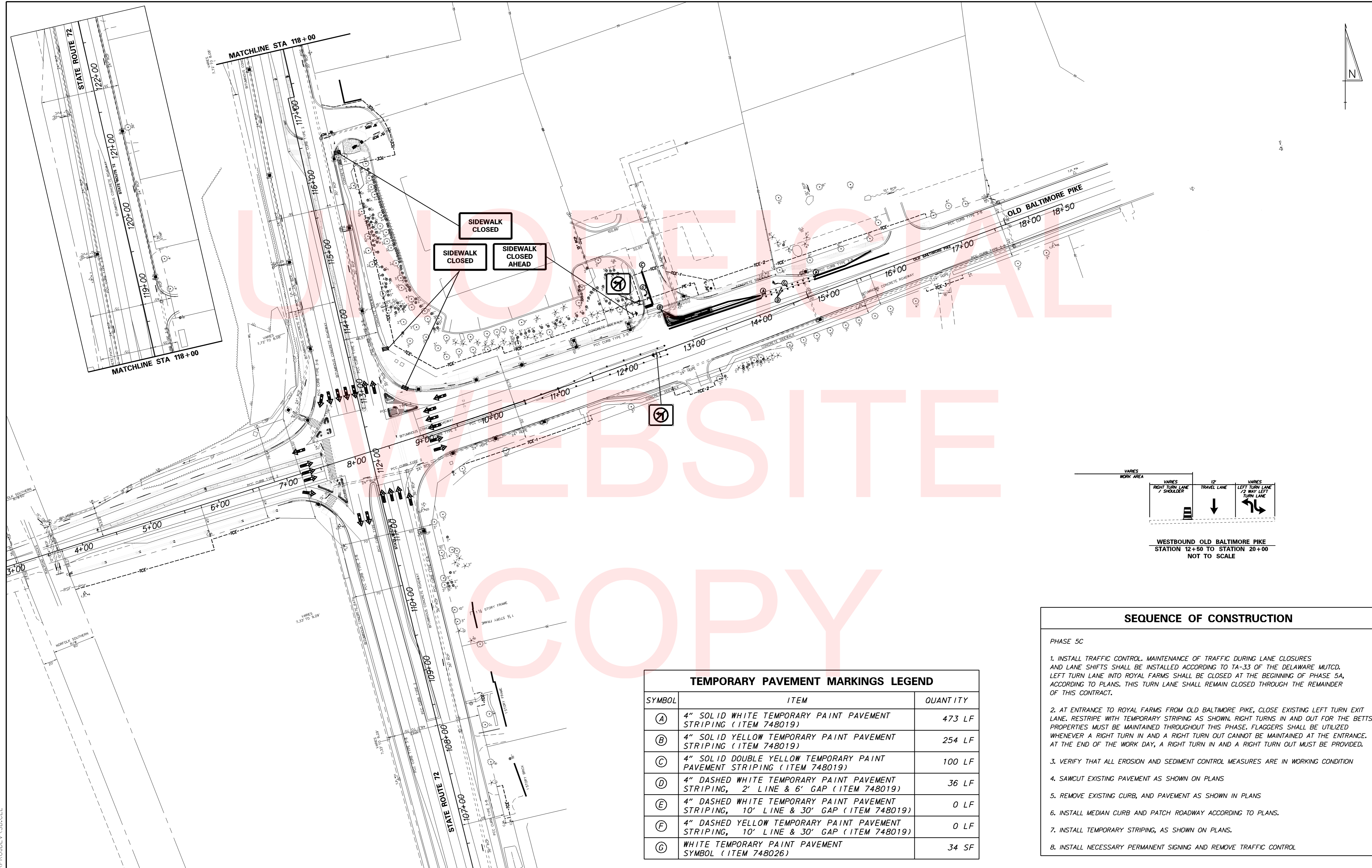
SEQUENCE OF CONSTRUCTION

- PHASE 5B
- INSTALL TRAFFIC CONTROL TO CLOSE RIGHT TURN LANE FROM WB OLD BALTIMORE PIKE TO NB SR 72. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS SHALL BE INSTALLED ACCORDING TO TA-33 OF THE DELAWARE MUTCD. LEFT TURN LANE INTO ROYAL FARMS SHALL BE CLOSED AT THE BEGINNING OF PHASE 5A, ACCORDING TO PLANS. THIS TURN LANE SHALL REMAIN CLOSED THROUGH THE REMAINDER OF THIS CONTRACT.
 - OUTSIDE LANE OF NB SR 72 MAY BE CLOSED WHEN NECESSARY FOR WORK IN THE NE TRIANGULAR CHANNELIZING ISLAND. LANE SHIFTS SHALL BE INSTALLED ACCORDING TO TA-33 OF THE DELAWARE MUTCD. WHILE THIS LANE IS CLOSED, OUTSIDE LEFT TURN LANE FROM EB OLD BALTIMORE PIKE TO NB OLD BALTIMORE PIKE SHALL BE CLOSED ACCORDING TO PLANS.
 - THROUGH LANE OF WB OLD BALTIMORE PIKE MAY BE CLOSED WHEN NECESSARY FOR WORK IN THE NE TRIANGULAR CHANNELIZING ISLAND. LANE SHIFTS SHALL BE INSTALLED ACCORDING TO TA-33 OF THE DELAWARE MUTCD. TRAFFIC OFFICERS SHALL BE UTILIZED TO PROVIDE THROUGH AND RIGHT TURN MOVEMENTS FROM THE OUTSIDE LEFT TURN LANE WHILE THIS CLOSURE IS IN PLACE.
 - AT ENTRANCE TO ROYAL FARMS, CLOSE EXISTING RIGHT TURN EXIT LANE. RESTRIPE ENTRANCE WITH TEMPORARY STRIPING AS SHOWN. FOR THIS PHASE, MAINTAIN A SINGLE ENTRANCE LANE AND A SINGLE EXIT LANE, ALLOWING RIGHT TURNS IN AND RIGHT TURNS OUT, ONLY.
 - INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN
 - SAWCUT EXISTING PAVEMENT AS SHOWN ON PLANS.
 - REMOVE EXISTING CURB, AND PAVEMENT AS SHOWN IN PLANS.
 - CONSTRUCT DRAINAGE INLETS AND PIPES, ACCORDING TO PLANS. DRAINAGE SYSTEM SHALL BE INSTALLED BEGINNING AT THE DOWNSTREAM END AND WORKING UPSTREAM.
 - INSTALL CURB AND PATCH ROADWAY, ACCORDING TO PLANS.
 - INSTALL LIGHTING AND SIGNAL FACILITIES IN THE NE CORNER AND TRIANGULAR CHANNELIZING ISLAND, ACCORDING TO PLANS.
 - REMOVE PREVIOUS PATCHING AT NE TRIANGULAR CHANNELIZING ISLAND AREA. ADJUST MANHOLE TO FINAL ELEVATION.
 - INSTALL CURB, SIDEWALK, AND PATCH PAVEMENT FOR NE TRIANGULAR CHANNELIZING ISLAND, ACCORDING TO PLANS.
 - CONSTRUCT NEW SIDEWALK AND REMOVE EXISTING SIDEWALK ACCORDING TO PLANS. WHEN PEDESTRIAN ACCESS CANNOT BE MAINTAINED DURING CONSTRUCTION, PEDESTRIAN FLAGGING SHALL BE UTILIZED. AT THE END OF THE WORK DAY, CONTRACTOR SHALL UTILIZE EXISTING SIDEWALK AND PROPOSED SIDEWALK, ALONG WITH TEMPORARY PEDESTRIAN PATHWAYS IN ORDER TO MAINTAIN PEDESTRIAN ACCESS. INSTALL CONSTRUCTION SAFETY FENCE BETWEEN OPEN PEDESTRIAN PATHWAY AND CONSTRUCTION AREA.
 - AFTER FINAL GRADING IS COMPLETE, DELMARVA GAS SHALL ADJUST GAS VALVE AT STA. 9+60.
 - REOPEN BUS STOP IN PERMANENT POSITION. CONCRETE PAD FROM TEMPORARY BUS STOP AT STATION 16+75 WILL REMAIN IN PLACE.
 - INSTALL TOPSOIL, SEED, AND MULCH IN DISTURBED AREAS
 - INSTALL NECESSARY PERMANENT SIGNING AND REMOVE TRAFFIC CONTROL

TEMPORARY PAVEMENT MARKINGS LEGEND

SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	626 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	0 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	100 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	0 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	34 SF

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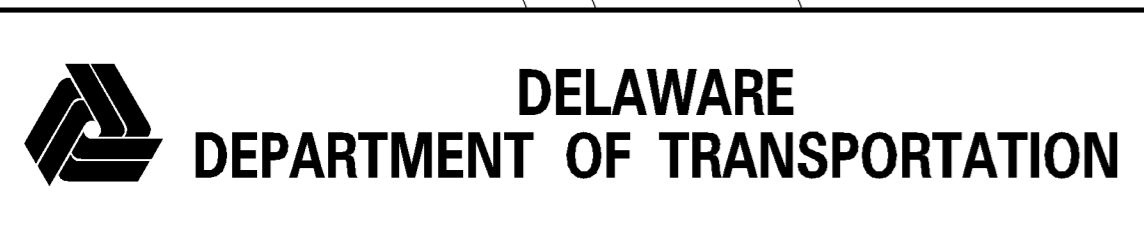


WESTBOUND OLD BALTIMORE PIKE
STATION 12+50 TO STATION 20+00
NOT TO SCALE

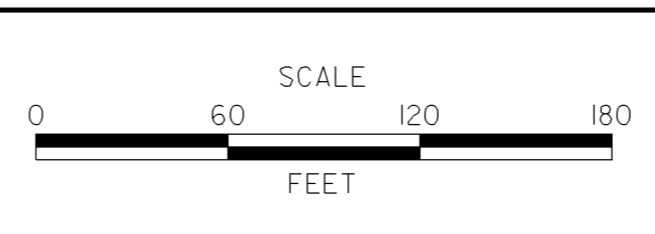
TEMPORARY PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	473 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	254 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	100 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	36 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	34 SF

SEQUENCE OF CONSTRUCTION	
PHASE 5C	
1. INSTALL TRAFFIC CONTROL. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS SHALL BE INSTALLED ACCORDING TO TA-33 OF THE DELAWARE MUTCD. LEFT TURN LANE INTO ROYAL FARMS SHALL BE CLOSED AT THE BEGINNING OF PHASE 5A, ACCORDING TO PLANS. THIS TURN LANE SHALL REMAIN CLOSED THROUGH THE REMAINDER OF THIS CONTRACT.	
2. AT ENTRANCE TO ROYAL FARMS FROM OLD BALTIMORE PIKE, CLOSE EXISTING LEFT TURN EXIT LANE. RESTRIPE WITH TEMPORARY STRIPING AS SHOWN. RIGHT TURNS IN AND OUT FOR THE BETTS PROPERTIES MUST BE MAINTAINED THROUGHOUT THIS PHASE. FLAGGERS SHALL BE UTILIZED WHENEVER A RIGHT TURN IN AND A RIGHT TURN OUT CANNOT BE MAINTAINED AT THE ENTRANCE. AT THE END OF THE WORK DAY, A RIGHT TURN IN AND A RIGHT TURN OUT MUST BE PROVIDED.	
3. VERIFY THAT ALL EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING CONDITION	
4. SAWCUT EXISTING PAVEMENT AS SHOWN ON PLANS	
5. REMOVE EXISTING CURB, AND PAVEMENT AS SHOWN IN PLANS	
6. INSTALL MEDIAN CURB AND PATCH ROADWAY ACCORDING TO PLANS.	
7. INSTALL TEMPORARY STRIPING, AS SHOWN ON PLANS.	
8. INSTALL NECESSARY PERMANENT SIGNING AND REMOVE TRAFFIC CONTROL	

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ADDENDUMS / REVISIONS	

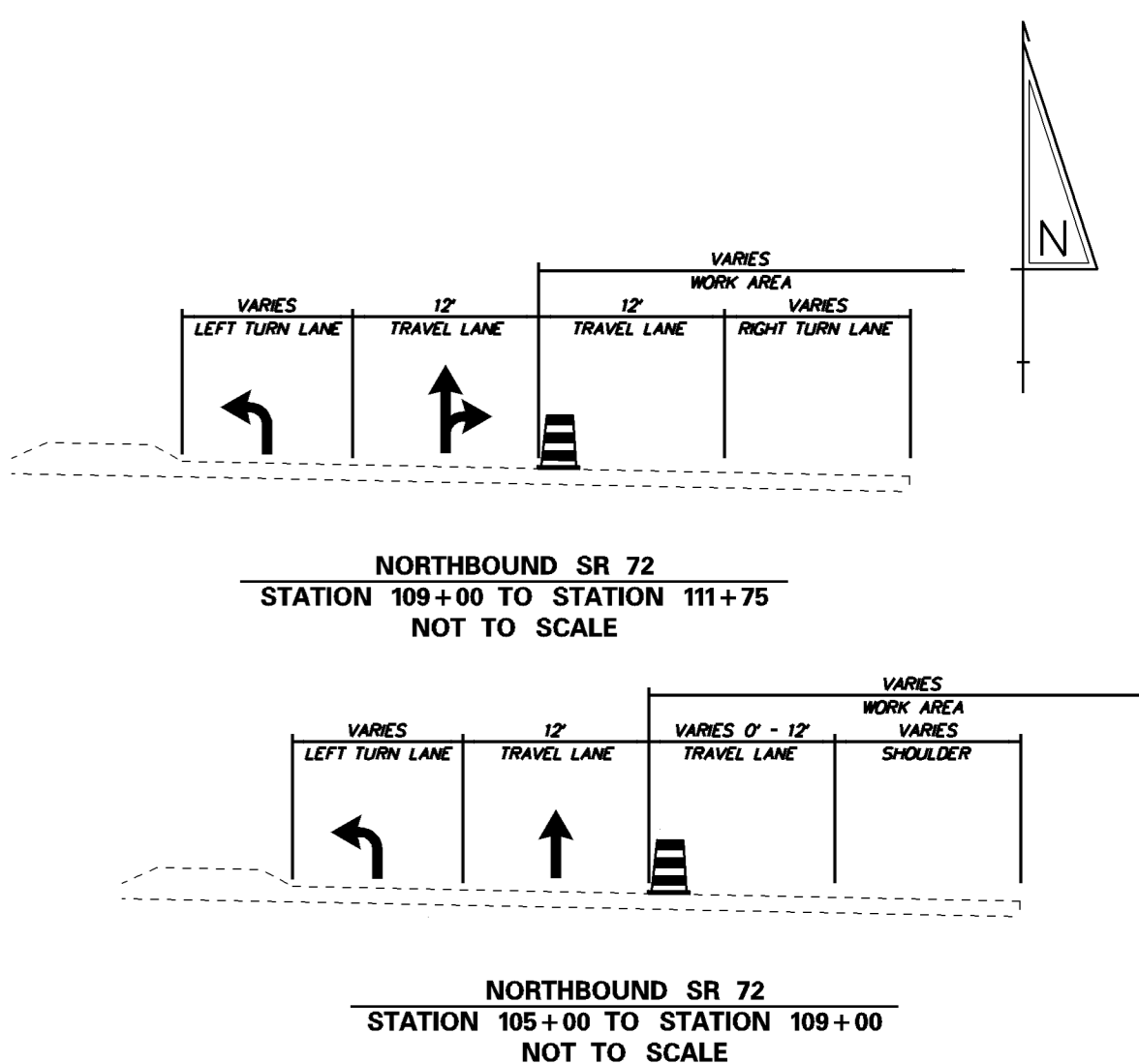
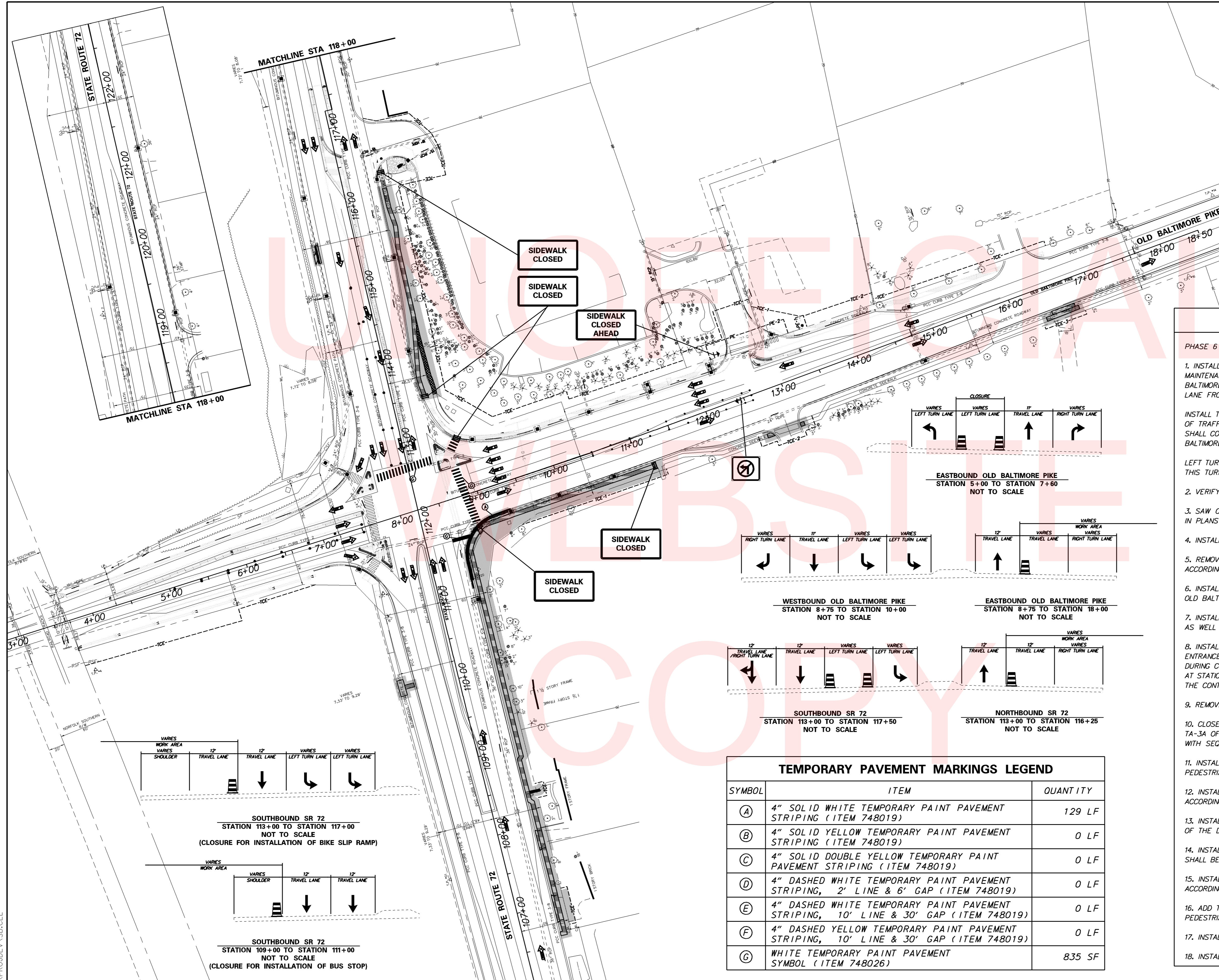


**HEP NCC,
SR 72 AND OLD BALTIMORE PIKE
INTERSECTION IMPROVEMENTS**

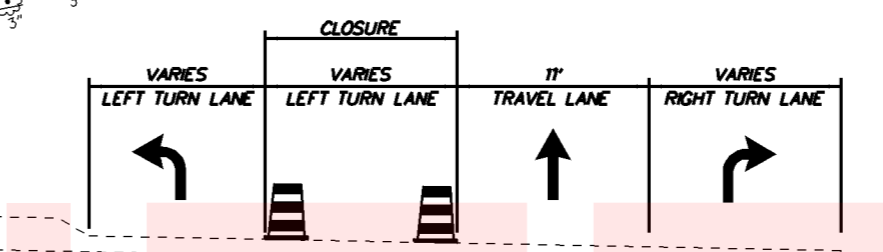
CONTRACT	BRIDGE NO.	N/A
T201200108	DESIGNED BY:	BCD
COUNTY	CHECKED BY:	MCN
NEW CASTLE		

**CONSTRUCTION PHASING,
M.O.T., AND EROSION
CONTROL PLAN - PHASE 5C**

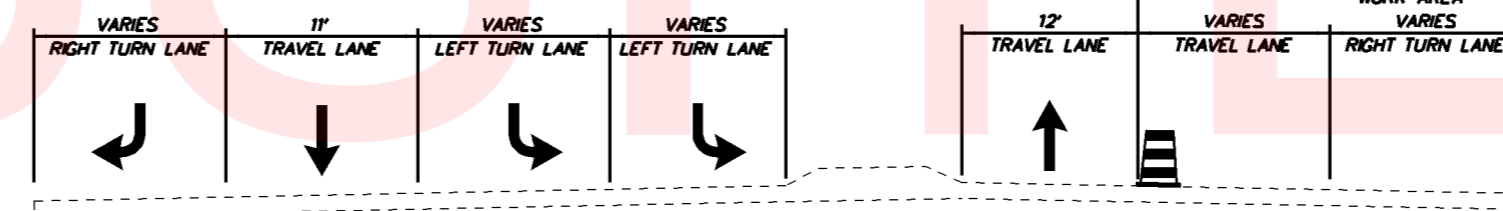
SHEET NO.	32
TOTAL SHTS.	52



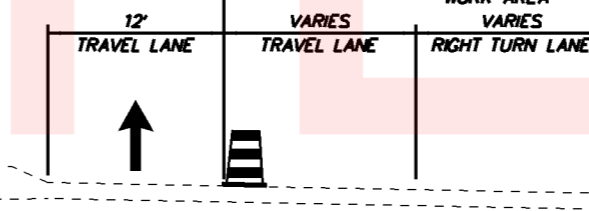
- ### SEQUENCE OF CONSTRUCTION
- PHASE 6
- INSTALL TRAFFIC CONTROL EXTENDING FROM NB SR72 ONTO EB OLD BALTIMORE PIKE. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS ON NB SR72 AND EB OLD BALTIMORE PIKE SHALL CONFORM TO TA-33 OF THE DELAWARE MUTCD. THE OUTSIDE LEFT TURN LANE FROM SB SR72 TO EB OLD BALTIMORE PIKE SHALL ALSO BE CLOSED ACCORDING TO PLANS.
 - INSTALL TRAFFIC CONTROL EXTENDING FROM WB OLD BALTIMORE PIKE ONTO NB SR72. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS ON NB SR72 AND WB OLD BALTIMORE PIKE SHALL CONFORM TO TA-33 OF THE DELAWARE MUTCD. THE OUTSIDE LEFT TURN LANE FROM EB OLD BALTIMORE PIKE TO NB SR 72 SHALL ALSO BE CLOSED ACCORDING TO PLANS.
 - LEFT TURN LANE INTO ROYAL FARMS WAS CLOSED AT THE BEGINNING OF PHASE 5A. THIS TURN LANE SHALL REMAIN CLOSED THROUGH THE REMAINDER OF THIS CONTRACT
 - VERIFY THAT ALL EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING CONDITION
 - SAW CUT PAVEMENT AND REMOVE CURB AT SOUTHEAST CORNER OF INTERSECTION, AS SHOWN IN PLANS
 - INSTALL CURB AT SOUTHEAST CORNER
 - REMOVE EXISTING SIDEWALK ON EAST SIDE OF NORTH LEG AND INSTALL NEW SIDEWALK, ACCORDING TO PLANS.
 - INSTALL SIDEWALK, BUS STOP, AND CURB RAMPS ALONG SOUTH EDGE OF EAST LEG OF OLD BALTIMORE PIKE AND EAST SIDE OF SR72, ACCORDING TO PLANS
 - INSTALL SIGNAL AND LIGHTING EQUIPMENT AT SE CORNER OF THE INTERSECTION, AS WELL AS THE EAST SIDE OF SR 72, ACCORDING TO PLANS.
 - INSTALL SIDEWALK DRIVEWAY CROSSING AT ENTRANCE TO FARM AND REPLACE CURB RAMPS AT ENTRANCE TO CHURCH. MAINTAIN ACCESS AND EGRESS FROM BOTH PARCELS AT ALL TIMES. DURING CONSTRUCTION, WHEN PEDESTRIAN ACCESS CANNOT BE MAINTAINED TO THE BUS STOP AT STATION 11+15, PEDESTRIAN FLAGGING SHALL BE UTILIZED. AT THE END OF THE WORK DAY, THE CONTRACTOR SHALL RESTORE PEDESTRIAN ACCESS TO BUS STOP.
 - REMOVE TRAFFIC CONTROL FOR CLOSURES ON NB SR 72 AND EB OLD BALTIMORE PIKE.
 - CLOSE OUTSIDE SHOULDER OF SB SR72, SOUTH OF THE INTERSECTION, ACCORDING TO TA-3A OF THE DELAWARE MUTCD. (THIS CLOSURE MAY BE IN PLACE CONCURRENTLY WITH SEQUENCES 3 THROUGH 8)
 - INSTALL BUS STOP AT STATION 110+00 AND PATCH MULTI-USE PATH, ACCORDING TO PLANS. PEDESTRIAN FLAGGERS SHALL BE UTILIZED WHILE PEDESTRIAN ACCESS CANNOT BE MAINTAINED.
 - INSTALL LIGHTING FACILITIES ALONG THE WEST EDGE OF THE SOUTH LEG OF THE INTERSECTION, ACCORDING TO LIGHTING PLANS
 - INSTALL OUTSIDE LANE CLOSURE OF SB SR72, NORTH OF THE INTERSECTION, UTILIZING TA-33 OF THE DELAWARE MUTCD
 - INSTALL BICYCLE SLIP RAMP AT STATION 115+50, ACCORDING TO PLANS. PEDESTRIAN FLAGGERS SHALL BE UTILIZED WHILE PEDESTRIAN ACCESS CANNOT BE MAINTAINED ON EXISTING FACILITIES.
 - INSTALL LIGHTING FACILITIES ALONG THE WEST EDGE OF THE NORTH LEG OF THE INTERSECTION, ACCORDING TO LIGHTING PLANS
 - ADD TEMPORARY CROSSWALK STRIPING IN FINAL CONFIGURATION AND OPEN ALL PERMANENT PEDESTRIAN FACILITIES.
 - INSTALL TOPSOIL, SEED, AND MULCH IN DISTURBED AREAS
 - INSTALL NECESSARY PERMANENT SIGNING AND REMOVE TRAFFIC CONTROL



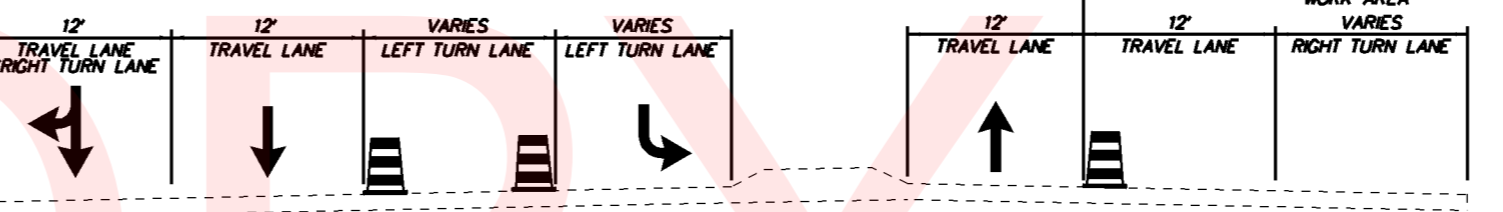
EASTBOUND OLD BALTIMORE PIKE
STATION 5+00 TO STATION 7+60
NOT TO SCALE



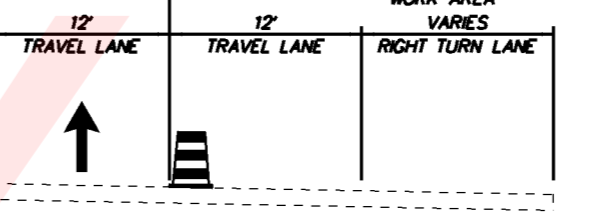
WESTBOUND OLD BALTIMORE PIKE
STATION 8+75 TO STATION 10+00
NOT TO SCALE



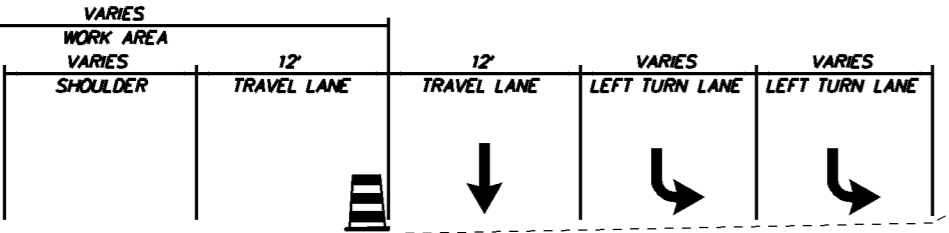
EASTBOUND OLD BALTIMORE PIKE
STATION 8+75 TO STATION 18+00
NOT TO SCALE



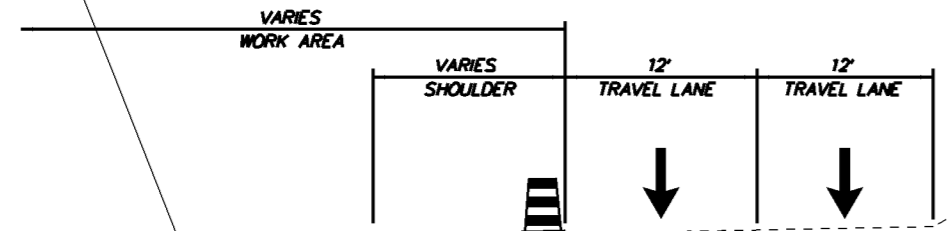
SOUTHBOUND SR 72
STATION 113+00 TO STATION 117+50
NOT TO SCALE



NORTHBOUND SR 72
STATION 113+00 TO STATION 116+25
NOT TO SCALE

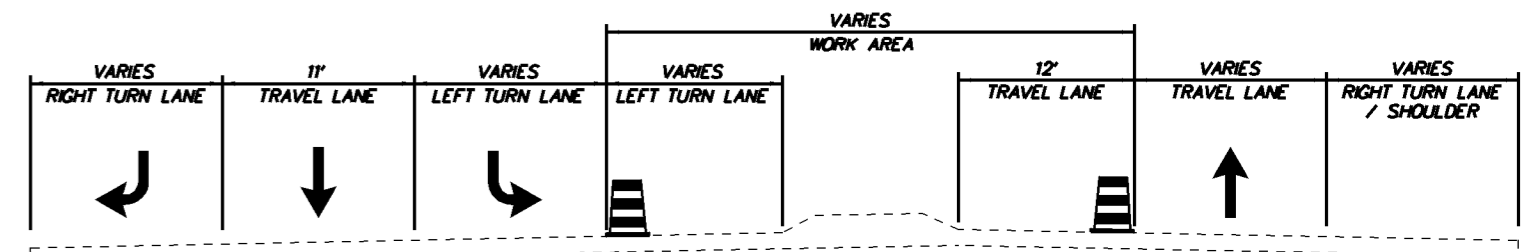
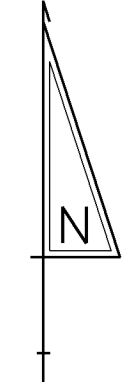
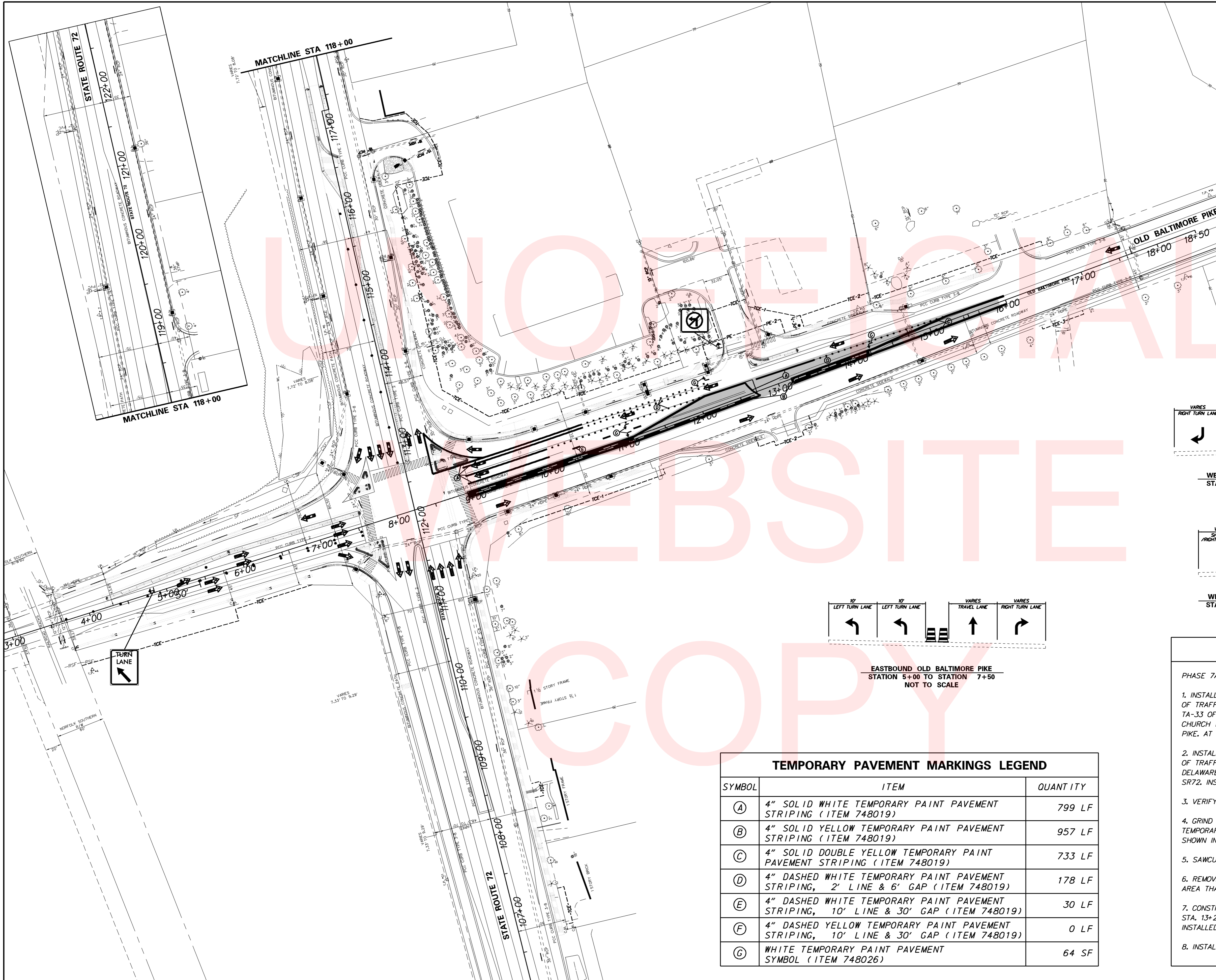


SOUTHBOUND SR 72
STATION 113+00 TO STATION 117+00
NOT TO SCALE
(CLOSURE FOR INSTALLATION OF BIKE SLIP RAMP)



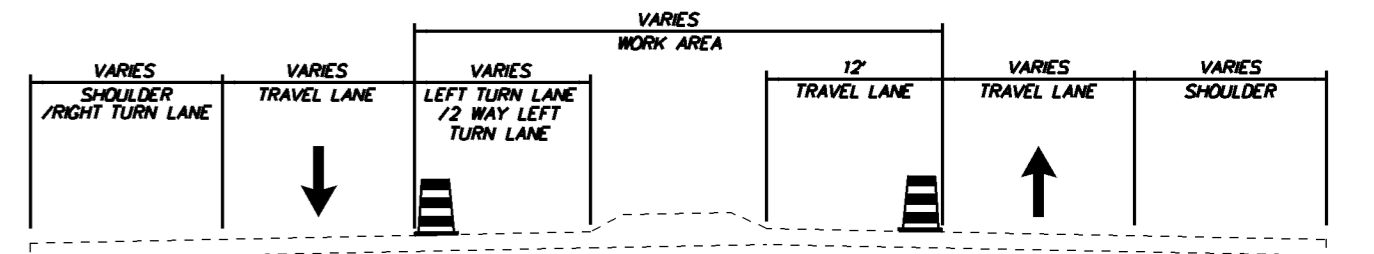
SOUTHBOUND SR 72
STATION 109+00 TO STATION 111+00
NOT TO SCALE
(CLOSURE FOR INSTALLATION OF BUS STOP)

TEMPORARY PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	129 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	0 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	0 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	0 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	835 SF



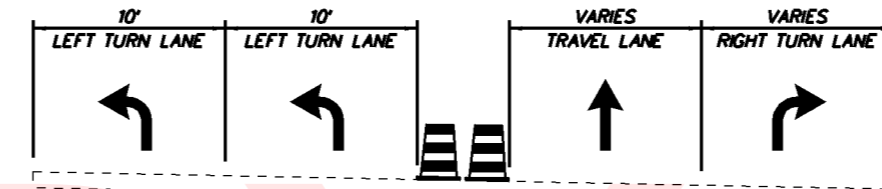
WESTBOUND OLD BALTIMORE PIKE
STATION 8+75 TO STATION 11+00
NOT TO SCALE

EASTBOUND OLD BALTIMORE PIKE
STATION 8+75 TO STATION 16+00
NOT TO SCALE



WESTBOUND OLD BALTIMORE PIKE
STATION 12+00 TO STATION 16+00
NOT TO SCALE

EASTBOUND OLD BALTIMORE PIKE
STATION 8+75 TO STATION 16+00
NOT TO SCALE



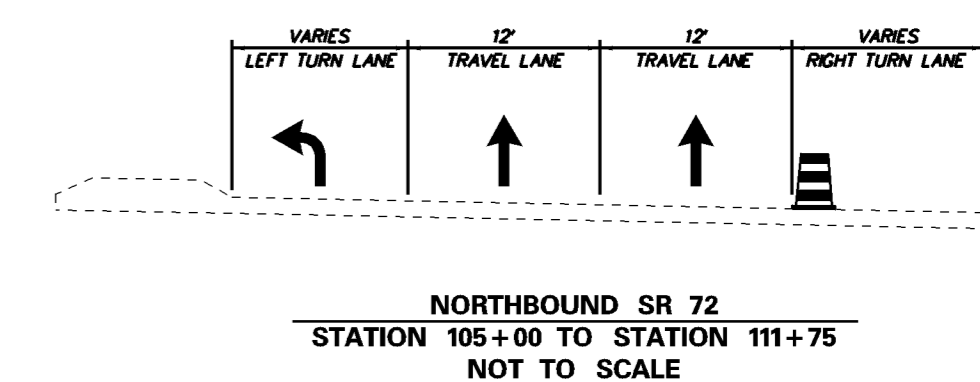
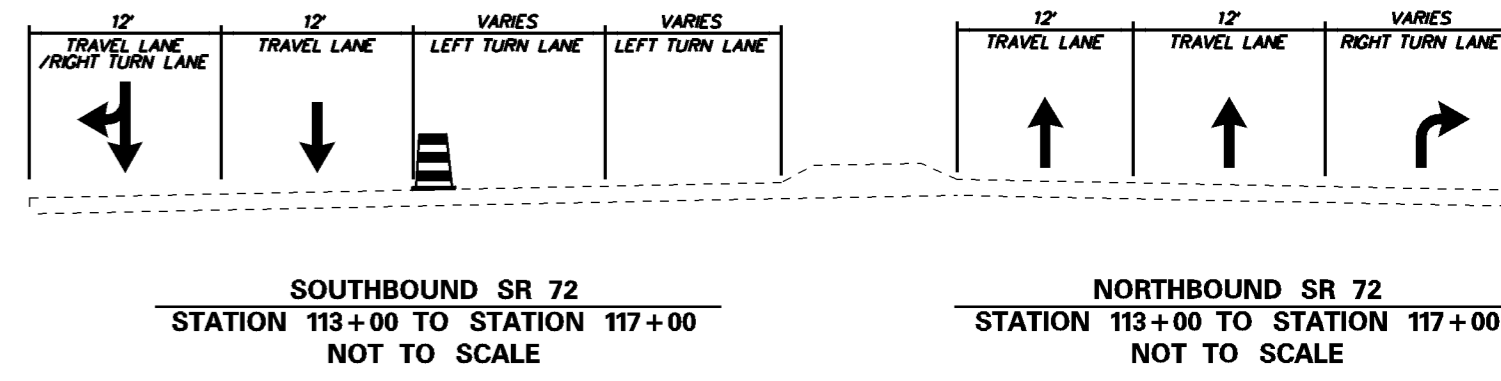
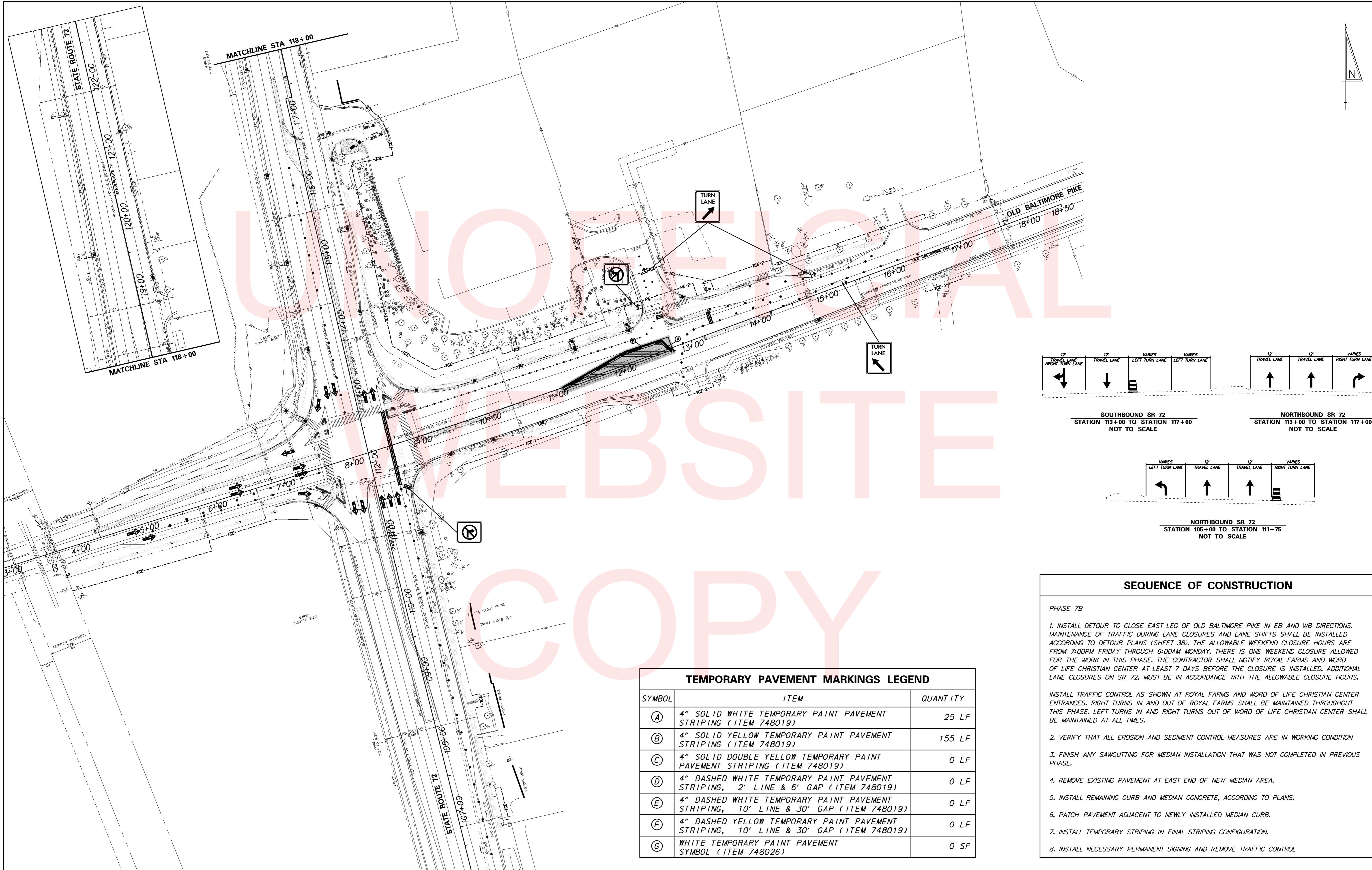
EASTBOUND OLD BALTIMORE PIKE
STATION 5+00 TO STATION 7+50
NOT TO SCALE

TEMPORARY PAVEMENT MARKINGS LEGEND

SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	799 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	957 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	733 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	178 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	30 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	64 SF

SEQUENCE OF CONSTRUCTION

- PHASE 7A
1. INSTALL TRAFFIC CONTROL TO CLOSE INNER LANE OF WB OLD BALTIMORE PIKE. MAINTENANCE OF TRAFFIC DURING LANE CLOSURE SHALL CONFORM TO TYPICAL APPLICATION TA-33 OF THE DELAWARE MUTCD. DURING ALLOWABLE LANE CLOSURE HOURS, LEFT TURN INTO CHURCH MAY BE CLOSED, ALLOWING ONLY RIGHTS IN AND RIGHTS OUT OFF OF EB OLD BALTIMORE PIKE. AT THE END OF THE WORK DAY, LEFT TURN INTO CHURCH MUST BE OPENED.
 2. INSTALL TRAFFIC CONTROL TO CLOSE INNER LANE OF EB OLD BALTIMORE PIKE. MAINTENANCE OF TRAFFIC DURING LANE CLOSURE SHALL CONFORM TO TYPICAL APPLICATION TA-33 OF THE DELAWARE MUTCD. LANE SHIFT SHALL BEGIN TO DEVELOP IN THROUGH LANE, WEST OF SR72. INSTALL SB SR72 LEFT TURN LANE CLOSURE ACCORDING TO PLANS.
 3. VERIFY THAT ALL EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING CONDITION
 4. GRIND OFF EXISTING STRIPING ON WB OLD BALTIMORE PIKE AND INSTALL TEMPORARY STRIPING ON WB OLD BALTIMORE PIKE IN FINAL CONFIGURATION, AS SHOWN IN PLANS
 5. SAWCUT PAVEMENT AS SHOWN ON PLANS
 6. REMOVE EXISTING MEDIAN AND PAVEMENT ACCORDING TO PLANS, LEAVING PAVEMENT IN AREA THAT WILL BE CONSTRUCTED DURING THE PHASE 7B.
 7. CONSTRUCT NEW MEDIAN CURB, BACK-TO-BACK CURB FROM STA. 8+90 TO 11+10, AND FROM STA. 13+20 TO 14+20. CURB ALONG SOUTH SIDE OF MEDIAN FROM 11+10 TO 12+60 SHALL ALSO BE INSTALLED DURING THIS PHASE. PATCH PAVEMENT ADJACENT TO NEW CURB, AS SHOWN ON PLANS.
 8. INSTALL NECESSARY PERMANENT SIGNING AND REMOVE TRAFFIC CONTROL



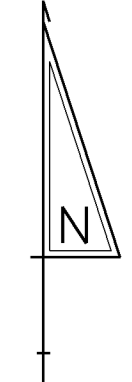
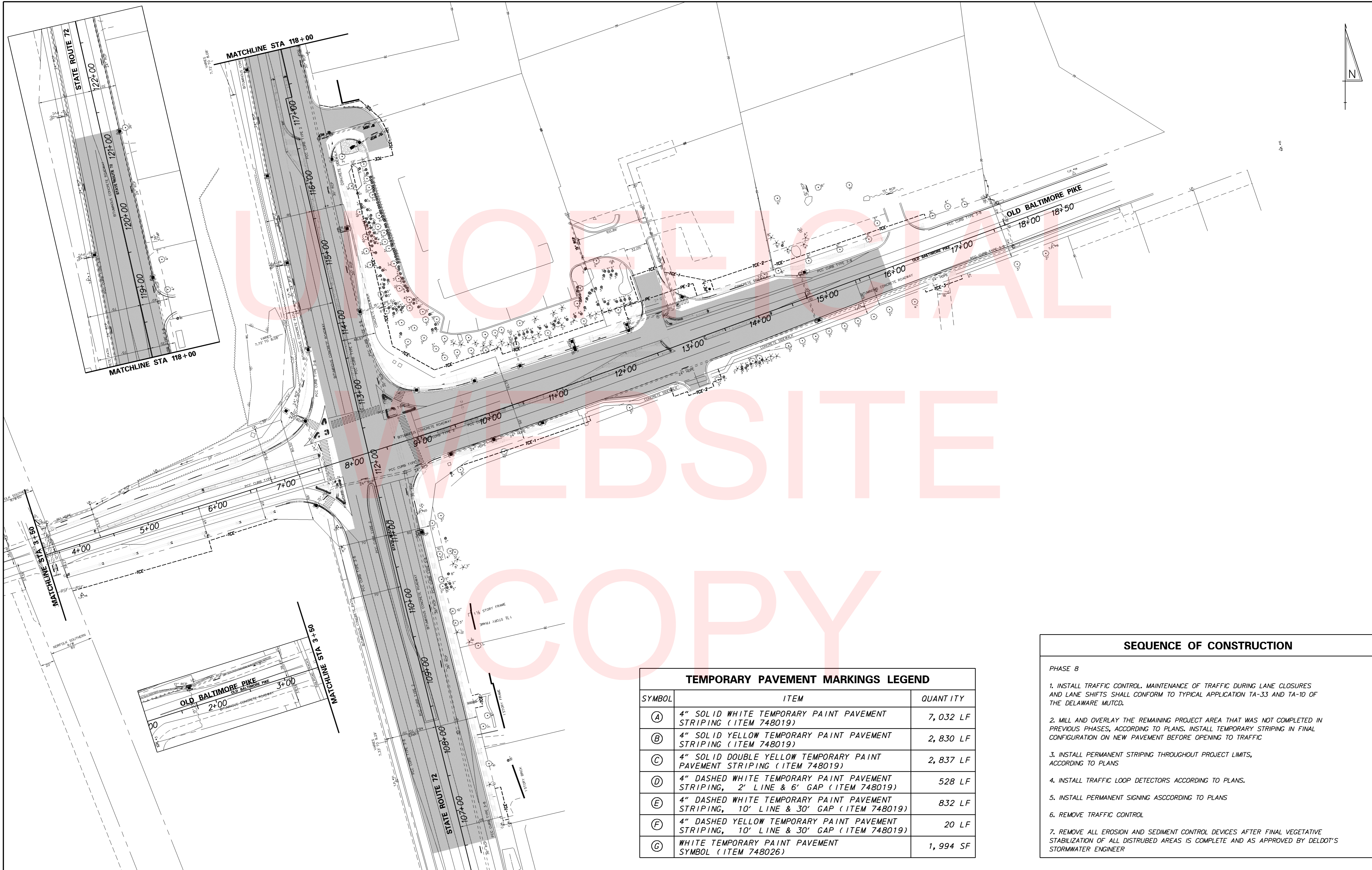
TEMPORARY PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	25 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	155 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	0 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	0 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	0 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	0 SF

SEQUENCE OF CONSTRUCTION

PHASE 7B

1. INSTALL DETOUR TO CLOSE EAST LEG OF OLD BALTIMORE PIKE IN EB AND WB DIRECTIONS. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS SHALL BE INSTALLED ACCORDING TO DETOUR PLANS (SHEET 3B). THE ALLOWABLE WEEKEND CLOSURE HOURS ARE FROM 7:00PM FRIDAY THROUGH 6:00AM MONDAY. THERE IS ONE WEEKEND CLOSURE ALLOWED FOR THE WORK IN THIS PHASE. THE CONTRACTOR SHALL NOTIFY ROYAL FARMS AND WORD OF LIFE CHRISTIAN CENTER AT LEAST 7 DAYS BEFORE THE CLOSURE IS INSTALLED. ADDITIONAL LANE CLOSURES ON SR 72, MUST BE IN ACCORDANCE WITH THE ALLOWABLE CLOSURE HOURS.
2. VERIFY THAT ALL EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING CONDITION
3. FINISH ANY SAWCUTTING FOR MEDIAN INSTALLATION THAT WAS NOT COMPLETED IN PREVIOUS PHASE.
4. REMOVE EXISTING PAVEMENT AT EAST END OF NEW MEDIAN AREA.
5. INSTALL REMAINING CURB AND MEDIAN CONCRETE, ACCORDING TO PLANS.
6. PATCH PAVEMENT ADJACENT TO NEWLY INSTALLED MEDIAN CURB.
7. INSTALL TEMPORARY STRIPING IN FINAL STRIPING CONFIGURATION.
8. INSTALL NECESSARY PERMANENT SIGNING AND REMOVE TRAFFIC CONTROL

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TEMPORARY PAVEMENT MARKINGS LEGEND

SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	7,032 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	2,830 LF
(C)	4" SOLID DOUBLE YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	2,837 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	528 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	832 LF
(F)	4" DASHED YELLOW TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	20 LF
(G)	WHITE TEMPORARY PAINT PAVEMENT SYMBOL (ITEM 748026)	1,994 SF

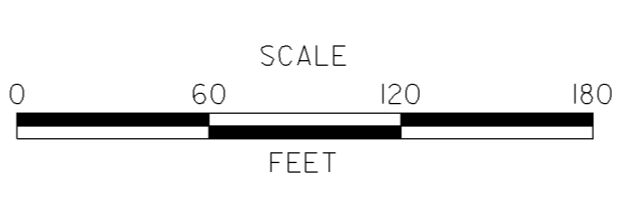
SEQUENCE OF CONSTRUCTION

PHASE 8

1. INSTALL TRAFFIC CONTROL. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS SHALL CONFORM TO TYPICAL APPLICATION TA-33 AND TA-10 OF THE DELAWARE MUTCD.
2. MILL AND OVERLAY THE REMAINING PROJECT AREA THAT WAS NOT COMPLETED IN PREVIOUS PHASES, ACCORDING TO PLANS. INSTALL TEMPORARY STRIPING IN FINAL CONFIGURATION ON NEW PAVEMENT BEFORE OPENING TO TRAFFIC
3. INSTALL PERMANENT STRIPING THROUGHOUT PROJECT LIMITS, ACCORDING TO PLANS
4. INSTALL TRAFFIC LOOP DETECTORS ACCORDING TO PLANS.
5. INSTALL PERMANENT SIGNING ACCORDING TO PLANS
6. REMOVE TRAFFIC CONTROL
7. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES AFTER FINAL VEGETATIVE STABILIZATION OF ALL DISTURBED AREAS IS COMPLETE AND AS APPROVED BY DELDOT'S STORMWATER ENGINEER

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ADDENDUMS / REVISIONS



CONTRACT T201200108	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: BCD CHECKED BY: MCN

PORTABLE CHANGEABLE MESSAGE SIGNS

PRIOR TO DETOUR
(10 DAYS PRIOR TO BEGINNING OF DETOUR)

PCMS-1

OLD BALT
PIKE
TO CLOSE

STARTING
XX/XX/XX

DURING DETOUR
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF DETOUR)

PCMS-2

OLD BALT
PIKE
CLOSED

FOLLOW
DETOUR

SPECIAL SIGNS

Y

Old Balt.
Pike
CLOSED
WEST OF
72
FOLLOW
DETOUR

6" C
6" C
6" C
6" C
24"
6" C
6" C

D/G RETROFLECTIVE
FLUORESCENT ORANGE

Z

Old Balt.
Pike
CLOSED
EAST OF
896
FOLLOW
DETOUR

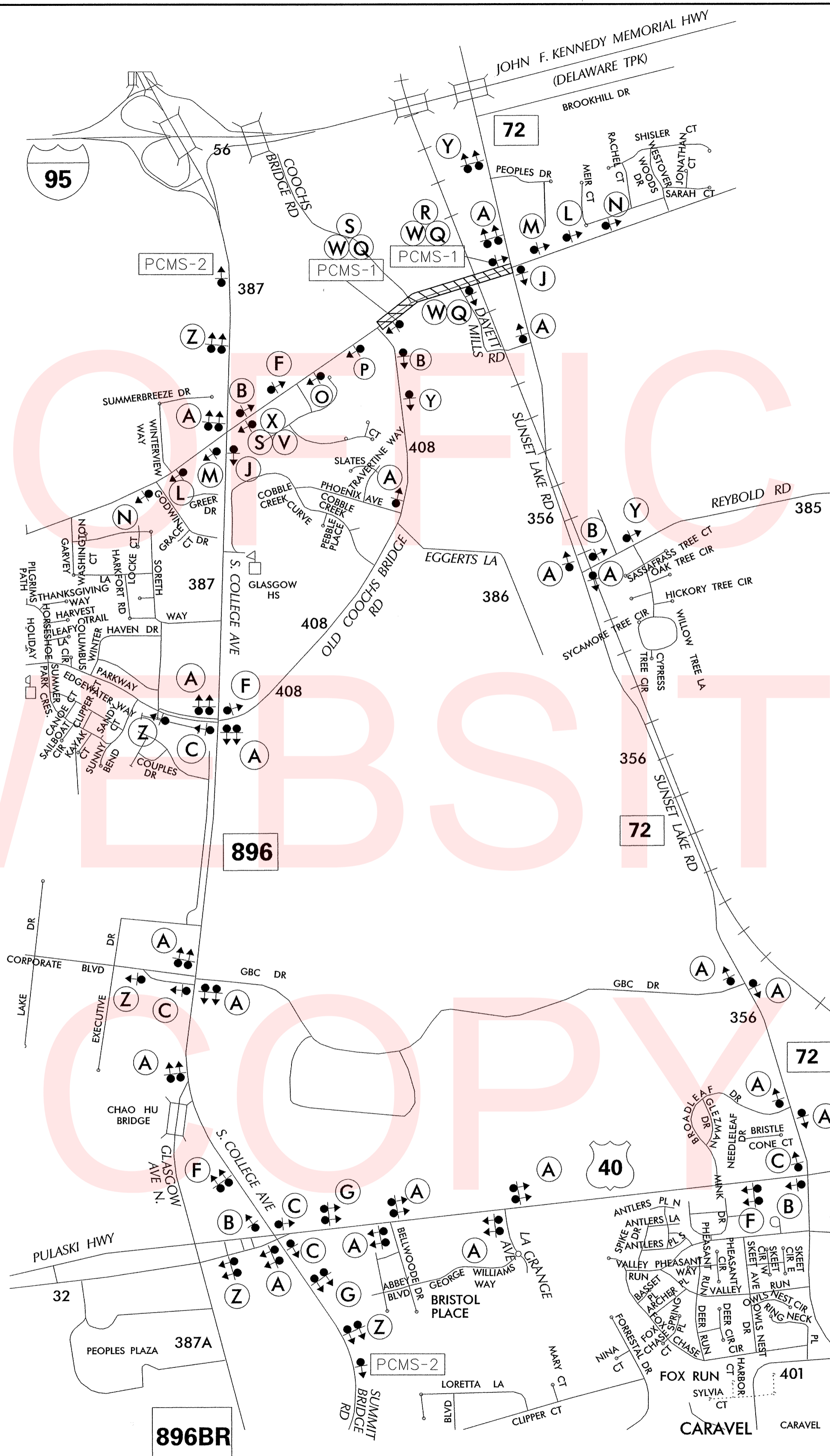
6" C
6" C
6" C
6" C
24"
6" C
6" C

D/G RETROFLECTIVE
FLUORESCENT ORANGE

72

896

BLACK LEGENDS;
WHITE BACKGROUNDS



LEGEND

A	B	C	D	E
DETOUR M4-9-DE	DETOUR M4-9	DETOUR M4-9	DETOUR M4-9	DETOUR M4-9
F	G	H	I	J
DETOUR M4-9	DETOUR M4-9	DETOUR M4-9	DETOUR M4-9	END DETOUR M4-8a
K	L	M		
DETOUR AHEAD W20-2	DETOUR 1000 FT W20-2	DETOUR 500 FT W20-2		
N	O	P		
ROAD CLOSED AHEAD W20-3	ROAD CLOSED 1000 FT W20-3	ROAD CLOSED 500 FT W20-3		
Q	R	S		
ROAD CLOSED R11-2	DETOUR M4-10(L)	DETOUR M4-10(R)		
T1	T2	V		
ROAD CLOSED 1/2 MILE AHEAD LOCAL TRAFFIC ONLY R11-3a	ROAD CLOSED 500 FT AHEAD LOCAL TRAFFIC ONLY R11-3a	ROAD CLOSED TO THRU TRAFFIC R11-4		
W		X		
BARRICADE		BARRICADE		

GENERAL NOTES

- ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH "THE DELAWARE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
- THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD PART 6) FOR BARRICADES AND SIGNS (AS PER LATEST REVISION.)
- DESIGN OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE FHWA STANDARD HIGHWAY SIGNS BOOK.
- SIZES OF ALL SIGNS SHALL BE IN ACCORDANCE WITH "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.) SIZE OF SIGN SHALL BE BASED ON TYPE OF ROADWAY ON WHICH THE SIGN IS INSTALLED.
- SIGNS NO LONGER IN USE SHALL BE COMPLETELY COVERED WITH NO RETROFLECTIVE MATERIAL SHOWING, OR SHALL BE REMOVED, AS DIRECTED BY THE ENGINEER.
- FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD) WILL PREVAIL.
- SIGNS "N" THROUGH "O" AND "T" AND "V", THE WORD "ROAD" SHOULD BE CHANGED TO "RAMP", "RR XING", OR "BRIDGE" WHERE APPLICABLE.
- WARNING SIGNS AND DETOUR TRAILBLAZERS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROFLECTIVE FLUORESCENT ORANGE SHEETING.
- "W" BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF THE ROADWAY.
- BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED _____ DATE: _____

RECOMMENDED _____ DATE: _____

RECOMMENDED _____ DATE: _____

APPROVED CHIEF SAFETY OFFICER *Michael A. [Signature]* DATE: 10-5-15

APPROVED TRAFFIC ENGINEER *[Signature]* DATE: 10/5/15

<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>	ADDENDUM / REVISIONS		<p>NOT TO SCALE</p> <p>SR 72 and Old Baltimore Pike</p>	CONTRACT	ROAD NO.	<p>N26</p> <p>VEHICULAR DETOUR PLAN for Old Baltimore Pike between DE 896 and DE 72</p>	SHEET NO.
				T201200108	DESIGNED BY: JM		37
				COUNTY	CHECKED BY: PH		TOTAL SHTS.
				NEW CASTLE			52

\\DOT\F508\CADD\ACTIVE_DESIGNS\TRAFFIC\USERS\PETER\BENEFITS_AND_TRAINING\TRAININGS\2015_CO-OP - JOSEPH MCMAHON\ASSIGNMENTS\OLD BALTIMORE PIKE BETWEEN DE 72 AND DE 896.DGN

PORTABLE CHANGEABLE MESSAGE SIGNS

PRIOR TO DETOUR
(10 DAYS PRIOR TO BEGINNING OF DETOUR)

PCMS-1

**OLD BALT
PIKE
CLOSING**

**STARTING
XXXXXX**

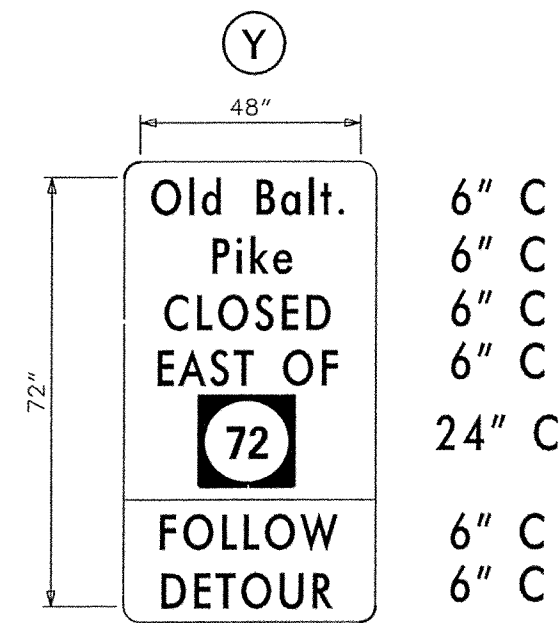
DURING DETOUR
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF DETOUR)

PCMS-2

**OLD BALT
PIKE
CLOSED**

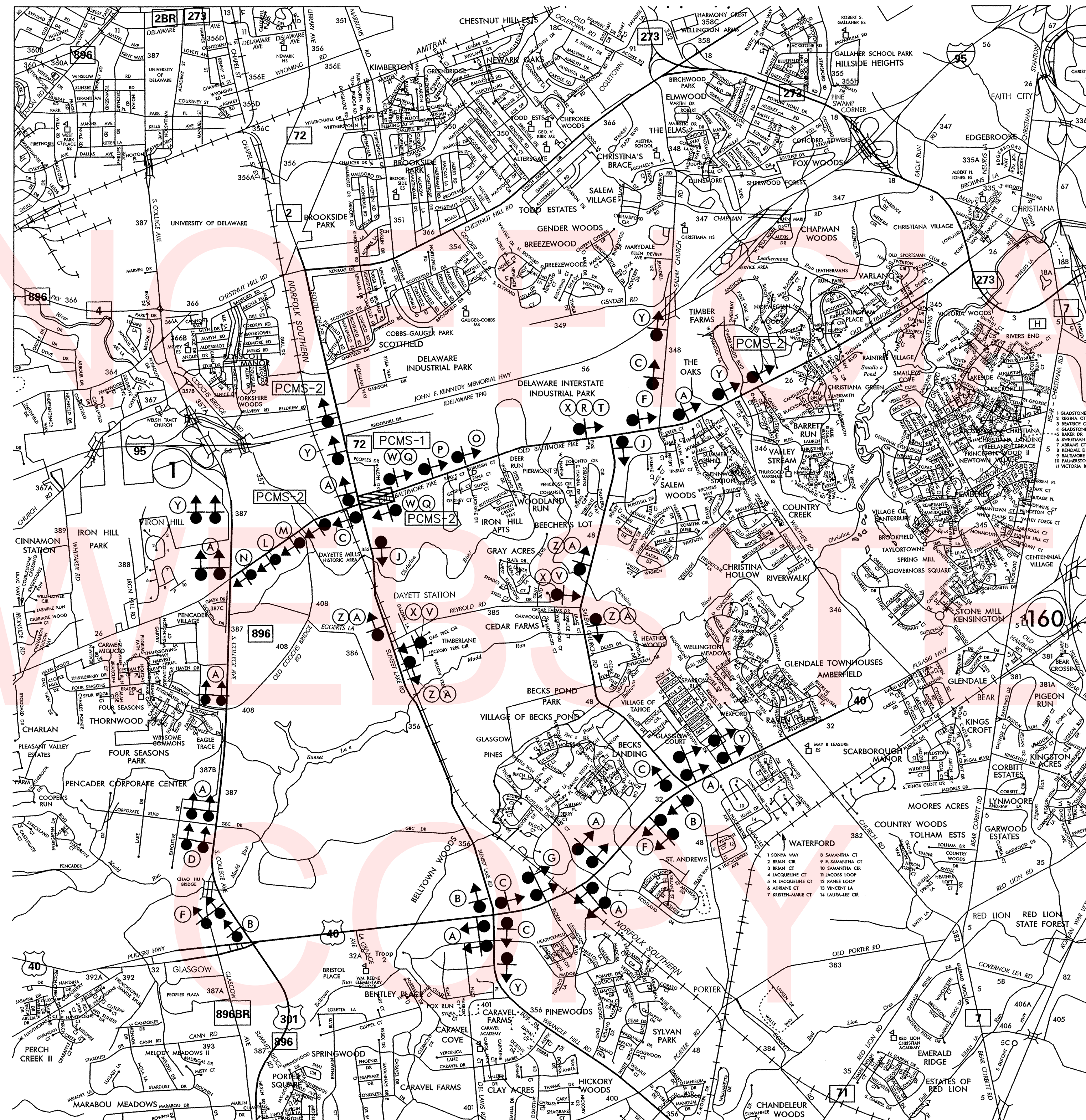
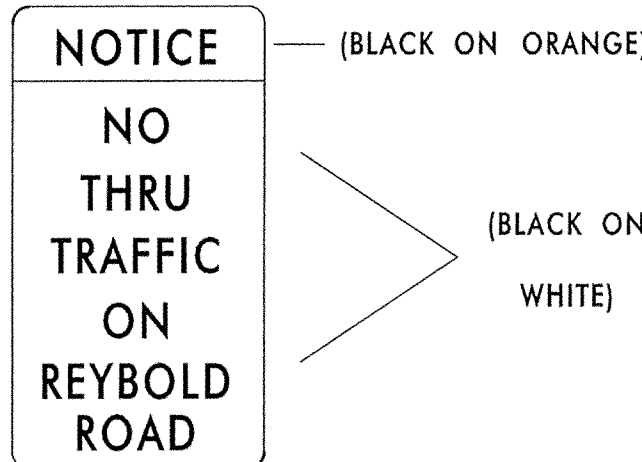
**FOLLOW
DETOUR**

SPECIAL SIGNS

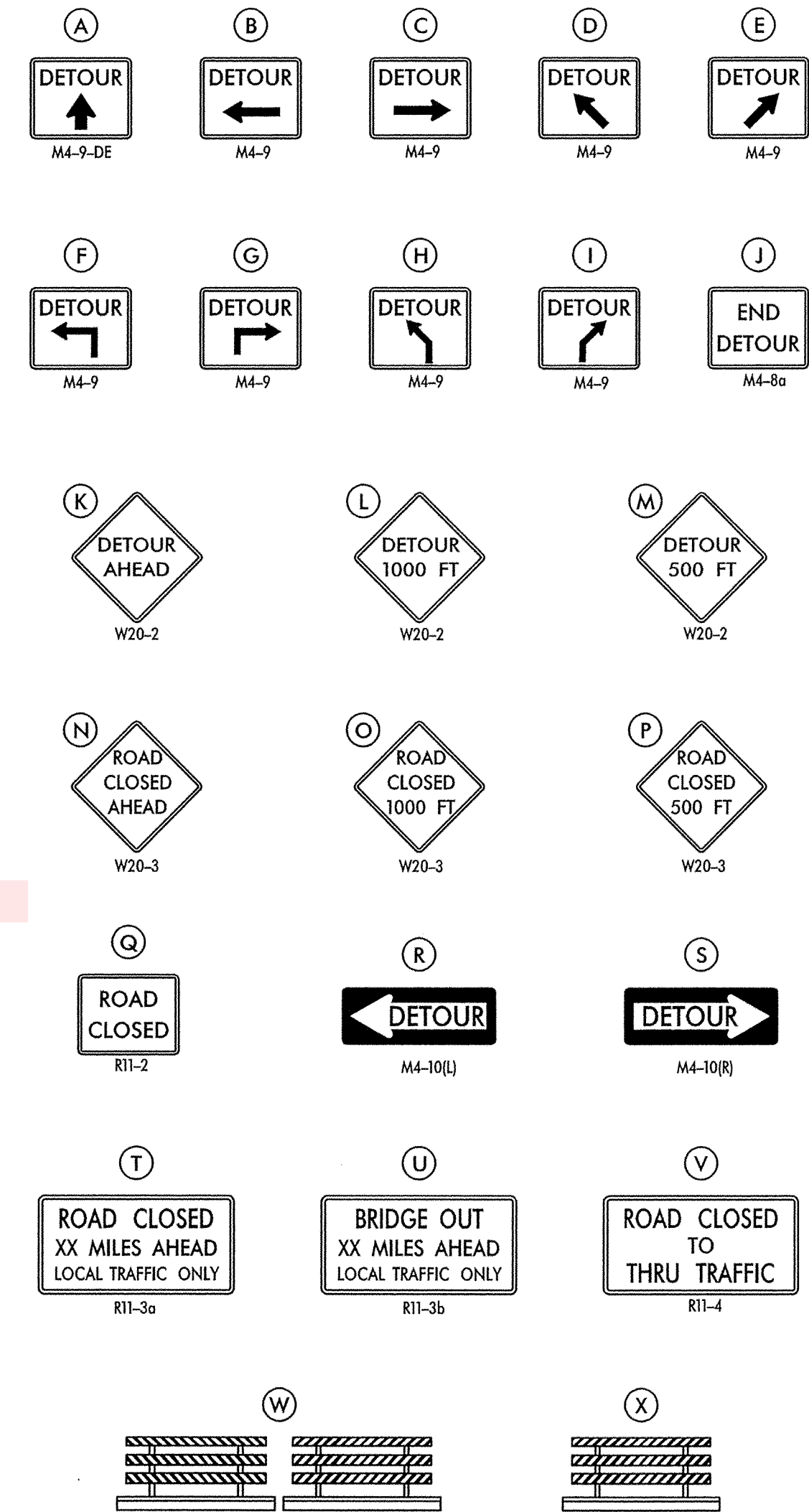


D/G RETROREFLECTIVE
FLUORESCENT ORANGE

Z



LEGEND



GENERAL NOTES

1. ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH "THE DELAWARE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
2. THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD PART 6) FOR BARRICADES AND SIGNS (AS PER LATEST REVISION.)
3. DESIGN OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE FHWA STANDARD HIGHWAY SIGNS BOOK.
4. SIZES OF ALL SIGNS SHALL BE IN ACCORDANCE WITH "THE DELAWARE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.) SIZE OF SIGN SHALL BE BASED ON TYPE OF ROADWAY ON WHICH THE SIGN IS INSTALLED.
5. SIGNS NO LONGER IN USE SHALL BE COMPLETELY COVERED WITH NO RETROREFLECTIVE MATERIAL SHOWING, OR SHALL BE REMOVED, AS DIRECTED BY THE ENGINEER.
6. FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD) WILL PREVAIL.
7. SIGNS "N" THROUGH "Q" AND "T" AND "V", THE WORD "ROAD" SHOULD BE CHANGED TO "RAMP", "RR XING", OR "BRIDGE" WHERE APPLICABLE.
8. WARNING SIGNS AND DETOUR TRAILBLAZERS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT ORANGE SHEETING.
9. "W" BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF THE ROADWAY.
10. BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED _____ DATE: _____

RECOMMENDED _____ DATE: _____

RECOMMENDED *[Signature]* DATE: 7/16/15

APPROVED CHIEF SAFETY OFFICER *[Signature]* DATE: 7/16/15

APPROVED TRAFFIC ENGINEER *[Signature]* DATE: 7/16/15



ADDENDUM / REVISIONS

NOT TO SCALE

SR 72 and Old Baltimore Pike

CONTRACT
T201200108
COUNTY
New Castle

PERMIT NO.
DESIGNED BY: Joe McMahon
CHECKED BY: PH

VEHICULAR DETOUR PLAN
for Old Baltimore Pike
EAST of SR 72

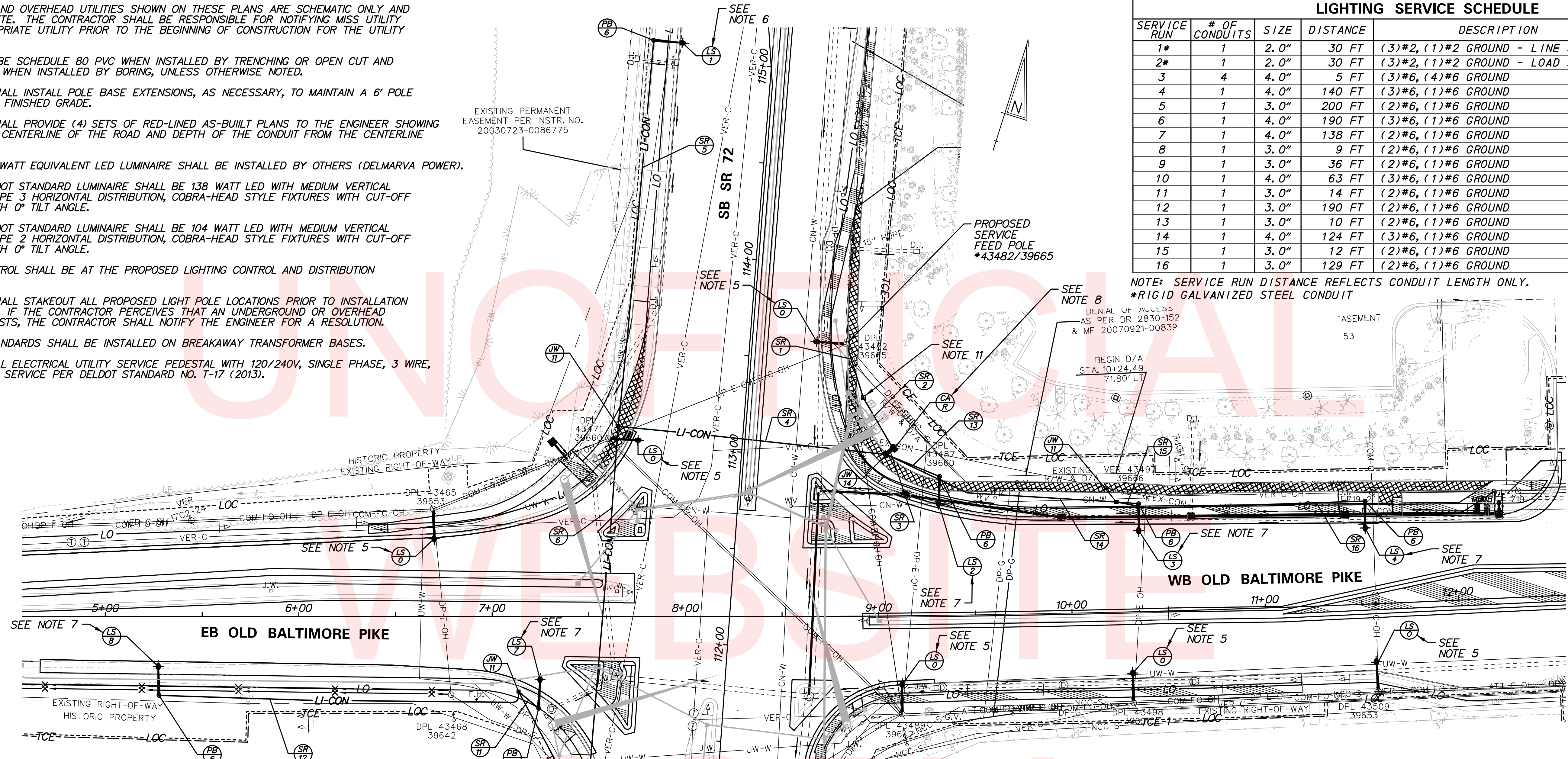
SHEET NO.
38
TOTAL SHTS.
52

NOTES:

- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS.
- ALL CONDUIT SHALL BE SCHEDULE 80 PVC WHEN INSTALLED BY TRENCHING OR OPEN CUT AND SCHEDULE 80 HDPE WHEN INSTALLED BY BORING, UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL INSTALL POLE BASE EXTENSIONS, AS NECESSARY, TO MAINTAIN A 6' POLE BASE DEPTH BELOW FINISHED GRADE.
- THE CONTRACTOR SHALL PROVIDE (4) SETS OF RED-LINED AS-BUILT PLANS TO THE ENGINEER SHOWING OFFSETS FROM THE CENTERLINE OF THE ROAD AND DEPTH OF THE CONDUIT FROM THE CENTERLINE OF THE ROAD.
- THE PROPOSED 150 WATT EQUIVALENT LED LUMINAIRE SHALL BE INSTALLED BY OTHERS (DELMARVA POWER).
- THE PROPOSED DELDOT STANDARD LUMINAIRE SHALL BE 138 WATT LED WITH MEDIUM VERTICAL DISTRIBUTION, IES TYPE 3 HORIZONTAL DISTRIBUTION, COBRA-HEAD STYLE FIXTURES WITH CUT-OFF OPTICS MOUNTED WITH 0° TILT ANGLE.
- THE PROPOSED DELDOT STANDARD LUMINAIRE SHALL BE 104 WATT LED WITH MEDIUM VERTICAL DISTRIBUTION, IES TYPE 2 HORIZONTAL DISTRIBUTION, COBRA-HEAD STYLE FIXTURES WITH CUT-OFF OPTICS MOUNTED WITH 0° TILT ANGLE.
- PHOTOELECTRIC CONTROL SHALL BE AT THE PROPOSED LIGHTING CONTROL AND DISTRIBUTION ENCLOSURE.
- THE CONTRACTOR SHALL STAKEOUT ALL PROPOSED LIGHT POLE LOCATIONS PRIOR TO INSTALLATION OF THE POLE BASE. IF THE CONTRACTOR PERCEIVES THAT AN UNDERGROUND OR OVERHEAD UTILITY CONFLICT EXISTS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER FOR A RESOLUTION.
- DELDOT LIGHTING STANDARDS SHALL BE INSTALLED ON BREAKAWAY TRANSFORMER BASES.
- FURNISH AND INSTALL ELECTRICAL UTILITY SERVICE PEDESTAL WITH 120/240V, SINGLE PHASE, 3 WIRE, 100 AMP ELECTRICAL SERVICE PER DELDOT STANDARD NO. T-17 (2013).

LIGHTING SERVICE SCHEDULE					
SERVICE RUN	# OF CONDUITS	SIZE	DISTANCE	DESCRIPTION	INSTALLATION
1*	1	2.0"	30 FT	(3)#2, (1)#2 GROUND - LINE SIDE	TRENCH
2*	1	2.0"	30 FT	(3)#2, (1)#2 GROUND - LOAD SIDE	TRENCH
3	4	4.0"	5 FT	(3)#6, (4)#6 GROUND	TRENCH
4	1	4.0"	140 FT	(3)#6, (1)#6 GROUND	BORE
5	1	3.0"	200 FT	(2)#6, (1)#6 GROUND	TRENCH
6	1	4.0"	190 FT	(3)#6, (1)#6 GROUND	BORE
7	1	4.0"	138 FT	(2)#6, (1)#6 GROUND	BORE
8	1	3.0"	9 FT	(2)#6, (1)#6 GROUND	TRENCH
9	1	3.0"	36 FT	(2)#6, (1)#6 GROUND	TRENCH
10	1	4.0"	63 FT	(3)#6, (1)#6 GROUND	TRENCH / OPEN CUT
11	1	3.0"	14 FT	(2)#6, (1)#6 GROUND	TRENCH
12	1	3.0"	190 FT	(2)#6, (1)#6 GROUND	TRENCH
13	1	3.0"	10 FT	(2)#6, (1)#6 GROUND	TRENCH
14	1	4.0"	124 FT	(3)#6, (1)#6 GROUND	TRENCH
15	1	3.0"	12 FT	(2)#6, (1)#6 GROUND	TRENCH
16	1	3.0"	129 FT	(2)#6, (1)#6 GROUND	TRENCH

NOTE: SERVICE RUN DISTANCE REFLECTS CONDUIT LENGTH ONLY.
*RIGID GALVANIZED STEEL CONDUIT



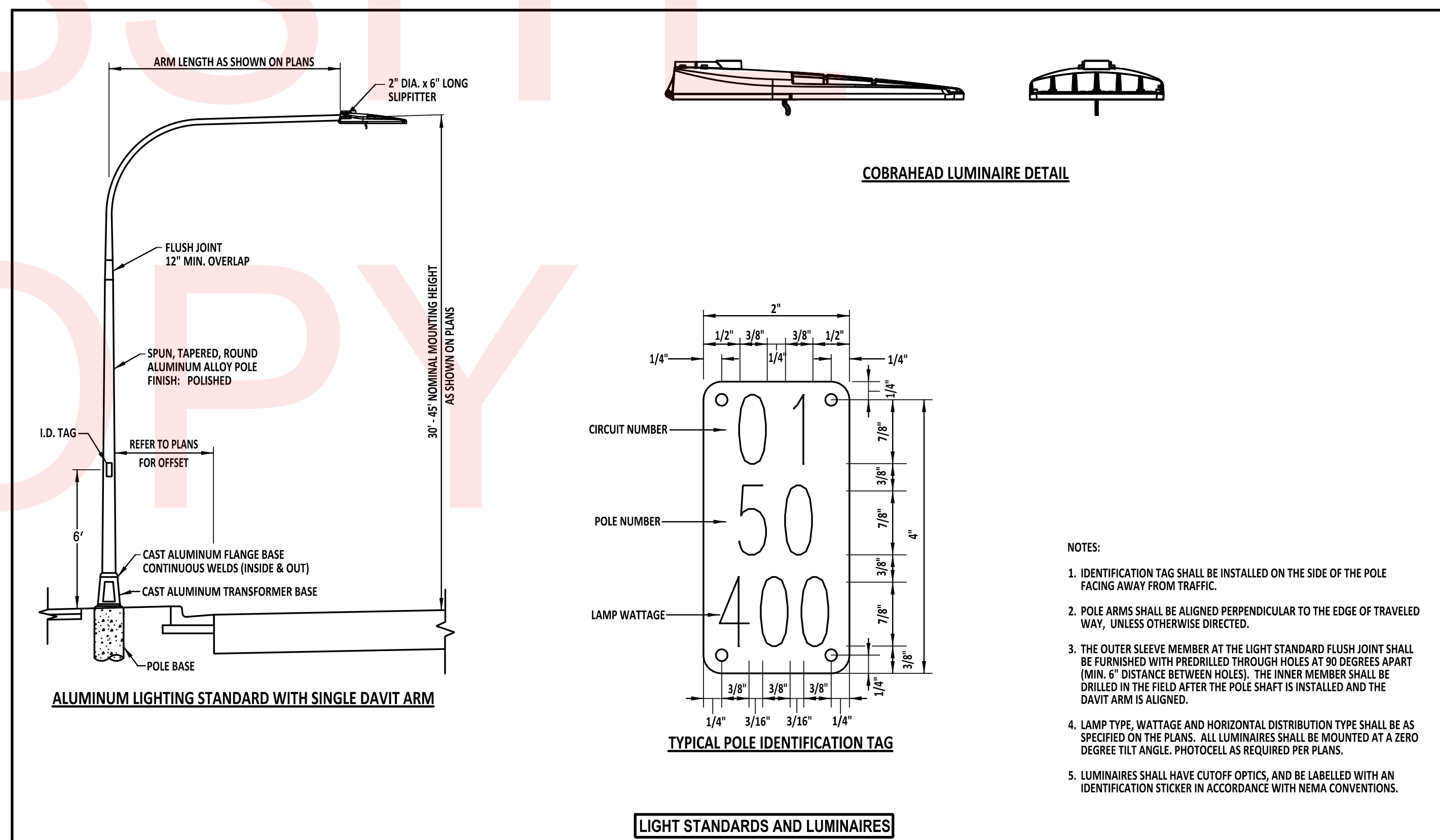
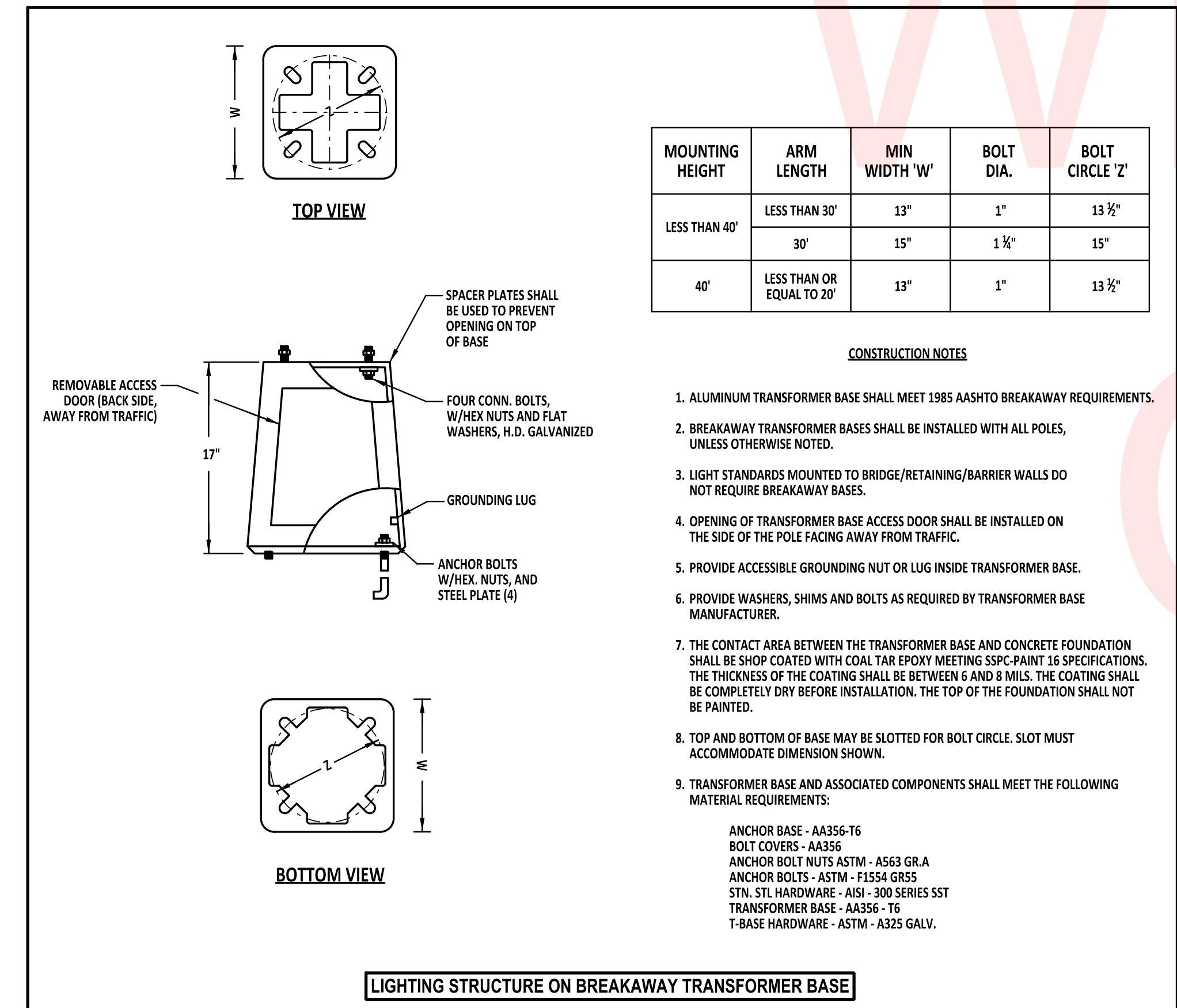
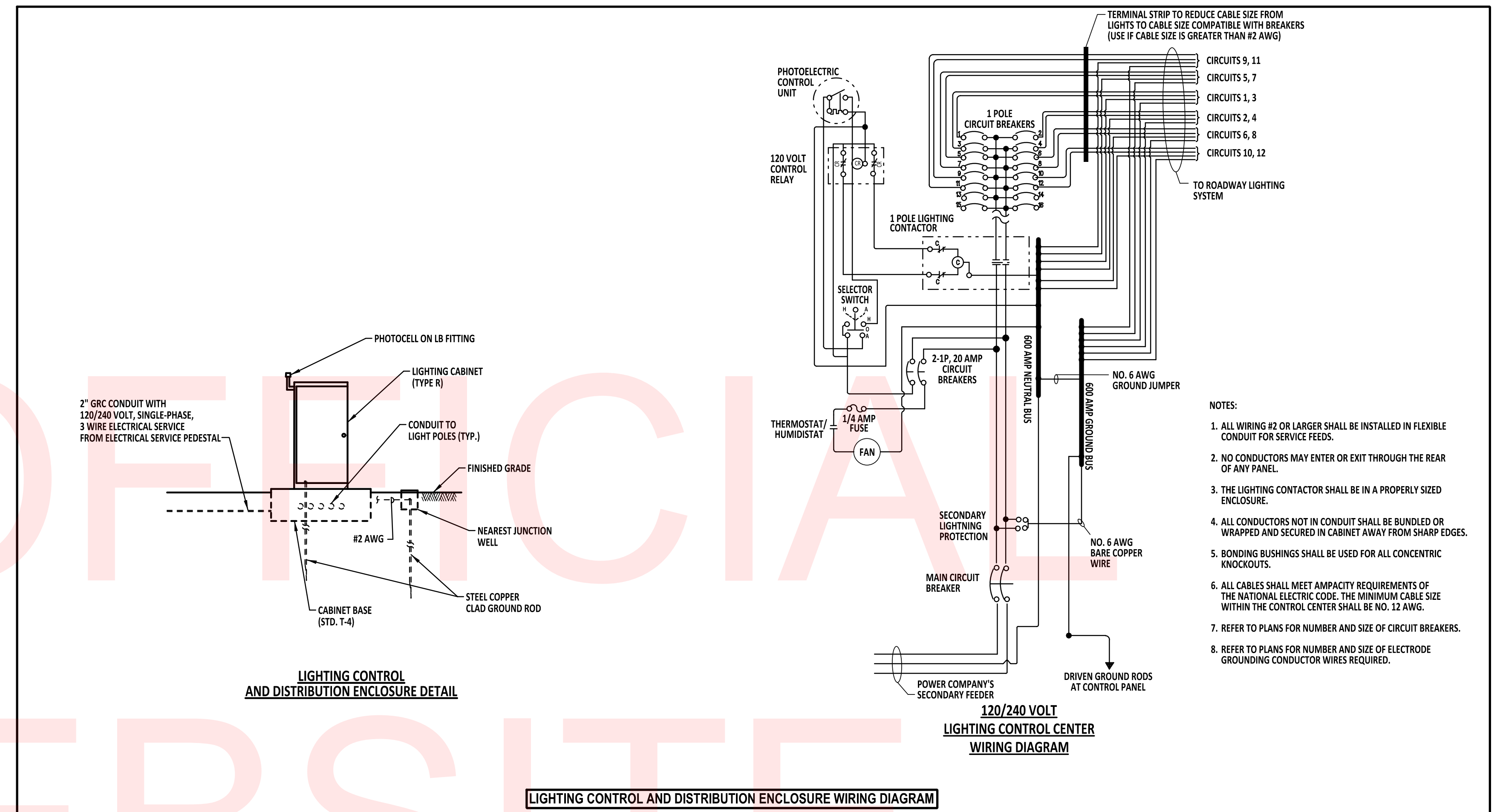
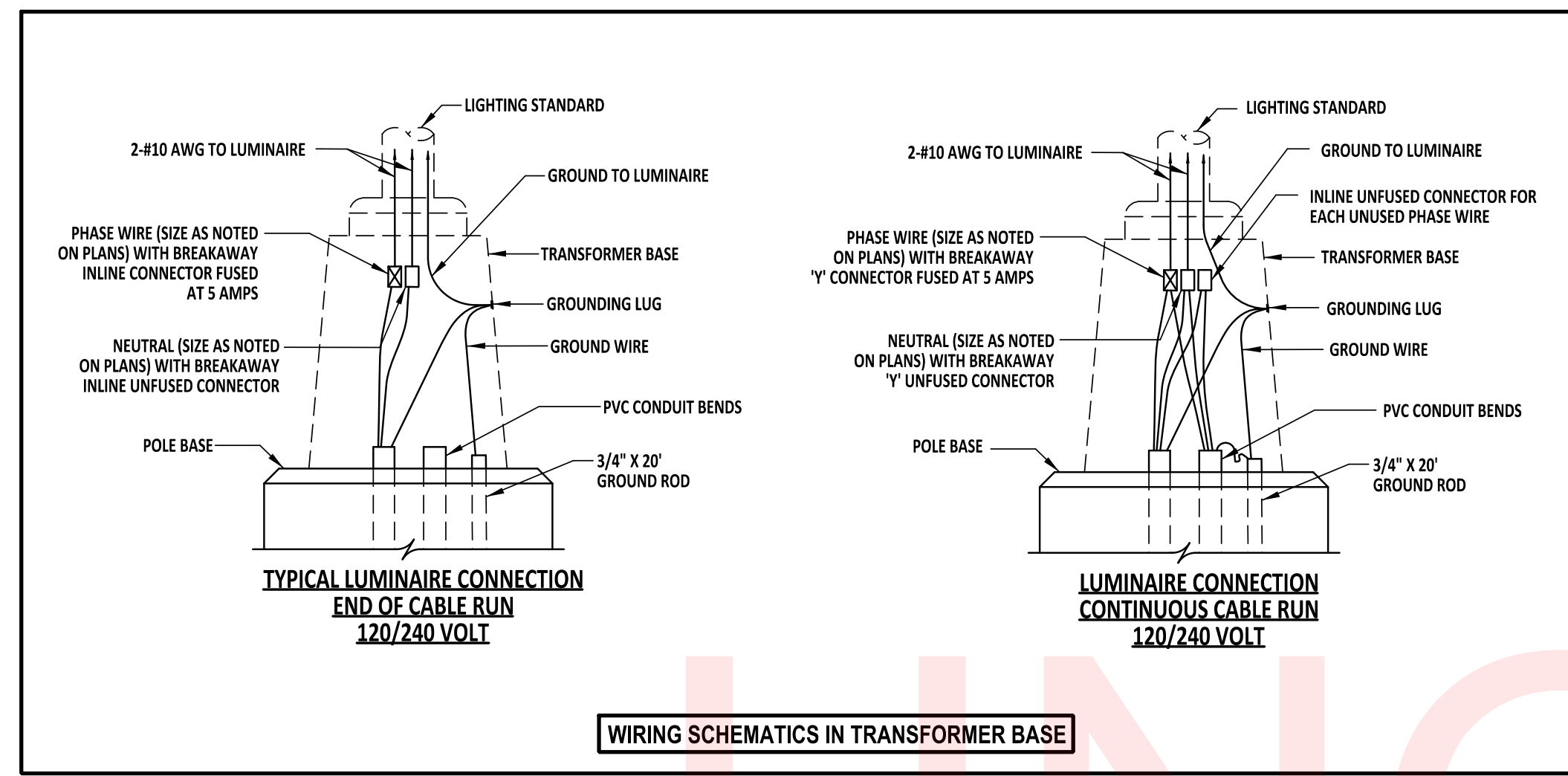
LIGHTING SYMBOL LEGEND	
SYMBOL	DESCRIPTION
	PROPOSED LIGHTING STANDARD AND POLE BASE
	PROPOSED LIGHTING CONTROL AND DISTRIBUTION ENCLOSURE AND IDENTIFIER
	PROPOSED LIGHTING JUNCTION WELL
	PROPOSED LIGHTING STANDARD IDENTIFIER
	PROPOSED LIGHTING SERVICE RUN
	PROPOSED SERVICE RUN IDENTIFIER
	PROPOSED LIGHTING JUNCTION WELL IDENTIFIER (TYPE)
	PROPOSED LIGHTING POLE BASE IDENTIFIER (TYPE)
	PROPOSED SERVICE PEDESTAL
	EXISTING UTILITY POLE
	EXISTING LIGHTING STANDARD

PANELBOARD SE					
AIC RATING - 22 KAIC		100 AMP BUS		100 AMP MCCB	
SOLID NEUTRAL		120/240 VOLTS		1PHASE, 3 WIRE + GROUND	
ENCLOSURE: BASE MOUNTED CABINET SURFACE MOUNTED					
LOAD SERVED	CIRCUIT BREAKER FRAME	TRIP	POLE	CKT. NO.	LOAD SERVED
4-104W LED	100	20	I	1 2	SPARE
3-104W LED & 1-138W LED	100	20	I	3 4	SPARE
SPARE	100	20	I	5 6	SPARE
SPARE	100	20	I	7 8	SPARE
SPARE	100	20	I	9 10	SPARE

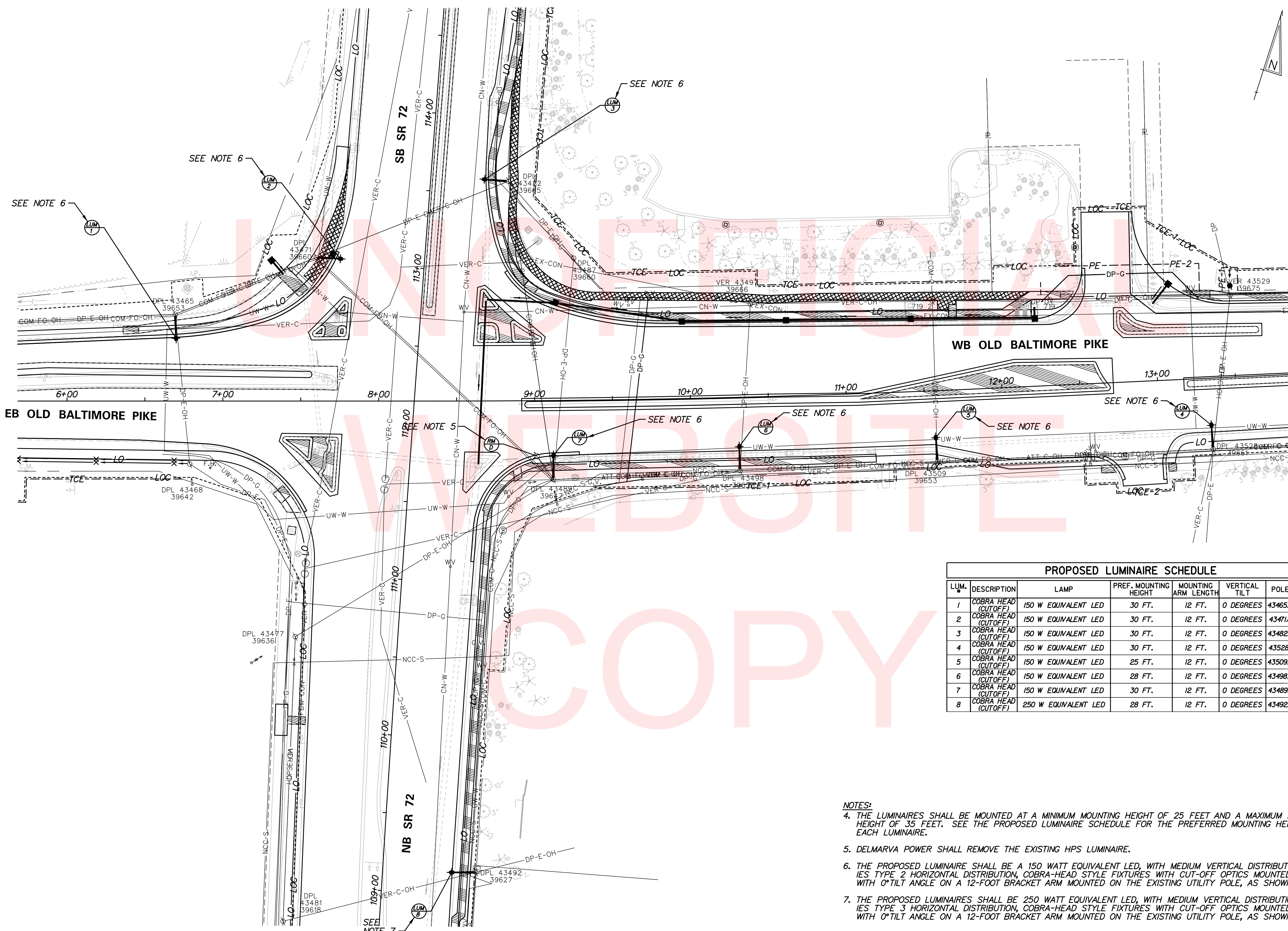
LIGHTING STANDARD SCHEDULE						
NO.	CIRCUIT NO.	STATION	OFFSET	HEIGHT	ARM	LIGHT STANDARD
LS-1	3	115+08.8	61.8' LT.	30 FT	12 FT	138 W LED, IES TYPE 3 DISTRIBUTION
LS-2	1	9+31.4	71.9' LT.	30 FT	12 FT	104 W LED, IES TYPE 2 DISTRIBUTION
LS-3	3	10+34.4	55.9' LT.	30 FT	12 FT	104 W LED, IES TYPE 2 DISTRIBUTION
LS-4	1	11+54.1	53.3' LT.	30 FT	12 FT	104 W LED, IES TYPE 2 DISTRIBUTION
LS-5	3	110+70.0	63.7' RT.	30 FT	12 FT	104 W LED, IES TYPE 2 DISTRIBUTION
LS-6	1	110+84.8	64.1' LT.	30 FT	12 FT	104 W LED, IES TYPE 2 DISTRIBUTION
LS-7	3	7+24.0	47.4' RT.	30 FT	12 FT	104 W LED, IES TYPE 2 DISTRIBUTION
LS-8	1	5+28.1	40.8' RT.	30 FT	12 FT	104 W LED, IES TYPE 2 DISTRIBUTION

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DELAWARE DEPARTMENT OF TRANSPORTATION	ADDENDUM / REVISIONS	SCALE 0 30 60 90 FEET	HEP NCC, SR 72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS	CONTRACT T201200108	PERMIT NO. NA	LIGHTING PLAN	SHEET NO. 39
				COUNTY NEW CASTLE	DESIGNED BY: D.W.C. (WRA)		CHECKED BY: M.J.B. (WRA)



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LEGEND	
(RM C)	REMOVE BY CONTRACTOR
(RM D)	REMOVE BY DELMARVA POWER
(AB)	ABANDON
(PB X)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(PB Y)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(JW X)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
(JW Y)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
(CO X)	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
(CO Y)	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
(OH X)	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(OH Y)	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(MA XX)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
(MA YY)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
(CA X)	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
(CA Y)	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
(LS X)	PROPOSED LIGHTING STANDARD IDENTIFIER (* OF LIGHTING SERVICE)
(---)	PROPOSED SPAN WIRE
(---XX---	EXISTING SPAN WIRE
(---)	RIGHT-OF-WAY OR PROPERTY LINE
(P)	SERVICE PEDESTAL
(---)	PROPOSED LIGHTING STANDARD
(---)	EXISTING LIGHTING STANDARD
(■)	PROPOSED SIGNAL CABINET
(□)	EXISTING SIGNAL CABINET
(○)	PROPOSED SIGNAL POLE BASE
(⊙)	EXISTING SIGNAL POLE BASE
(⊗)	PROPOSED PEDESTRIAN POLE BASE
(⊕)	EXISTING PEDESTRIAN POLE BASE
(■)	PROPOSED WOOD POLE
(⊗)	EXISTING UTILITY POLE
(■)	PROPOSED JUNCTION WELL
(J.W.)	EXISTING JUNCTION WELL
(→)	PROPOSED SIGNAL HEAD
(→)	EXISTING SIGNAL HEAD

PROPOSED LUMINAIRE SCHEDULE						
LUM.	DESCRIPTION	LAMP	PREF. MOUNTING HEIGHT	MOUNTING ARM LENGTH	VERTICAL TILT	POLE NO.
1	COBRA HEAD (CUTOFF)	150 W EQUIVALENT LED	30 FT.	12 FT.	0 DEGREES	43465/39653
2	COBRA HEAD (CUTOFF)	150 W EQUIVALENT LED	30 FT.	12 FT.	0 DEGREES	43471/39660
3	COBRA HEAD (CUTOFF)	150 W EQUIVALENT LED	30 FT.	12 FT.	0 DEGREES	43482/39685
4	COBRA HEAD (CUTOFF)	150 W EQUIVALENT LED	30 FT.	12 FT.	0 DEGREES	43528/39661
5	COBRA HEAD (CUTOFF)	150 W EQUIVALENT LED	25 FT.	12 FT.	0 DEGREES	43509/39653
6	COBRA HEAD (CUTOFF)	150 W EQUIVALENT LED	28 FT.	12 FT.	0 DEGREES	43498/39650
7	COBRA HEAD (CUTOFF)	150 W EQUIVALENT LED	30 FT.	12 FT.	0 DEGREES	43489/39647
8	COBRA HEAD (CUTOFF)	250 W EQUIVALENT LED	28 FT.	12 FT.	0 DEGREES	43492/39627

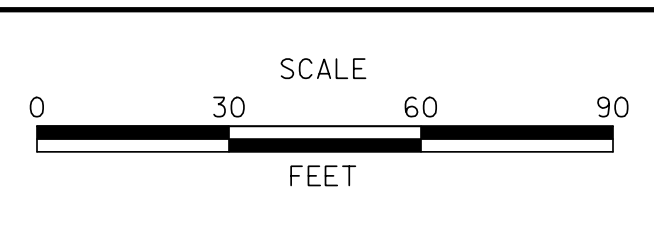
- NOTES:**
- THE LUMINAIRES SHALL BE MOUNTED AT A MINIMUM MOUNTING HEIGHT OF 25 FEET AND A MAXIMUM MOUNTING HEIGHT OF 35 FEET. SEE THE PROPOSED LUMINAIRE SCHEDULE FOR THE PREFERRED MOUNTING HEIGHT OF EACH LUMINAIRE.
 - DELMARVA POWER SHALL REMOVE THE EXISTING HPS LUMINAIRE.
 - THE PROPOSED LUMINAIRE SHALL BE A 150 WATT EQUIVALENT LED, WITH MEDIUM VERTICAL DISTRIBUTION, IES TYPE 2 HORIZONTAL DISTRIBUTION, COBRA-HEAD STYLE FIXTURES WITH CUT-OFF OPTICS MOUNTED WITH 0° TILT ANGLE ON A 12-FOOT BRACKET ARM MOUNTED ON THE EXISTING UTILITY POLE, AS SHOWN.
 - THE PROPOSED LUMINAIRE SHALL BE 250 WATT EQUIVALENT LED, WITH MEDIUM VERTICAL DISTRIBUTION, IES TYPE 3 HORIZONTAL DISTRIBUTION, COBRA-HEAD STYLE FIXTURES WITH CUT-OFF OPTICS MOUNTED WITH 0° TILT ANGLE ON A 12-FOOT BRACKET ARM MOUNTED ON THE EXISTING UTILITY POLE, AS SHOWN.

- GENERAL NOTES**
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS.
 - LUMINAIRE AND MOUNTING BRACKET ARM TO BE INSTALLED, OWNED, AND MAINTAINED BY UTILITY COMPANY.
 - DELMARVA POWER CONSTRUCTION ACTIVITIES PERFORMED OUTSIDE DELDOT'S RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH DELMARVA POWER POLICIES AND STANDARDS FOR WORKING WITHIN PRIVATE UTILITY EASEMENTS.

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ADDENDUM / REVISIONS



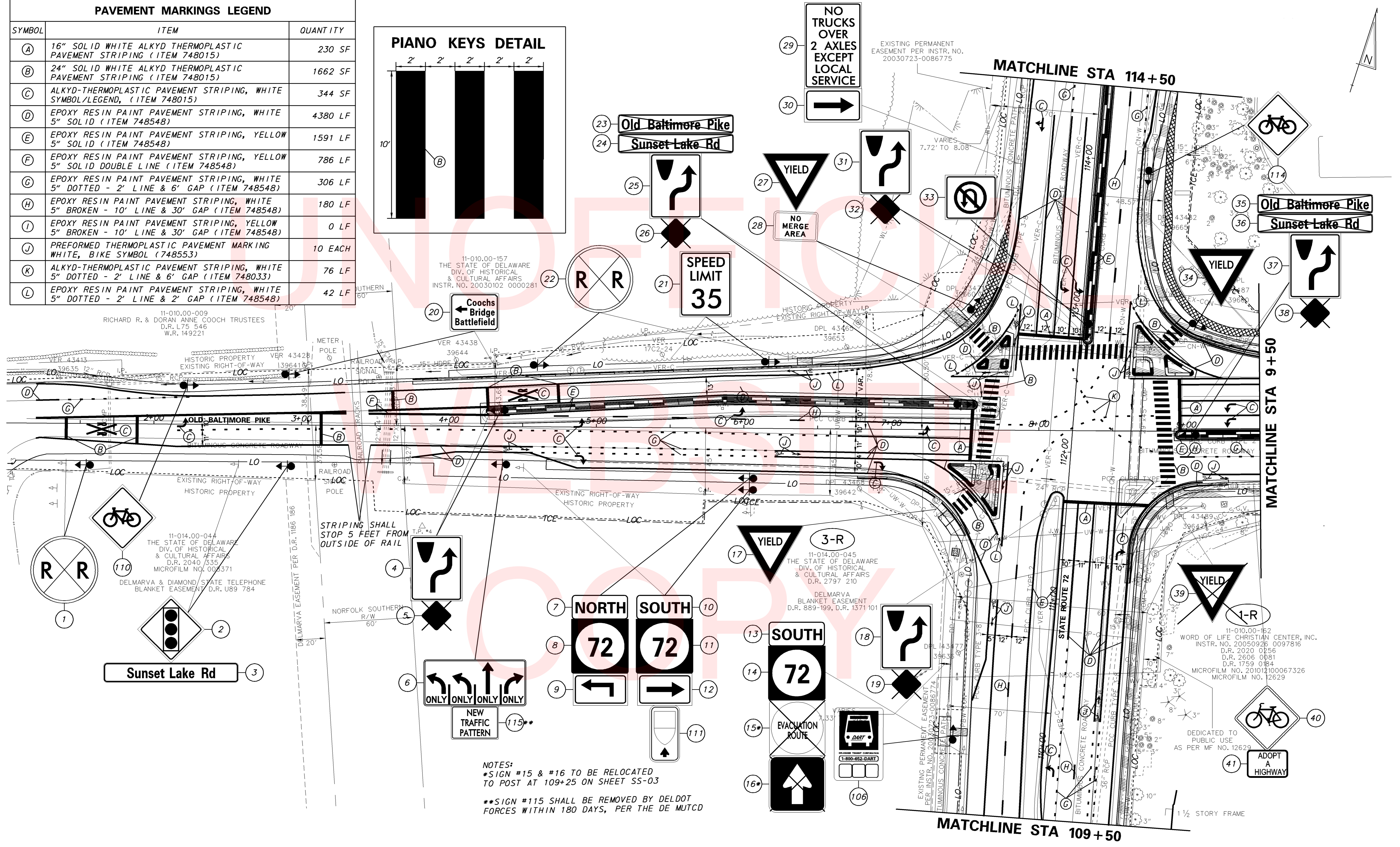
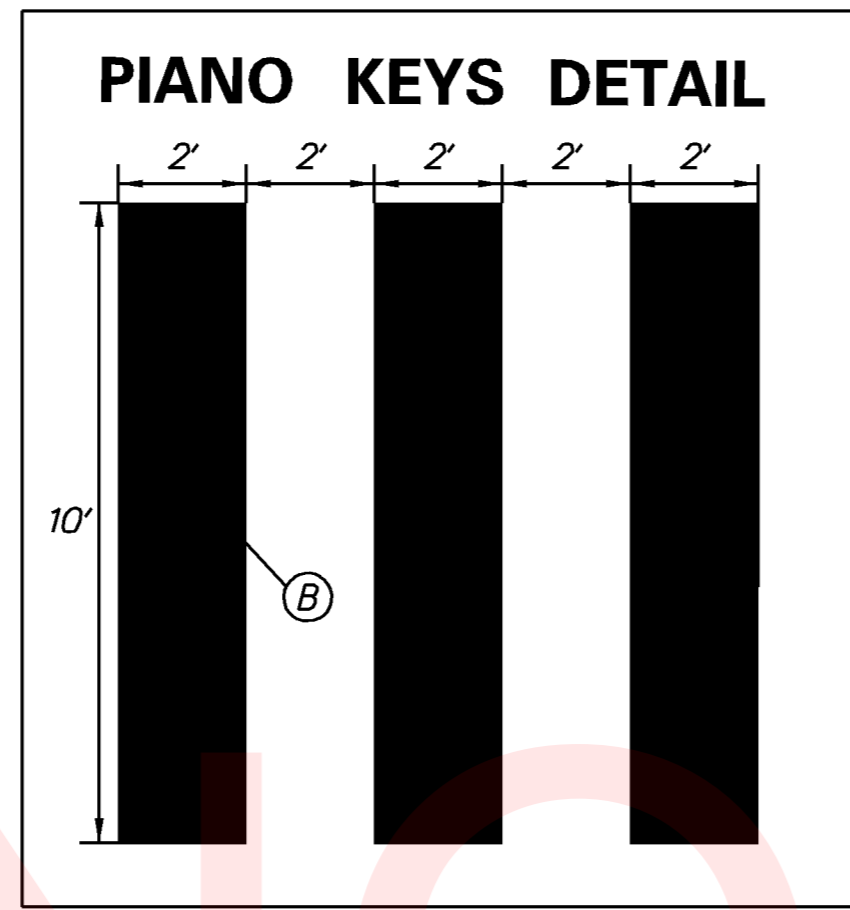
**HEP NCC,
 SR 72 AND OLD BALTIMORE PIKE
 INTERSECTION IMPROVEMENTS**

CONTRACT	PERMIT NO.	N/A
T201200108	DESIGNED BY: D.W.C. (WRA)	
COUNTY	CHECKED BY: M.J.B. (WRA)	
NEW CASTLE		

LEASED LIGHTING PLAN		SHEET NO.
		41
		TOTAL SHTS.
		52

PAVEMENT MARKINGS LEGEND

SYMBOL	ITEM	QUANTITY
(A)	16" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	230 SF
(B)	24" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	1662 SF
(C)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE SYMBOL/LEGEND, (ITEM 748015)	344 SF
(D)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" SOLID (ITEM 748548)	4380 LF
(E)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID (ITEM 748548)	1591 LF
(F)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID DOUBLE LINE (ITEM 748548)	786 LF
(G)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748548)	306 LF
(H)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" BROKEN - 10' LINE & 30' GAP (ITEM 748548)	180 LF
(I)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" BROKEN - 10' LINE & 30' GAP (ITEM 748548)	0 LF
(J)	PREFORMED THERMOPLASTIC PAVEMENT MARKING WHITE, BIKE SYMBOL (748553)	10 EACH
(K)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748033)	76 LF
(L)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 2' GAP (ITEM 748548)	42 LF



11-010.00-157
THE STATE OF DELAWARE
DIV. OF HISTORICAL
& CULTURAL AFFAIRS
INSTR. NO. 20030102 0000281

11-010.00-009
RICHARD R. & DORAN ANNE COOCH TRUSTEES
D.R. 175 546
W.R. 149221

11-014.00-044
THE STATE OF DELAWARE
DIV. OF HISTORICAL
& CULTURAL AFFAIRS
D.R. 2040 335
MICROFILM NO. 003371

DELMARVA & DIAMOND STATE TELEPHONE
BLANKET EASEMENT D.R. U89 784

11-014.00-045
THE STATE OF DELAWARE
DIV. OF HISTORICAL
& CULTURAL AFFAIRS
D.R. 2797 210

DELMARVA
BLANKET EASEMENT
D.R. 889-199, D.R. 1371 101

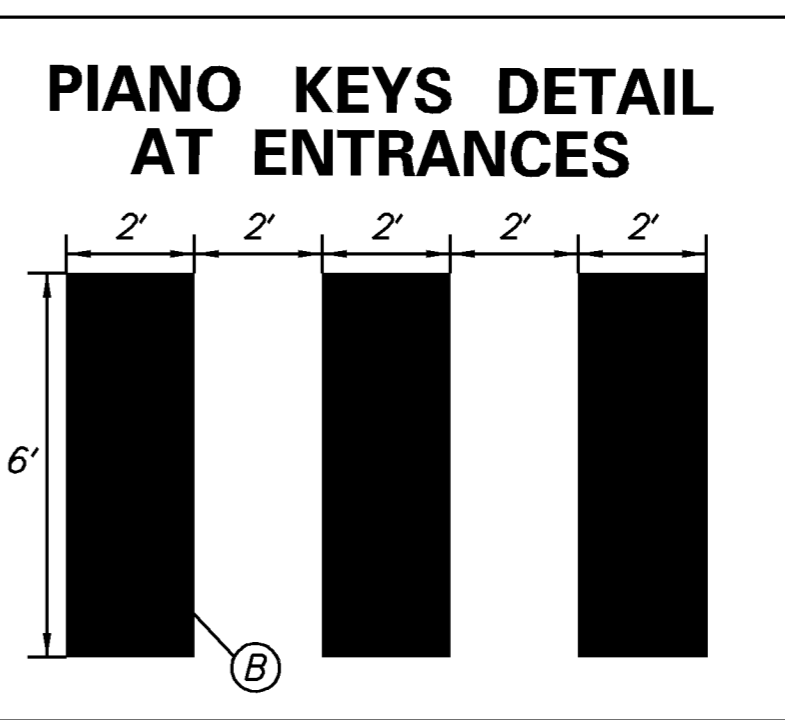
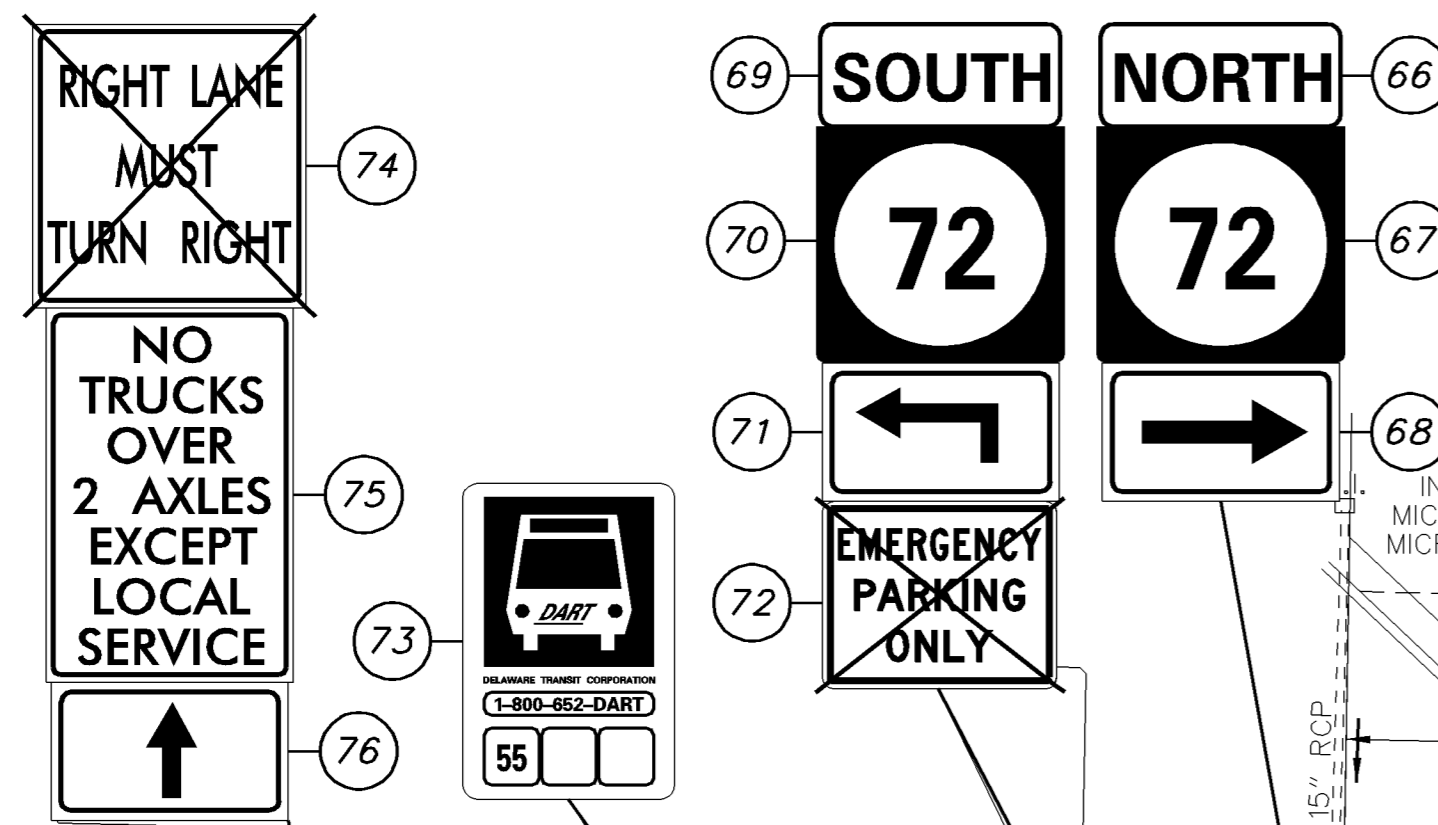
11-010.00-162
WORD OF LIFE CHRISTIAN CENTER, INC.
INSTR. NO. 20050926 0097816
D.R. 2020 0256
D.R. 2606 0881
D.R. 1759 0184
MICROFILM NO. 201012100067326
MICROFILM NO. 12629

DEDICATED TO
PUBLIC USE
AS PER MF NO. 12629

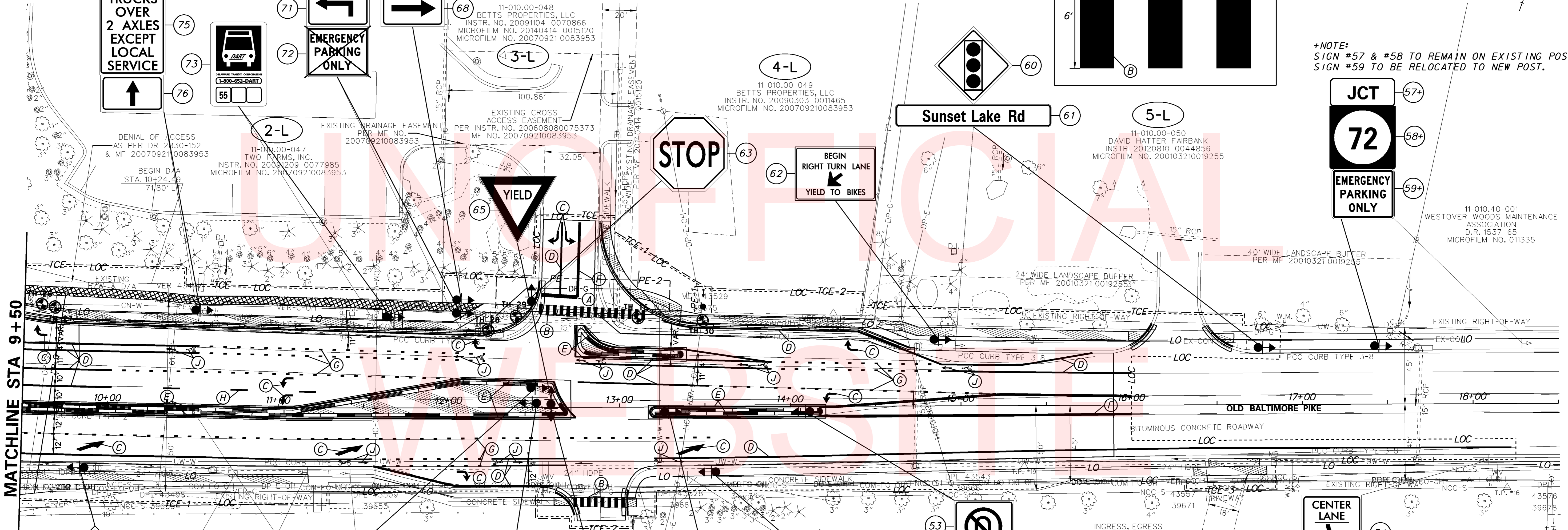
NOTES:
*SIGN #15 & #16 TO BE RELOCATED
TO POST AT 109+25 ON SHEET SS-03

**SIGN #115 SHALL BE REMOVED BY DELDOT
FORCES WITHIN 180 DAYS, PER THE DE MUTCD

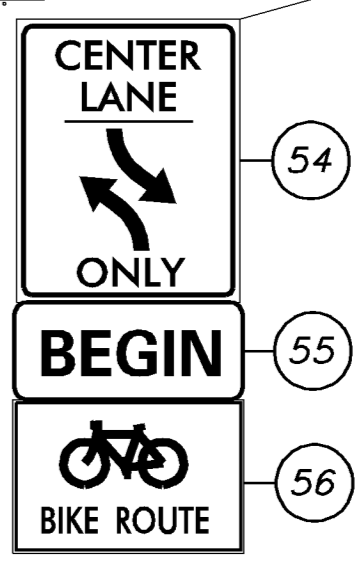
W:\MSVB\CELLS\PROJDEV\SB.CEL



+NOTE:
SIGN #57 & #58 TO REMAIN ON EXISTING POST.
SIGN #59 TO BE RELOCATED TO NEW POST.



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	16" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	29 SF
(B)	24" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	288 SF
(C)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE SYMBOL/LEGEND, (ITEM 748015)	254 SF
(D)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" SOLID (ITEM 748548)	2214 LF
(E)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID (ITEM 748548)	971 LF
(F)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID DOUBLE LINE (ITEM 748548)	959 LF
(G)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748548)	370 LF
(H)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" BROKEN - 10' LINE & 30' GAP (ITEM 748548)	40 LF
(I)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" BROKEN - 10' LINE & 30' GAP (ITEM 748548)	0 LF
(J)	PERFORMED THERMOPLASTIC PAVEMENT MARKING WHITE, BIKE SYMBOL (748553)	8 EACH
(K)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748033)	0 LF
(L)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 2' GAP (ITEM 748548)	0 LF



11-010.00-162
WORD OF LIFE CHRISTIAN CENTER, INC.
INSTR. NO. 20050926 0097816
D.R. 2020 0256
D.R. 2606 0081
D.R. 1759 0184
MICROFILM NO. 201012100067326
MICROFILM NO. 12629

INGRESS, EGRESS & REGRESS EASEMENT
PER MF NO. 201012100067326



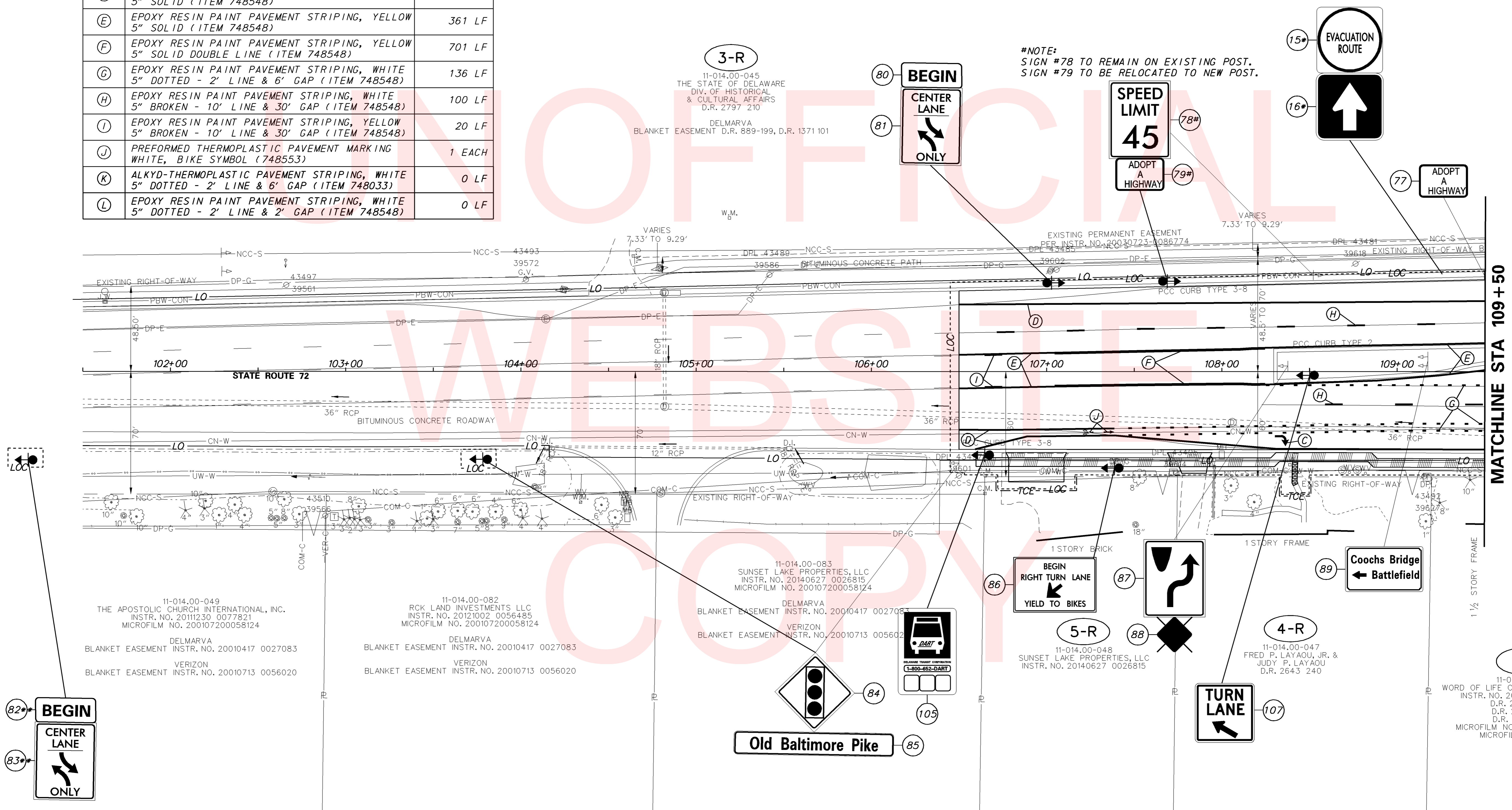
PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	16" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	0 SF
(B)	24" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	0 SF
(C)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE SYMBOL/LEGEND, (ITEM 748015)	15.5 SF
(D)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" SOLID (ITEM 748548)	681 LF
(E)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID (ITEM 748548)	361 LF
(F)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID DOUBLE LINE (ITEM 748548)	701 LF
(G)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748548)	136 LF
(H)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" BROKEN - 10' LINE & 30' GAP (ITEM 748548)	100 LF
(I)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" BROKEN - 10' LINE & 30' GAP (ITEM 748548)	20 LF
(J)	PREFORMED THERMOPLASTIC PAVEMENT MARKING WHITE, BIKE SYMBOL (748553)	1 EACH
(K)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748033)	0 LF
(L)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 2' GAP (ITEM 748548)	0 LF

*NOTE:
SIGN #15 & #16 TO BE RELOCATED
FROM POST AT 109+90 ON SHEET SS-01

3-R
11-014.00-045
THE STATE OF DELAWARE
DIV. OF HISTORICAL
& CULTURAL AFFAIRS
D.R. 2797 210

DELMARVA
BLANKET EASEMENT D.R. 889-199, D.R. 1371 101

*NOTE:
SIGN #78 TO REMAIN ON EXISTING POST.
SIGN #79 TO BE RELOCATED TO NEW POST.



11-014.00-049
THE APOSTOLIC CHURCH INTERNATIONAL, INC.
INSTR. NO. 20111230 0077821
MICROFILM NO. 200107200058124

DELMARVA
BLANKET EASEMENT INSTR. NO. 20010417 0027083

VERIZON
BLANKET EASEMENT INSTR. NO. 20010713 0056020

11-014.00-082
RCK LAND INVESTMENTS LLC
INSTR. NO. 20121002 0056485
MICROFILM NO. 200107200058124

DELMARVA
BLANKET EASEMENT INSTR. NO. 20010417 0027083

VERIZON
BLANKET EASEMENT INSTR. NO. 20010713 0056020

11-014.00-083
SUNSET LAKE PROPERTIES, LLC
INSTR. NO. 20140627 0026815
MICROFILM NO. 200107200058124

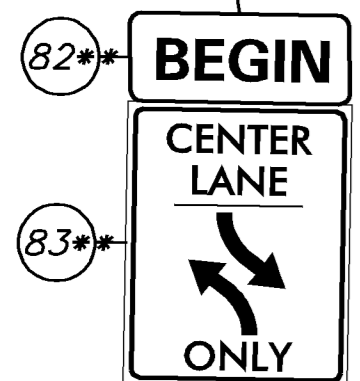
DELMARVA
BLANKET EASEMENT INSTR. NO. 20010417 0027083

VERIZON
BLANKET EASEMENT INSTR. NO. 20010713 0056020

11-014.00-048
SUNSET LAKE PROPERTIES, LLC
INSTR. NO. 20140627 0026815

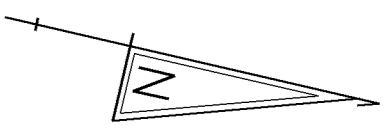
11-014.00-047
FRED P. LAYAOU, JR. &
JUDY P. LAYAOU
D.R. 2643 240

1-R
11-010.00-162
WORD OF LIFE CHRISTIAN CENTER, INC.
INSTR. NO. 20050926 0097816
D.R. 2020 0256
D.R. 2606 0081
D.R. 1759 0184
MICROFILM NO. 201012100067326
MICROFILM NO. 12629

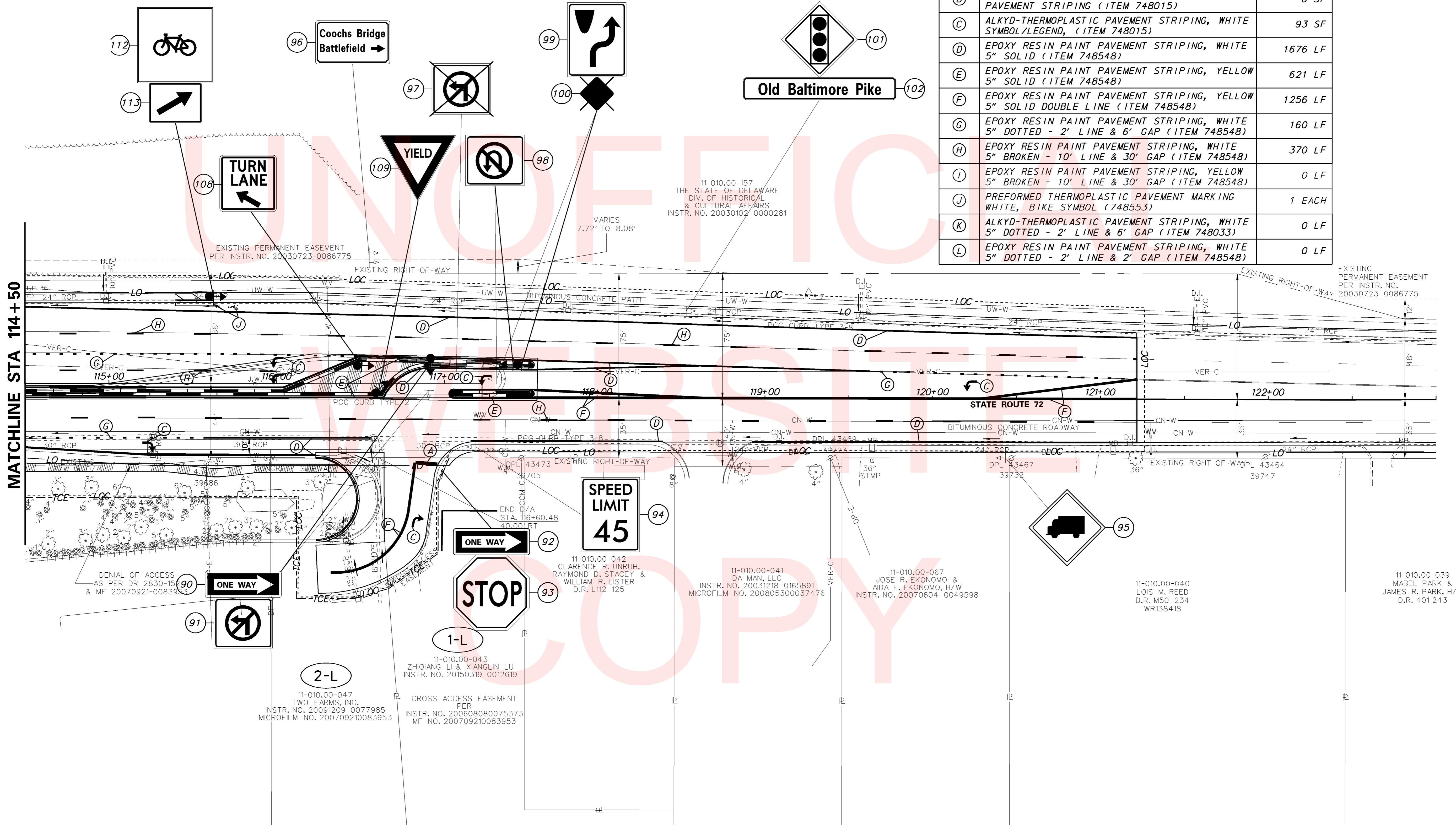


**NOTE:
SIGN #82 & #83 TO BE INSTALLED
AT BEGINNING OF EXISTING
TWO-WAY LEFT-TURN LANE,
SOUTH OF SITE, ACCORDING TO
FIGURE 3B-7 OF THE DEMUTCD.

W:\MS\8\CELLS\PROVIDE\SB.CEL



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	16" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	18 SF
(B)	24" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	0 SF
(C)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE SYMBOL/LEGEND, (ITEM 748015)	93 SF
(D)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" SOLID (ITEM 748548)	1676 LF
(E)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID (ITEM 748548)	621 LF
(F)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID DOUBLE LINE (ITEM 748548)	1256 LF
(G)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748548)	160 LF
(H)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" BROKEN - 10' LINE & 30' GAP (ITEM 748548)	370 LF
(I)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" BROKEN - 10' LINE & 30' GAP (ITEM 748548)	0 LF
(J)	PREFORMED THERMOPLASTIC PAVEMENT MARKING WHITE, BIKE SYMBOL (748553)	1 EACH
(K)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748033)	0 LF
(L)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 2' GAP (ITEM 748548)	0 LF



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PERMANENT SIGN SCHEDULE

#	SHEET NO.	PLAN IDENTIFIER	SIGN DESIGNATION	QTY.	DESCRIPTION	SIGN WIDTH (IN)	SIGN HEIGHT (IN)	SIGN AREA (SF)	ITEM 749687 SINGLE POST (EACH)			ITEM 749690 INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON MULTIPLE SIGN POSTS (SF)			POST INSTALLATION TYPE	Code X11 12' Post (W/ Basepost)	ITEM 749688 4" HOLE, 0-6" (EACH)	ITEM 749689 4" HOLE, >6" (EACH)	REMARKS
									SIGN DISPOSITION	REMOVE	INSTALL	SIGN DISPOSITION	REMOVE	INSTALL					
1124	SS-1	1	W10-1(36)	1	HIGHWAY-RAIL GRADE CROSSING ADVANCE WARNING 36 Inch	36	1		REPOSITION	1	1				SOIL	1			
	SS-1	2	W3-3(36)	1	SIGNAL AHEAD (Symbol) - 36"x36"	36"	36"		REPOSITION	1	1				SOIL	1			
1367	SS-1	3	W16-8P	1	ADVANCE STREET NAME (1 - line plaque)	42"	9"		REPOSITION	1	1				SOIL	1			Sunset Lake Road
310	SS-1	4	R4-7(18)	1	KEEP RIGHT (Symbol) - 18x24	18"	24"		REPOSITION	1	1				CONCRETE	1			
1955	SS-1	5	OM1-3	1	TYPE 1 OBJECT MARKER	18"	18"		REMOVE	1					CONCRETE	1			
	SS-1	6	R3-8 CUSTOM	1	ADVANCE INTERSECTION LANE CONTROL (Left, Left Ahead, Right) - 72"x30"	72"	30"	15.0				NEW		15.0	SOIL	2			
1508	SS-1	7	M3-1(24)	1	CARDINAL DIRECTION - NORTH - 24x12	24"	12"	2.0	NEW		1				SOIL	1			
1488	SS-1	8	M1-5(24)	1	STATE ROUTE (2 - Digit Sign) - 24x24	24"	24"	4.0	NEW		1				SOIL	1			
1589	SS-1	9	M5-1 L	1	ADVANCE TURN ARROW (Left - 90)	21"	15"	2.2	NEW		1				SOIL	1			
1514	SS-1	10	M3-3(24)	1	CARDINAL DIRECTION - SOUTH - 24x12	24"	12"		REPOSITION	1	1				SOIL	1			
1488	SS-1	11	M1-5(24)	1	STATE ROUTE (2 - Digit Sign) - 24x24	24"	24"		REPOSITION	1	1				SOIL	1			
1604	SS-1	12	M6-1_R(21)	1	DIRECTIONAL ARROW (Right) - 21x15	21"	15"		REPOSITION	1	1				SOIL	1			
1514	SS-1	13	M3-3(24)	1	CARDINAL DIRECTION - SOUTH - 24x12	24"	12"		REMAIN						SOIL	1			
1488	SS-1	14	M1-5(24)	1	STATE ROUTE (2 - Digit Sign) - 24x24	24"	24"		REMAIN						SOIL	1			
1924	SS-1	15	EM-1-DE	1	EVACUATION ROUTE	24"	24"		REMOVE	1					SOIL	1			
1618	SS-1	16	M6-3(B)	1	DIRECTIONAL ARROW (Up) - 12x9	12"	9"		REMOVE	1					SOIL	1			
8	SS-1	17	R1-2(36)	1	YIELD	36"	36"		REMAIN						SOIL	1			
310	SS-1	18	R4-7(18)	1	KEEP RIGHT (Symbol) - 18x24	18"	24"		REMAIN						SOIL	1			
1955	SS-1	19	OM1-3	1	TYPE 1 OBJECT MARKER	18"	18"		REMOVE	1					SOIL	1			
1660	SS-1	20	D1-3	1	DESTINATION (3 Line)	0	42"		REPOSITION	1	1				SOIL	1			(Left Arrow) Coochs Bridge Battlefield
50	SS-1	21	R2-1-35(24)	1	SPEED LIMIT (35 MPH - 24x30)	24"	30"		REPOSITION	1	1				SOIL	1			
1124	SS-1	22	W10-1(36)	1	HIGHWAY-RAIL GRADE CROSSING ADVANCE WARNING 36 Inch	36	1		REPOSITION	1	1				SOIL	1			
1673	SS-1	23	D3-1(9)	1	STREET NAME (1 Line)	0	9"		REMOVE	1					CONCRETE	1			Old Baltimore Pike
1673	SS-1	24	D3-1(9)	1	STREET NAME (1 Line)	0	9"		REMOVE	1					CONCRETE	1			Sunset Lake Road
310	SS-1	25	R4-7(18)	1	KEEP RIGHT (Symbol) - 18x24	18"	24"		REPOSITION	1	1				CONCRETE	1			
1955	SS-1	26	OM1-3	1	TYPE 1 OBJECT MARKER	18"	18"		REMOVE	1					CONCRETE	1			
8	SS-1	27	R1-2(36)	1	YIELD	36"	36"		REMAIN						CONCRETE	1			
937	SS-1	28	W4-5P(18)	1	NO MERGE AREA (plaque) - 18x24	18"	24"		REMAIN						CONCRETE	1			
623	SS-1	29	R12-3-DE	1	NO TRUCKS OVER 2 AXLES EXCEPT LOCAL SERVICES	24"	36"		REMAIN						CONCRETE	1			
1604	SS-1	30	M6-1_R(21)	1	DIRECTIONAL ARROW (Right) - 21x15	21"	15"		REMAIN						CONCRETE	1	1		
310	SS-1	31	R4-7(18)	1	KEEP RIGHT (Symbol) - 18x24	18"	24"		REPOSITION	1	1				CONCRETE	1			
1955	SS-1	32	OM1-3	1	TYPE 1 OBJECT MARKER	18"	18"		REMOVE	1					CONCRETE	1			
120	SS-1	33	R3-4(24)	1	U - TURN PROHIBITION (Symbol) - 24x24	24"	24"		REPOSITION	1	1				CONCRETE	1			
8	SS-1	34	R1-2(36)	1	YIELD	36"	36"		REMAIN						CONCRETE	1			
1673	SS-1	35	D3-1(9)	1	STREET NAME (1 Line)	0	9"		REMOVE	1					CONCRETE	1			Old Baltimore Pike
1673	SS-1	36	D3-1(9)	1	STREET NAME (1 Line)	0	9"		REMOVE	1					CONCRETE	1			Sunset Lake Road
310	SS-1	37	R4-7(18)	1	KEEP RIGHT (Symbol) - 18x24	18"	24"		REPOSITION	1	1				CONCRETE	1			
1955	SS-1	38	OM1-3	1	TYPE 1 OBJECT MARKER	18"	18"		REMOVE	1					CONCRETE	1			
8	SS-1	39	R1-2(36)	1	YIELD	36"	36"		REMAIN						CONCRETE	1			
1165	SS-1	40	W11-1(36)	1	BICYCLE (Symbol) - 36x36	36"	36"		REMAIN						CONCRETE	1			
PAGE TOTALS								23	25	17	0	15	13	1	0				

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ADDENDUMS / REVISIONS

NOT TO SCALE

HEP NCC,
SR 72 AND OLD BALTIMORE PIKE
INTERSECTION IMPROVEMENTS

CONTRACT	BRIDGE NO.	N/A
T201200108	DESIGNED BY:	BCD
COUNTY	CHECKED BY:	MCN
NEW CASTLE		

SIGNING, STRIPING
AND CONDUIT PLAN

SHEET NO.	46
TOTAL SHTS.	52

PERMANENT SIGN SCHEDULE

NO.	SHEET NO.	PLAN IDENTIFIER	CODE	QTY.	DESCRIPTION	SIGN WIDTH (IN)	SIGN HEIGHT (IN)	SIGN AREA (SF)	ITEM 749687 SINGLE POST (EACH)			ITEM 749690 INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON MULTIPLE SIGN POSTS (SF)			POST INSTALLATION TYPE	Code X11 12' Post (W/ Basepost)	ITEM 749688 4" HOLE, 0-6" (EACH)	ITEM 749689 4" HOLE, >6" (EACH)	REMARKS
									SIGN DISPOSITION	REMOVE	INSTALL	SIGN DISPOSITION	REMOVE	INSTALL					
1872	SS-1	41	D14-3-DE	1	ADOPT A HIGHWAY	24"	0		REMAIN										
1110	SS-2	42	W9-2_L(36)	1	LANE ENDS MERGE LEFT - 36x36	36"	36"	9.0	NEW		1			SOIL	1				
1443	SS-2	43	W21-10-DE(36)	1	WATCH FOR ENTERING TRAFFIC - 36x36	36"	36"		REMOVE	1									
1694	SS-2	44	D4-3-DE	1	BUS STOP (DART)	12"	18"		REMAIN										
310	SS-2	45	R4-7(18)	1	KEEP RIGHT (Symbol) - 18x24	18"	24"		REPOSITION	1	1			CONCRETE	1				
1955	SS-2	46	OM1-3	1	TYPE 1 OBJECT MARKER	18"	18"		REMOVE	1									
906	SS-2	47	W4-2_R(36)	1	LANE ENDS (Right) - 36x36	36"	36"		REMAIN										
304	SS-2	48	R4-4	1	BEGIN RIGHT TURN LANE YIELD TO BIKES	36"	30"		REMAIN										
4	SS-2	49	R1-1(36)	1	STOP	36"	36"		REPOSITION	1	1								
124	SS-2	50	R3-5_R	1	MANDATORY MOVEMENT LANE CONTROL (Right)	30"	36"		REMAIN										
120	SS-2	51	R3-4(24)	1	U - TURN PROHIBITION (Symbol) - 24x24	24"	24"	4.0	NEW		1			CONCRETE	1				
1113	SS-2	52	W9-2-DE_L(36)	1	LANE ENDS (Left Arrows) - 36x36	36"	36"		REPOSITION	1	1								
120	SS-2	53	R3-4(24)	1	U - TURN PROHIBITION (Symbol) - 24x24	24"	24"	4.0	NEW		1			CONCRETE	1				
146	SS-2	54	R3-9b(24)	1	TWO-WAY LEFT TURN ONLY (Ground Mounted) - 24"x36"	24"	36"		REMAIN										
1576	SS-2	55	M4-14(24)	1	BEGIN - 24x12	24"	12"		REMAIN										
1836	SS-2	56	D11-1	1	BIKE ROUTE	24"	18"		REMAIN										
1504	SS-2	57	M2-1(21)	1	JUNCTION - 21x15	21"	15"		REMAIN										
1488	SS-2	58	M1-5(24)	1	STATE ROUTE (2 - Digit Sign) - 24x24	24"	24"		REMAIN										
488	SS-2	59	R8-4(30)	1	EMERGENCY PARKING ONLY - 30x24	30"	24"		REPOSITION	1	1								
877	SS-2	60	W3-3(30)	1	SIGNAL AHEAD (Symbol) - 30x30	30"	30"		REPOSITION	1	1								
1367	SS-2	61	W16-8P	1	ADVANCE STREET NAME (1 - line plaque)	0	9"		REPOSITION	1	1			SOIL	1				Sunset Lake Road
304	SS-2	62	R4-4	1	BEGIN RIGHT TURN LANE YIELD TO BIKES	36"	30"	7.5	NEW		1			SOIL	1				
4	SS-2	63	R1-1(36)	1	STOP	36"	36"		REPOSITION	1	1								
393	SS-2	64	R6-1_R(54)	1	ONE WAY (ENCLOSED IN RIGHT ARROW) - 54x18	54"	18"	6.8				NEW		CONCRETE	2	2			
8	SS-2	65	R1-2(36)	1	YIELD	36"	36"	4.5	NEW		1			CONCRETE	1				
1508	SS-2	66	M3-1(24)	1	CARDINAL DIRECTION - NORTH - 24x12	24"	12"	2.0	NEW		1								
1488	SS-2	67	M1-5(24)	1	STATE ROUTE (2 - Digit Sign) - 24x24	24"	24"	4.0	NEW		1			SOIL	1				
1604	SS-2	68	M6-1_R(21)	1	DIRECTIONAL ARROW (Right) - 21x15	21"	15"	2.2	NEW		1								
1514	SS-2	69	M3-3(24)	1	CARDINAL DIRECTION - SOUTH - 24x12	24"	12"	2.0	NEW		1								
1488	SS-2	70	M1-5(24)	1	STATE ROUTE (2 - Digit Sign) - 24x24	24"	24"	4.0	NEW		1			SOIL	1				
1589	SS-2	71	M5-1_L	1	ADVANCE TURN ARROW (Left - 90)	21"	15"	2.2	NEW		1								
488	SS-2	72	R8-4(30)	1	EMERGENCY PARKING ONLY - 30x24	30"	24"		REMOVE	1									
1694	SS-2	73	D4-3-DE	1	BUS STOP (DART)	12"	18"		REPOSITION	1	1			SOIL	1				
135	SS-2	74	R3-7_R(36)	1	RIGHT LANE MUST TURN RIGHT - 36"x36"	36"	36"		REMOVE	1									
623	SS-2	75	R12-3-DE	1	NO TRUCKS OVER 2 AXLES EXCEPT LOCAL SERVICES	24"	36"		REPOSITION	1	1			SOIL	1				
1619	SS-2	76	M6-3(21)	1	DIRECTIONAL ARROW (Up) - 21x15	21"	15"		REPOSITION	1	1								
1872	SS-3	77	D14-3-DE	1	ADOPT A HIGHWAY	24"	0		NEW		1			SOIL	1				
58	SS-3	78	R2-1-45(24)	1	SPEED LIMIT (45 MPH - 24x30)	24"	30"		REMAIN										
1872	SS-3	79	D14-3-DE	1	ADOPT A HIGHWAY	24"	0		REPOSITION	1	1								
PAGE TOTALS								52		15	23		0	7		14	2	0	

COPY

PERMANENT SIGN SCHEDULE

NO.	SHEET NO.	PLAN IDENTIFIER	CODE	QTY.	DESCRIPTION	SIGN WIDTH (IN)	SIGN HEIGHT (IN)	SIGN AREA (SF)	ITEM 749687 SINGLE POST (EACH)			ITEM 749690 INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON MULTIPLE SIGN POSTS (SF)			POST INSTALLATION TYPE	Code X11 12' Post (W/ Basepost)	ITEM 749688 4" HOLE, 0-6" (EACH)	ITEM 749689 4" HOLE, >6" (EACH)	REMARKS	
									SIGN DISPOSITION	REMOVE	INSTALL	SIGN DISPOSITION	REMOVE	INSTALL						
1576	SS-3	80	M4-14(24)	1	BEGIN - 24x12	24"	12"	2.0	NEW		1			SOIL	1					
146	SS-3	81	R3-9b(24)	1	TWO-WAY LEFT TURN ONLY (Ground Mounted) - 24"x36"	24"	36"	6.0	NEW		1			SOIL	1					
1576	SS-3	82	M4-14(24)	1	BEGIN - 24x12	24"	12"	2.0	NEW		1			SOIL	1					
146	SS-3	83	R3-9b(24)	1	TWO-WAY LEFT TURN ONLY (Ground Mounted) - 24"x36"	24"	36"	6.0	NEW		1			SOIL	1					
877	SS-3	84	W3-3(30)	1	SIGNAL AHEAD (Symbol) - 30x30	30"	30"		REPOSITION	1	1			SOIL	1					
1367	SS-3	85	W16-8P	1	ADVANCE STREET NAME (1 - line plaque)	0	9"		REPOSITION	1	1			SOIL	1				Old Baltimore Pike	
304	SS-3	86	R4-4	1	BEGIN RIGHT TURN LANE YIELD TO BIKES	36"	30"	7.5	NEW		1			SOIL	1					
310	SS-3	87	R4-7(18)	1	KEEP RIGHT (Symbol) - 18x24	18"	24"		REMAIN											
1955	SS-3	88	OM1-3	1	TYPE 1 OBJECT MARKER	18"	18"		REMOVE	1										
1653	SS-3	89	D1-2	1	DESTINATION (2 Line)	0	30"		REMAIN										(Left Arrow) Coochs Bridge Battlefield	
393	SS-4	90	R6-1_R(54)	1	ONE WAY (ENCLOSED IN RIGHT ARROW) - 54x18	54"	18"		REPOSITION			6.8	6.8	CONCRETE	2					
110	SS-4	91	R3-2(24)	1	LEFT TURN PROHIBITION (Symbol) - 24x24	24"	24"		REPOSITION			4.0	4.0	CONCRETE						
393	SS-4	92	R6-1_R(54)	1	ONE WAY (ENCLOSED IN RIGHT ARROW) - 54x18	54"	18"		REMAIN											
4	SS-4	93	R1-1(36)	1	STOP	36"	36"		REMAIN											
58	SS-4	94	R2-1-45(24)	1	SPEED LIMIT (45 MPH - 24x30)	24"	30"		REMAIN											
1208	SS-4	95	W11-10(30)	1	TRUCK (Symbol) - 30x30	30"	30"		REMAIN											
1653	SS-4	96	D1-2	1	DESTINATION (2 Line)	0	30"		REMAIN										Coochs Bridge Battlefield (Right Arrow)	
110	SS-4	97	R3-2(24)	1	LEFT TURN PROHIBITION (Symbol) - 24x24	24"	24"		REMOVE	1										
120	SS-4	98	R3-4(24)	1	U - TURN PROHIBITION (Symbol) - 24x24	24"	24"		REPOSITION	1	1			CONCRETE	1					
310	SS-4	99	R4-7(18)	1	KEEP RIGHT (Symbol) - 18x24	18"	24"		REPOSITION	1	1			CONCRETE	1					
1955	SS-4	100	OM1-3	1	TYPE 1 OBJECT MARKER	18"	18"		REMOVE	1										
877	SS-4	101	W3-3(30)	1	SIGNAL AHEAD (Symbol) - 30x30	30"	30"		REMAIN											
1367	SS-4	102	W16-8P	1	ADVANCE STREET NAME (1 - line plaque)	0	9"		REMAIN										Old Baltimore Pike	
393	SS-2	103	R6-1_R(54)	1	ONE WAY (ENCLOSED IN RIGHT ARROW) - 54x18	54"	18"	6.8				NEW	6.8	CONCRETE	2					
248	SS-2	104	R3-18(36)	1	COMBINATION U-TURN AND LEFT TURN PROHIBITION (Symbol) - 36x36	36"	36"	9.0	NEW		1			CONCRETE	1					
1694	SS-3	105	D4-3-DE	1	BUS STOP (DART)	12"	18"	1.5	NEW		1			SOIL	1					
1694	SS-1	106	D4-3-DE	1	BUS STOP (DART)	12"	18"	1.5	NEW		1			SOIL	1					
250	SS-3	107	R3-20_L	1	BEGIN LEFT TURN LANE	24"	36"	6.0	NEW		1			CONCRETE	1	1				
250	SS-4	108	R3-20_L	1	BEGIN LEFT TURN LANE	24"	36"	6.0	NEW		1			CONCRETE	1					
8	SS-4	109	R1-2(36)	1	YIELD	36"	36"	4.5	NEW		1			CONCRETE	1					
1165	SS-1	110	W11-1(36)	1	BICYCLE (Symbol) - 36x36	36"	36"	9.0	NEW		1			SOIL	1					
	SS-1	111	custom	1	Bike Route	6"	12"		REPOSITION	1	1									
1837	SS-4	112	D11-1a	1	BIKE (Symbol)	18"	18"	2.3	NEW		1			SOIL	1					
1608	SS-4	113	M6-2_R(E)	1	DIRECTIONAL ARROW (Right - upward 45) - 12x9	12"	9"	0.8	NEW		1									
1165	SS-1	114	W11-1(36)	1	BICYCLE (Symbol) - 36x36	36"	36"	9.0	NEW		1			SOIL	1					
1459	SS-1	115	W23-2P-DE(30)	1	NEW TRAFFIC PATTERN (plaque) - 30x24	30"	24"	5.0	NEW		1								DelDOT forces remove after 30-180 days	
49	S-1, S-2	D3-1		2	STREET NAME (1 Line)	102"	18"	25.5											8" / 6" D-Series, Old Baltimore Pike	
49	S-3	D3-1a		1	STREET NAME + ROUTE SHIELD (1 Line)	120"	18"	15.0											8" / 6" D-Series, Sunset Lake Rd (SR 72)	
49	S-4	D3-1a		1	STREET NAME + ROUTE SHIELD (1 Line)	108"	18"	13.5											8" / 6" D-Series, S. Chapel St (SR 72)	
PAGE TOTALS															19	1				
JOB TOTALS																46	4	0		

COPY

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ADDENDUMS / REVISIONS

NOT TO SCALE

HEP NCC,
SR 72 AND OLD BALTIMORE PIKE
INTERSECTION IMPROVEMENTS

CONTRACT	BRIDGE NO.	N/A
T201200108	DESIGNED BY:	BCD
COUNTY	CHECKED BY:	MCN
NEW CASTLE		

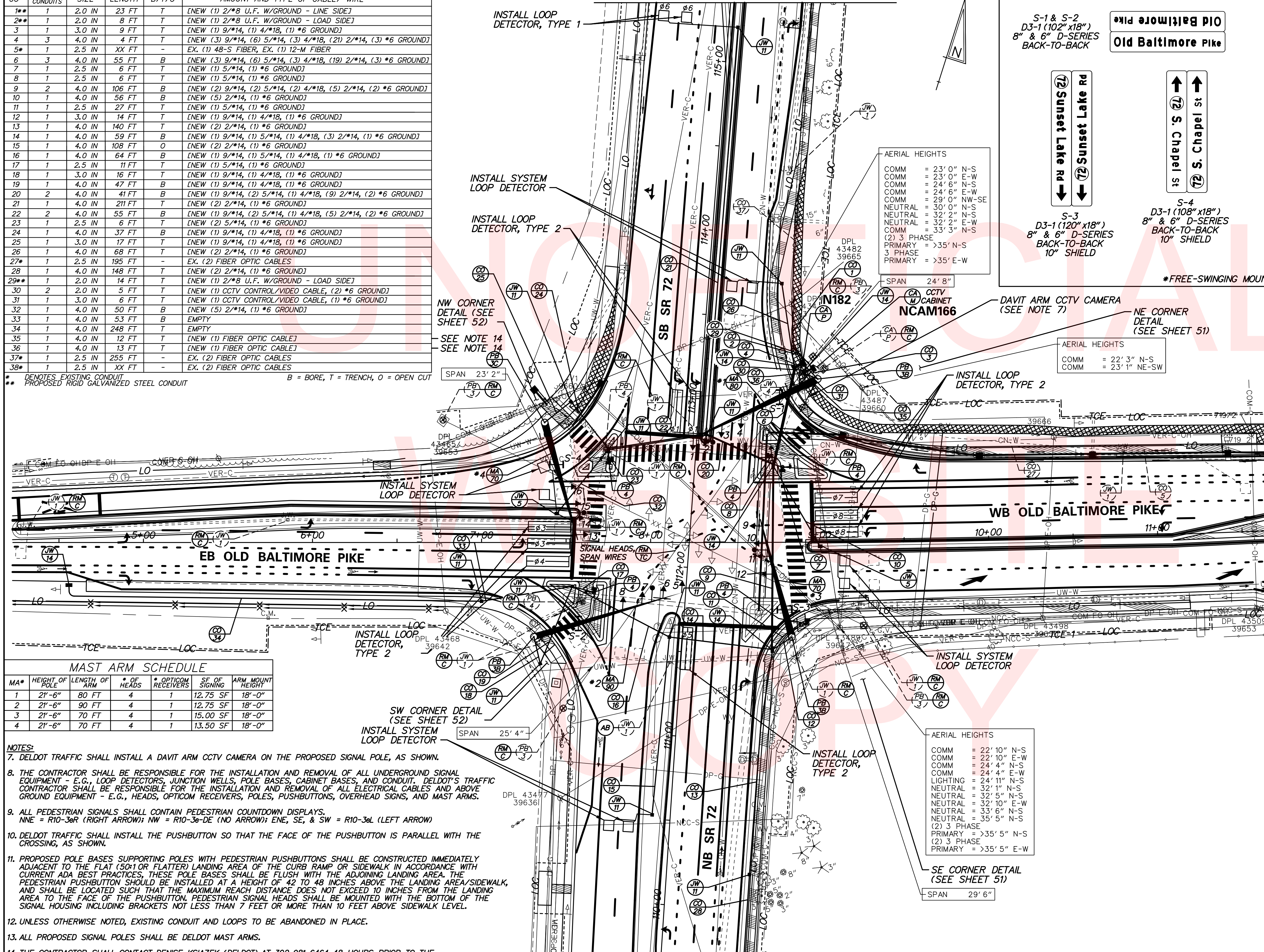
SIGNING, STRIPING
AND CONDUIT PLAN

SHEET NO.	48
TOTAL SHTS.	52

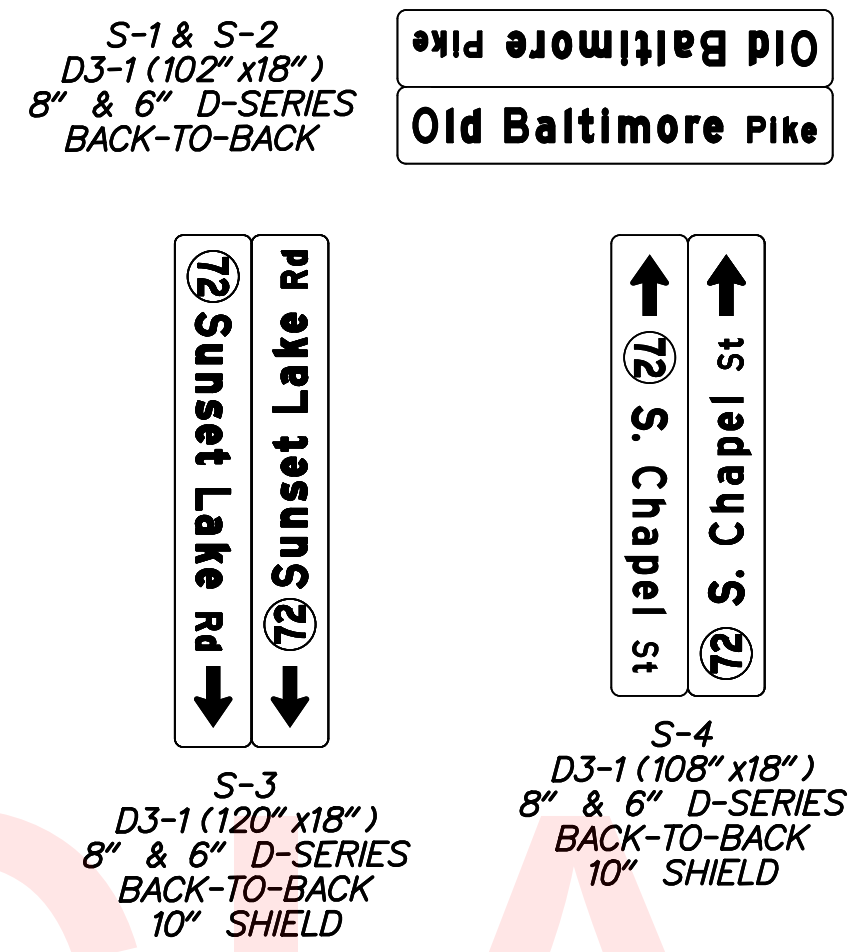
CO#	*# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE / WIRE
1**	1	2.0 IN	23 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LINE SIDE]
2**	1	2.0 IN	8 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LOAD SIDE]
3	1	3.0 IN	9 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
4	3	4.0 IN	4 FT	T	[NEW (3) 9/*14, (6) 5/*14, (3) 4/*18, (21) 2/*14, (3) *6 GROUND]
5*	1	2.5 IN	XX FT	-	EX. (1) 48-S FIBER, EX. (1) 12-M FIBER
6	3	4.0 IN	55 FT	B	[NEW (3) 9/*14, (6) 5/*14, (3) 4/*18, (19) 2/*14, (3) *6 GROUND]
7	1	2.5 IN	6 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
8	1	2.5 IN	6 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
9	2	4.0 IN	106 FT	B	[NEW (2) 9/*14, (2) 5/*14, (2) 4/*18, (5) 2/*14, (2) *6 GROUND]
10	1	4.0 IN	56 FT	B	[NEW (5) 2/*14, (1) *6 GROUND]
11	1	2.5 IN	27 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
12	1	3.0 IN	14 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
13	1	4.0 IN	140 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
14	1	4.0 IN	59 FT	B	[NEW (1) 9/*14, (1) 5/*14, (1) 4/*18, (3) 2/*14, (1) *6 GROUND]
15	1	4.0 IN	108 FT	O	[NEW (2) 2/*14, (1) *6 GROUND]
16	1	4.0 IN	64 FT	B	[NEW (1) 9/*14, (1) 5/*14, (1) 4/*18, (1) *6 GROUND]
17	1	2.5 IN	11 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
18	1	3.0 IN	16 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
19	1	4.0 IN	47 FT	B	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
20	2	4.0 IN	41 FT	B	[NEW (1) 9/*14, (2) 5/*14, (1) 4/*18, (9) 2/*14, (2) *6 GROUND]
21	1	4.0 IN	211 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
22	2	4.0 IN	55 FT	B	[NEW (1) 9/*14, (2) 5/*14, (1) 4/*18, (5) 2/*14, (2) *6 GROUND]
23	1	2.5 IN	6 FT	T	[NEW (2) 5/*14, (1) *6 GROUND]
24	1	4.0 IN	37 FT	B	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
25	1	3.0 IN	17 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
26	1	4.0 IN	68 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
27*	1	2.5 IN	195 FT	-	EX. (2) FIBER OPTIC CABLES
28	1	4.0 IN	148 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
29**	1	2.0 IN	14 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LOAD SIDE]
30	2	2.0 IN	5 FT	T	[NEW (1) CCTV CONTROL/VIDEO CABLE, (2) *6 GROUND]
31	1	3.0 IN	6 FT	T	[NEW (1) CCTV CONTROL/VIDEO CABLE, (1) *6 GROUND]
32	1	4.0 IN	50 FT	B	[NEW (5) 2/*14, (1) *6 GROUND]
33	1	4.0 IN	53 FT	B	EMPTY
34	1	4.0 IN	248 FT	T	EMPTY
35	1	4.0 IN	12 FT	T	[NEW (1) FIBER OPTIC CABLE]
36	1	4.0 IN	13 FT	T	[NEW (1) FIBER OPTIC CABLE]
37*	1	2.5 IN	255 FT	-	EX. (2) FIBER OPTIC CABLES
38*	1	2.5 IN	XX FT	-	EX. (2) FIBER OPTIC CABLES

** DENOTES EXISTING CONDUIT
 ** PROPOSED RIGID GALVANIZED STEEL CONDUIT
 B = BORE, T = TRENCH, O = OPEN CUT

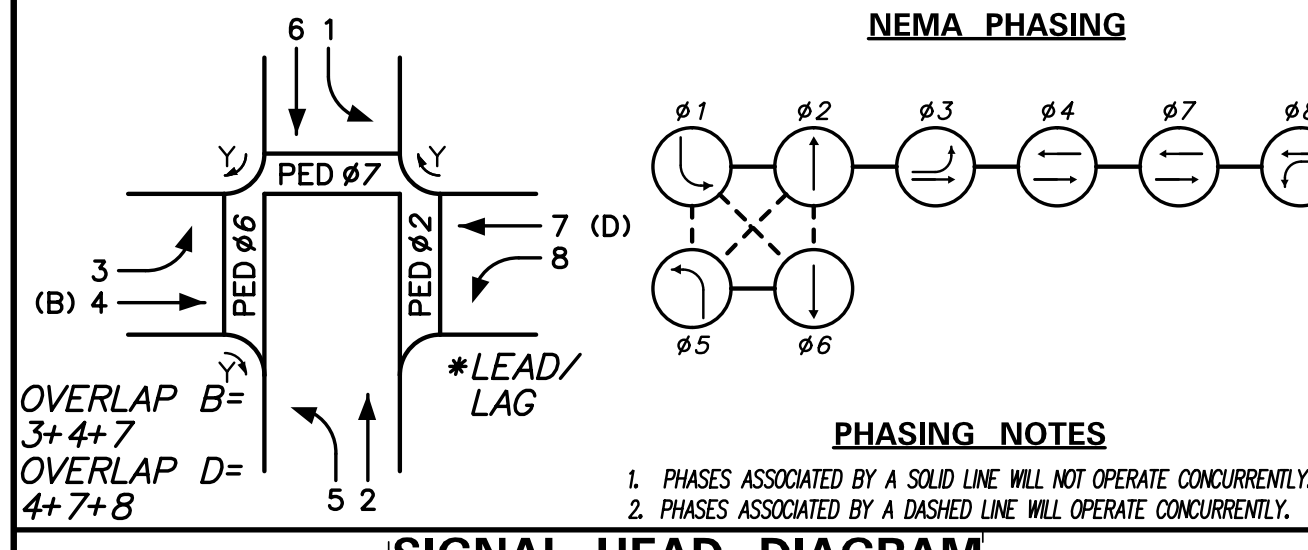
MATCH LINE A - SEE SHEET 50



PROPOSED OVERHEAD SIGNING



SIGNAL PHASING



LEGEND

EXISTING SYMBOL	PROPOSED SYMBOL
JUNCTION WELL	J.W.
LOOP DETECTOR, TYPE 1	[Symbol]
LOOP DETECTOR, TYPE 2	[Symbol]
LUMINAIRE	[Symbol]
MAST ARM	[Symbol]
MICROWAVE DETECTION	[Symbol]
OPTICOM RECEIVER	[Symbol]
OVERHEAD SIGNING	[Symbol]
PEDESTRIAN POLE/BASE	[Symbol]
PEDESTRIAN PUSHBUTTON	[Symbol]
PEDESTRIAN SIGNAL HEAD	[Symbol]
RIGHT-OF-WAY	R/W
SERVICE PEDESTAL	[Symbol]
SIGNAL CABINET	[Symbol]
SIGNAL HEAD	[Symbol]
SIGNAL POLE/BASE	[Symbol]
SPAN INSULATOR	[Symbol]
SPAN WIRE	XX
UTILITY POLE	[Symbol]
VIDEO DETECTION	[Symbol]

GENERAL SIGNAL NOTES

- PROPOSED LOOP DETECTORS: TYPE #1 - 6' x 6' - TO BE INSTALLED ON SR 72 THROUGH MOVEMENTS. TYPE #2 - 6' x 25' - TO BE INSTALLED ON OLD BALTIMORE PIKE LEFT-TURN AND THROUGH MOVEMENTS. TYPE #3 - 6' x 25' - TO BE INSTALLED ON SR 72 LEFT-TURN MOVEMENTS. SYSTEM - 6' x 6' - TO BE INSTALLED IN SR 72 AND OLD BALTIMORE PIKE RECEIVING LANES, AS SHOWN.
- ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - COVER, DELAWARE.
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS. SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- POLE BASES, CABINET BASE AND CONDUIT JUNCTION WELLS ARE TO BE REMOVED IN ACCORDANCE WITH SECTION 211 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
- ALL CONDUIT SHALL BE SCHEDULE 80 PVC WHEN INSTALLED BY TRENCHING OR OPEN CUT AND SCHEDULE 80 HDPE WHEN INSTALLED BY BORING UNLESS OTHERWISE NOTED.

MA#	HEIGHT OF POLE	LENGTH OF ARM	* OF HEADS	* OPTICOM RECEIVERS	SF OF SIGNING	ARM MOUNT HEIGHT
1	21'-6"	80 FT	4	1	12.75 SF	18'-0"
2	21'-6"	90 FT	4	1	12.75 SF	18'-0"
3	21'-6"	70 FT	4	1	15.00 SF	18'-0"
4	21'-6"	70 FT	4	1	13.50 SF	18'-0"

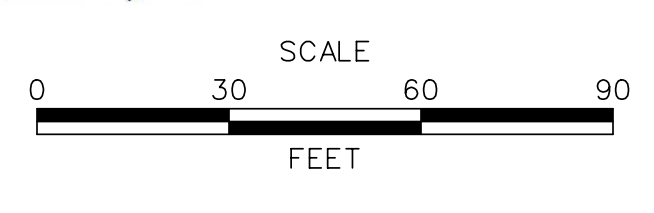
- NOTES:**
- DELDOT TRAFFIC SHALL INSTALL A DAVIT ARM CCTV CAMERA ON THE PROPOSED SIGNAL POLE, AS SHOWN.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL UNDERGROUND SIGNAL EQUIPMENT - E.G., LOOP DETECTORS, JUNCTION WELLS, POLE BASES, CABINET BASES, AND CONDUIT. DELDOT'S TRAFFIC CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL ELECTRICAL CABLES AND ABOVE GROUND EQUIPMENT - E.G., HEADS, OPTICOM RECEIVERS, POLES, PUSHBUTTONS, OVERHEAD SIGNS, AND MAST ARMS.
 - ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS. NNE = R10-3aR (RIGHT ARROW); NN = R10-3a-DE (NO ARROW); ENE, SE, & SW = R10-3aL (LEFT ARROW)
 - DELDOT TRAFFIC SHALL INSTALL THE PUSHBUTTON SO THAT THE FACE OF THE PUSHBUTTON IS PARALLEL WITH THE CROSSING, AS SHOWN.
 - PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50:1 OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHOULD BE INSTALLED AT A HEIGHT OF 42 TO 48 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE DOES NOT EXCEED 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 7 FEET OR MORE THAN 10 FEET ABOVE SIDEWALK LEVEL.
 - UNLESS OTHERWISE NOTED, EXISTING CONDUIT AND LOOPS TO BE ABANDONED IN PLACE.
 - ALL PROPOSED SIGNAL POLES SHALL BE DELDOT MAST ARMS.
 - THE CONTRACTOR SHALL CONTACT DENISE KSIAZEK (DELDOT) AT 302-981-6464 48 HOURS PRIOR TO THE INSTALLATION OF PROPOSED CONDUITS NOS. 35 AND 36 TO COORDINATE THE FIBER OPTIC CONNECTION IN THE PROPOSED SIGNAL CABINET AND CCTV CABINET.

MATCH LINE B - SEE SHEET 50

RECOMMENDED _____ DATE: _____ RECOMMENDED _____ DATE: _____ RECOMMENDED _____ DATE: 11/17 APPROVED TRAFFIC ENGINEER _____ DATE: _____ APPROVED FOR INSTALLATION _____ DATE: 11/17
 CHIEF TRAFFIC ENGINEER



ADDENDUM / REVISIONS



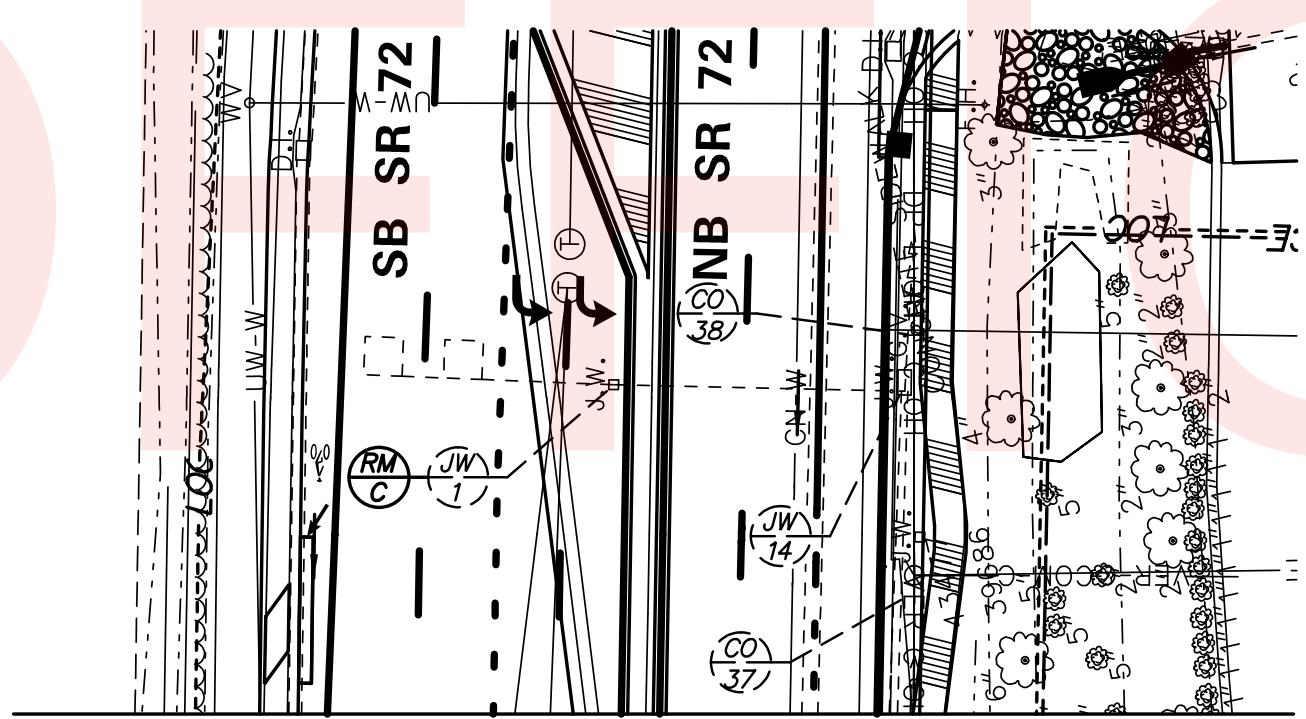
HEP NCC, SR 72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS

CONTRACT	T201200108	PERMIT NO.	N182 & NCAM166	SHEET NO.	49
COUNTY	NEW CASTLE	DESIGNED BY:	D.W.C. (WRA)	TOTAL SHTS.	52
		CHECKED BY:	M.J.B. (WRA)		
			SIGNAL PLAN		
			SR 72 @		
			OLD BALTIMORE PIKE		

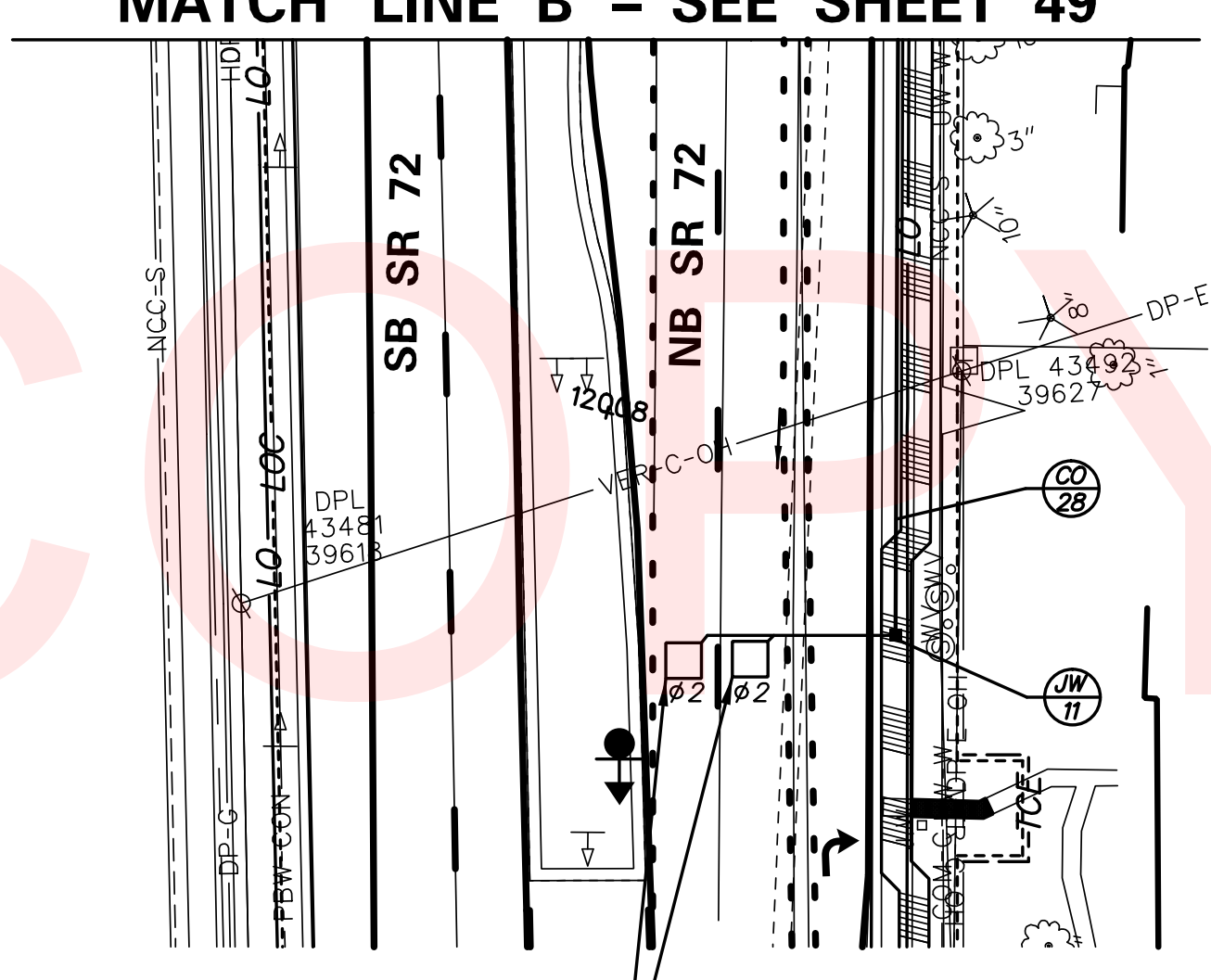
CONDUIT RUN SCHEDULE					
CO#	* OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1**	1	2.0 IN	23 FT	T	[NEW] (1) 2/*8 U.F. W/GROUND - LINE SIDE]
2**	1	2.0 IN	8 FT	T	[NEW] (1) 2/*8 U.F. W/GROUND - LOAD SIDE]
3	1	3.0 IN	9 FT	T	[NEW] (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
4	3	4.0 IN	4 FT	T	[NEW] (3) 9/*14, (6) 5/*14, (3) 4/*18, (21) 2/*14, (3) *6 GROUND]
5*	1	2.5 IN	XX FT	-	EX. (1) 48-S FIBER, EX. (1) 12-M FIBER
6	3	4.0 IN	55 FT	B	[NEW] (3) 9/*14, (6) 5/*14, (3) 4/*18, (19) 2/*14, (3) *6 GROUND]
7	1	2.5 IN	6 FT	T	[NEW] (1) 5/*14, (1) *6 GROUND]
8	1	2.5 IN	6 FT	T	[NEW] (1) 5/*14, (1) *6 GROUND]
9	2	4.0 IN	106 FT	B	[NEW] (2) 9/*14, (2) 5/*14, (2) 4/*18, (5) 2/*14, (2) *6 GROUND]
10	1	4.0 IN	56 FT	B	[NEW] (5) 2/*14, (1) *6 GROUND]
11	1	2.5 IN	27 FT	T	[NEW] (1) 5/*14, (1) *6 GROUND]
12	1	3.0 IN	14 FT	T	[NEW] (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
13	1	4.0 IN	140 FT	T	[NEW] (2) 2/*14, (1) *6 GROUND]
14	1	4.0 IN	59 FT	B	[NEW] (1) 9/*14, (1) 5/*14, (1) 4/*18, (3) 2/*14, (1) *6 GROUND]
15	1	4.0 IN	108 FT	O	[NEW] (2) 2/*14, (1) *6 GROUND]
16	1	4.0 IN	64 FT	B	[NEW] (1) 9/*14, (1) 5/*14, (1) 4/*18, (1) *6 GROUND]
17	1	2.5 IN	11 FT	T	[NEW] (1) 5/*14, (1) *6 GROUND]
18	1	3.0 IN	16 FT	T	[NEW] (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
19	1	4.0 IN	47 FT	B	[NEW] (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
20	2	4.0 IN	41 FT	B	[NEW] (1) 9/*14, (2) 5/*14, (1) 4/*18, (9) 2/*14, (2) *6 GROUND]
21	1	4.0 IN	211 FT	T	[NEW] (2) 2/*14, (1) *6 GROUND]
22	2	4.0 IN	55 FT	B	[NEW] (1) 9/*14, (2) 5/*14, (1) 4/*18, (5) 2/*14, (2) *6 GROUND]
23	1	2.5 IN	6 FT	T	[NEW] (2) 5/*14, (1) *6 GROUND]
24	1	4.0 IN	37 FT	B	[NEW] (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
25	1	3.0 IN	17 FT	T	[NEW] (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
26	1	4.0 IN	68 FT	T	[NEW] (2) 2/*14, (1) *6 GROUND]
27*	1	2.5 IN	195 FT	-	EX. (2) FIBER OPTIC CABLES
28	1	4.0 IN	148 FT	T	[NEW] (2) 2/*14, (1) *6 GROUND]
29**	1	2.0 IN	14 FT	T	[NEW] (1) 2/*8 U.F. W/GROUND - LOAD SIDE]
30	2	2.0 IN	5 FT	T	[NEW] (1) CCTV CONTROL/VIDEO CABLE, (2) *6 GROUND]
31	1	3.0 IN	6 FT	T	[NEW] (1) CCTV CONTROL/VIDEO CABLE, (1) *6 GROUND]
32	1	4.0 IN	50 FT	B	[NEW] (5) 2/*14, (1) *6 GROUND]
33	1	4.0 IN	53 FT	B	EMPTY
34	1	4.0 IN	248 FT	T	EMPTY
35	1	4.0 IN	12 FT	T	[NEW] (1) FIBER OPTIC CABLE]
36	1	4.0 IN	13 FT	T	[NEW] (1) FIBER OPTIC CABLE]
37*	1	2.5 IN	255 FT	-	EX. (2) FIBER OPTIC CABLES
38*	1	2.5 IN	XX FT	-	EX. (2) FIBER OPTIC CABLES

* DENOTES EXISTING CONDUIT
 ** PROPOSED RIGID GALVANIZED STEEL CONDUIT
 B = BORE, T = TRENCH, O = OPEN CUT

SEE NOTE 14
 SEE NOTE 14

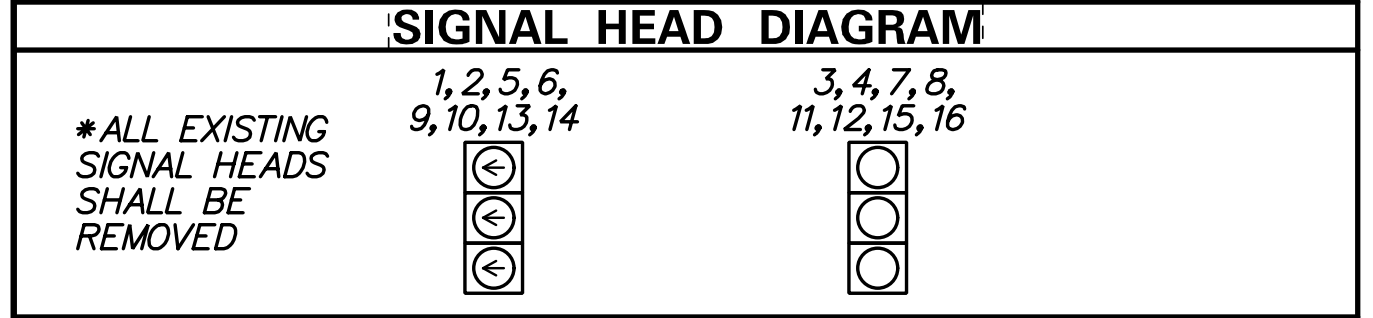
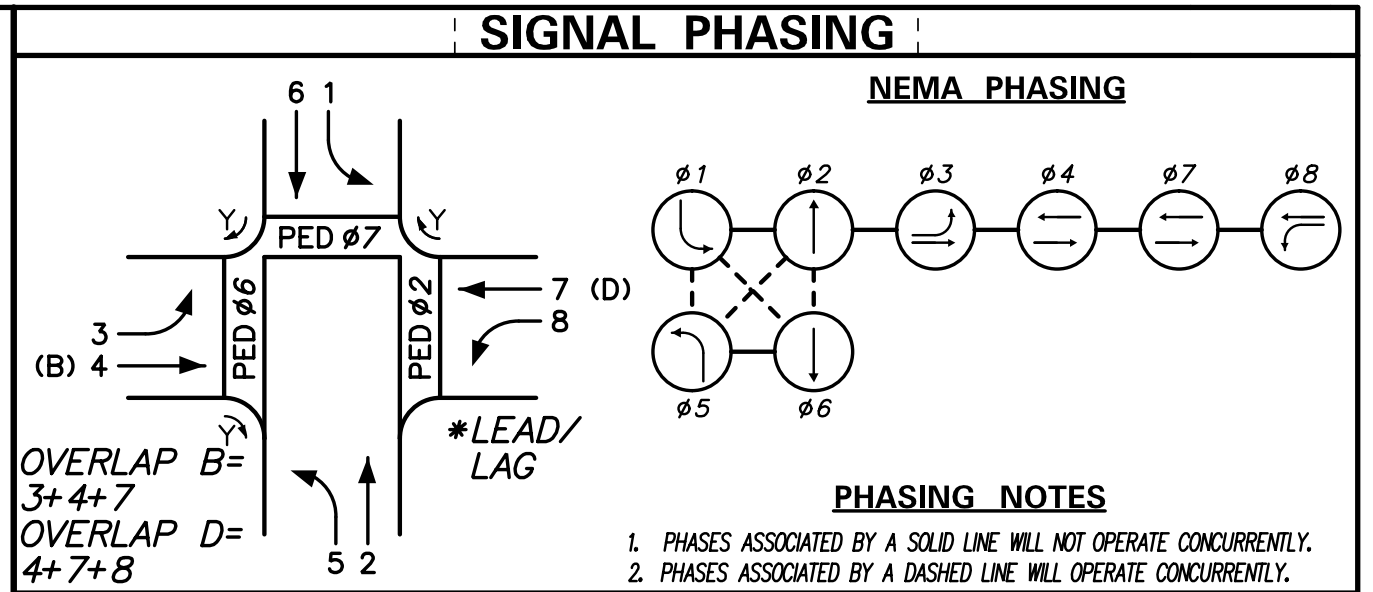


MATCH LINE A - SEE SHEET 49



MATCH LINE B - SEE SHEET 49

- NOTES:**
- DELDOT TRAFFIC SHALL INSTALL A DAVIT ARM CCTV CAMERA ON THE PROPOSED SIGNAL POLE, AS SHOWN.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL UNDERGROUND SIGNAL EQUIPMENT - E.G., LOOP DETECTORS, JUNCTION WELLS, POLE BASES, CABINET BASES, AND CONDUIT. DELDOT'S TRAFFIC CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL ELECTRICAL CABLES AND ABOVE GROUND EQUIPMENT - E.G., HEADS, OPTICOM RECEIVERS, POLES, PUSHBUTTONS, OVERHEAD SIGNS, AND MAST ARMS.
 - ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
 NNE = R10-3eR (RIGHT ARROW); NW = R10-3e-DE (NO ARROW); ENE, SE, & SW = R10-3eL (LEFT ARROW)
 - DELDOT TRAFFIC SHALL INSTALL THE PUSHBUTTON SO THAT THE FACE OF THE PUSHBUTTON IS PARALLEL WITH THE CROSSING, AS SHOWN.
 - PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50:1 OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHOULD BE INSTALLED AT A HEIGHT OF 42 TO 48 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE DOES NOT EXCEED 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 7 FEET OR MORE THAN 10 FEET ABOVE SIDEWALK LEVEL.
 - UNLESS OTHERWISE NOTED, EXISTING CONDUIT AND LOOPS TO BE ABANDONED IN PLACE.
 - ALL PROPOSED SIGNAL POLES SHALL BE DELDOT MAST ARMS.
 - THE CONTRACTOR SHALL CONTACT DENISE KSIAZEK (DELDOT) AT 302-981-6464 48 HOURS PRIOR TO THE INSTALLATION OF PROPOSED CONDUITS NOS. 35 AND 36 TO COORDINATE THE FIBER OPTIC CONNECTION IN THE PROPOSED SIGNAL CABINET AND CCTV CABINET.



LEGEND			
(AB)	ABANDON	(OH)	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(CA)	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)	(OH)	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(CB)	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)	(PB)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(CO)	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)	(PB)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(CP)	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)	(PL)	EXISTING POLE IDENTIFIER (* OF POLE)
(JW)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)	(PL)	PROPOSED POLE IDENTIFIER (* OF POLE)
(JW)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)	(RM)	REMOVE BY CONTRACTOR
(MA)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)	(RM)	REMOVE BY OTHERS
(MA)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)	(TC)	REMOVE BY TRAFFIC CONTRACTOR

	EXISTING SYMBOL	PROPOSED SYMBOL
JUNCTION WELL	J.W.	■
LOOP DETECTOR, TYPE 1	□	□
LOOP DETECTOR, TYPE 2	□	□
LUMINAIRE	○	○
MAST ARM	▶	▶
MICROWAVE DETECTION	▶	▶
OPTICOM RECEIVER	○	○
OVERHEAD SIGNING	—	—
PEDESTRIAN POLE/BASE	⊙	⊙
PEDESTRIAN PUSHBUTTON	→	→
PEDESTRIAN SIGNAL HEAD	→	→
RIGHT-OF-WAY	—	— R/W
SERVICE PEDESTAL	□	□
SIGNAL CABINET	□	□
SIGNAL HEAD	→	→
SIGNAL POLE/BASE	⊙	⊙
SPAN INSULATOR	◇	◇
SPAN WIRE	— XX —	— XX —
UTILITY POLE	⊙	⊙
VIDEO DETECTION	▶	▶

- GENERAL SIGNAL NOTES**
- PROPOSED LOOP DETECTORS
 TYPE #1 - 6' x 6' - TO BE INSTALLED ON SR 72 THROUGH MOVEMENTS.
 TYPE #2 - 6' x 25' - TO BE INSTALLED ON OLD BALTIMORE PIKE LEFT-TURN AND THROUGH MOVEMENTS.
 TYPE #2 - 6' x 25' - TO BE INSTALLED ON SR 72 LEFT-TURN MOVEMENTS.
 SYSTEM - 6' x 6' - TO BE INSTALLED IN SR 72 AND OLD BALTIMORE PIKE RECEIVING LANES, AS SHOWN.
 - ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 - ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS. SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
 - POLE BASES, CABINET BASE AND CONDUIT JUNCTION WELLS ARE TO BE REMOVED IN ACCORDANCE WITH SECTION 211 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
 - ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
 - ALL CONDUIT SHALL BE SCHEDULE 80 PVC WHEN INSTALLED BY TRENCHING OR OPEN CUT AND SCHEDULE 80 HDPE WHEN INSTALLED BY BORING UNLESS OTHERWISE NOTED.

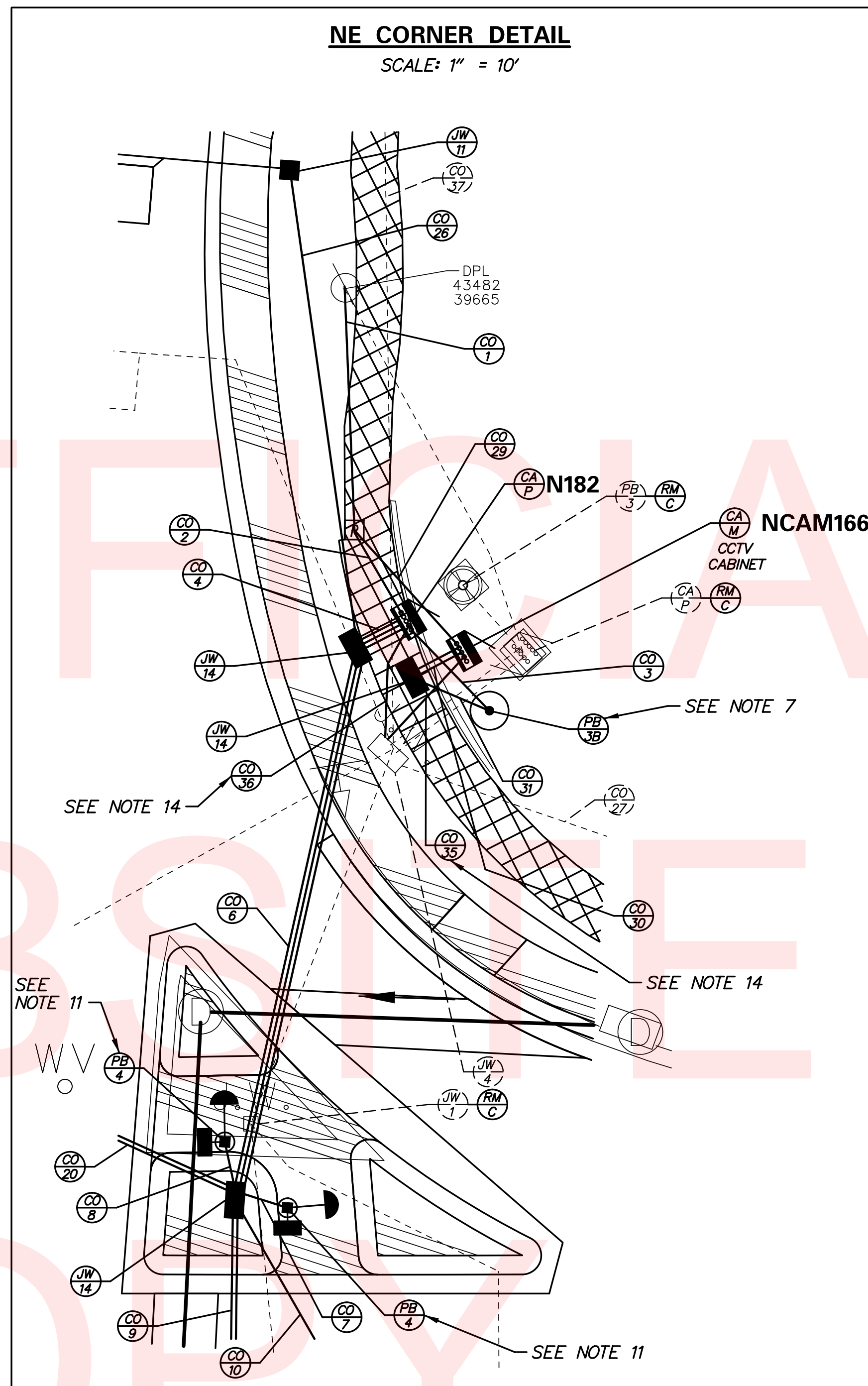
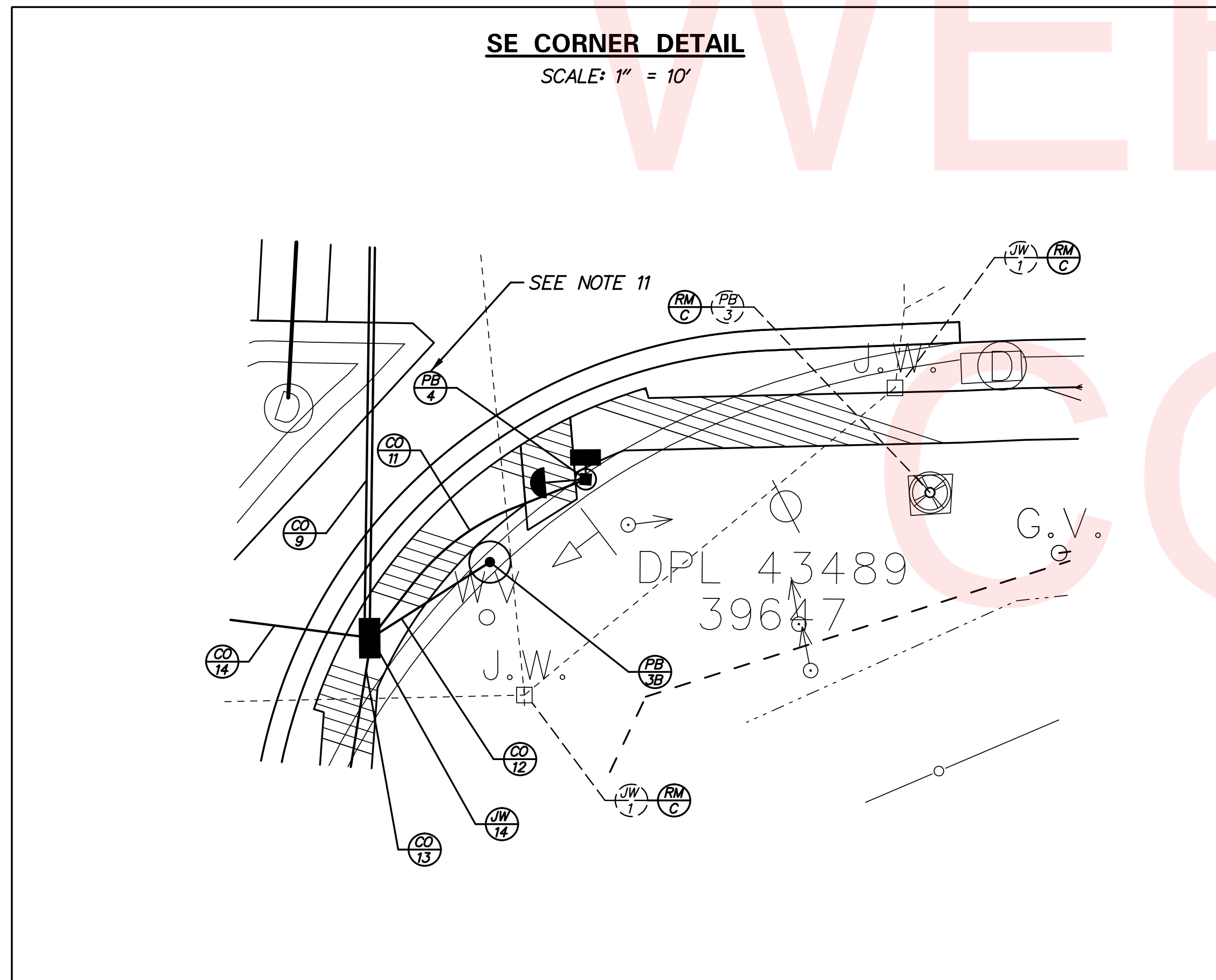
RECOMMENDED _____ DATE: _____	RECOMMENDED _____ DATE: _____	RECOMMENDED <i>[Signature]</i> DATE: <i>12/17</i>	APPROVED TRAFFIC ENGINEER _____ DATE: _____	APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER <i>[Signature]</i> DATE: <i>12/17</i>
ADDENDUM / REVISIONS		SCALE 0 30 60 90 FEET	HEP NCC, SR 72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS	SIGNAL PLAN SR 72 @ OLD BALTIMORE PIKE
DELAWARE DEPARTMENT OF TRANSPORTATION		CONTRACT T201200108	PERMIT NO. N182 & NCAM166	SHEET NO. 50
		COUNTY NEW CASTLE	DESIGNED BY: D.W.C. (WRA)	TOTAL SHTS. 52
			CHECKED BY: M.J.B. (WRA)	

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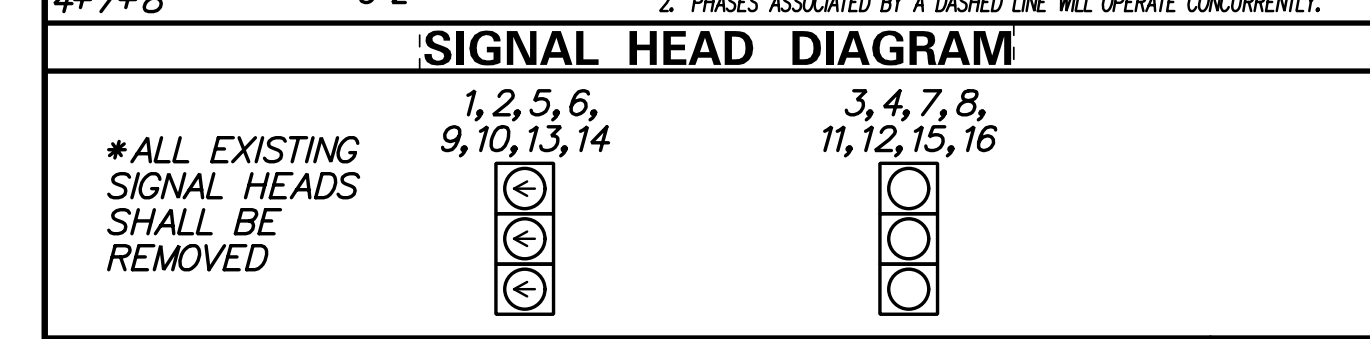
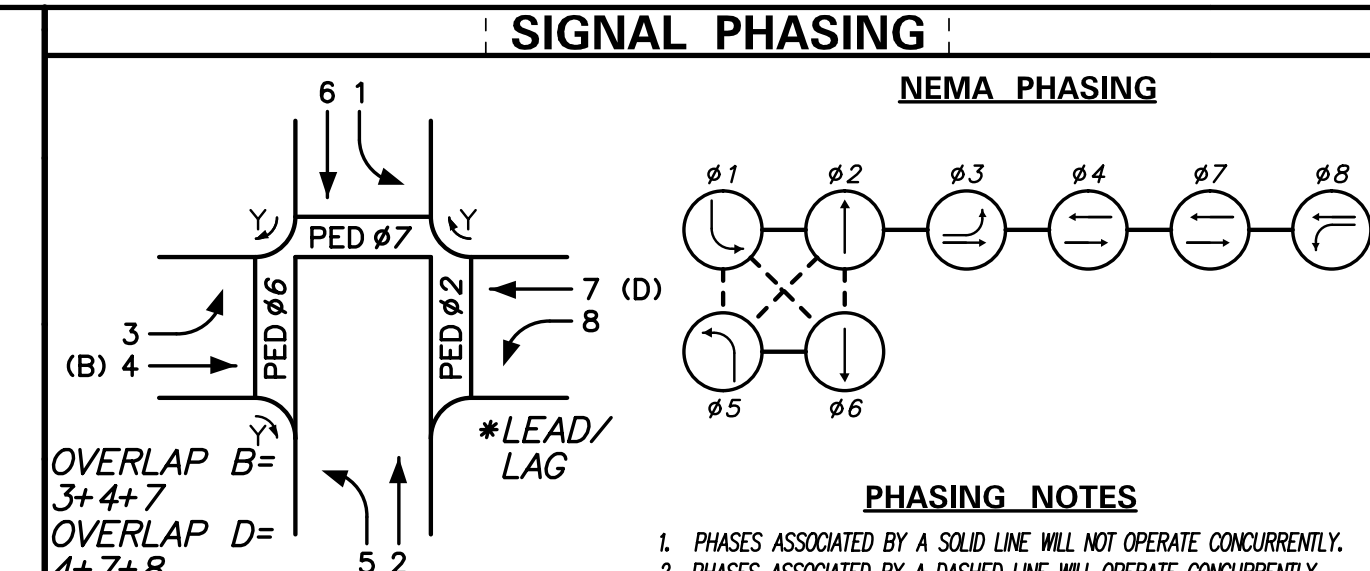
CO#	* OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
7**	1	2.0 IN	23 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LINE SIDE]
2**	1	2.0 IN	8 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LOAD SIDE]
3	1	3.0 IN	9 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
4	3	4.0 IN	4 FT	T	[NEW (3) 9/*14, (6) 5/*14, (3) 4/*18, (21) 2/*14, (3) *6 GROUND]
5*	1	2.5 IN	XX FT	-	EX. (1) 48-S FIBER, EX. (1) 12-M FIBER
6	3	4.0 IN	55 FT	B	[NEW (3) 9/*14, (6) 5/*14, (3) 4/*18, (19) 2/*14, (3) *6 GROUND]
7	1	2.5 IN	6 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
8	1	2.5 IN	6 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
9	2	4.0 IN	106 FT	B	[NEW (2) 9/*14, (2) 5/*14, (2) 4/*18, (5) 2/*14, (2) *6 GROUND]
10	1	4.0 IN	56 FT	B	[NEW (5) 2/*14, (1) *6 GROUND]
11	1	2.5 IN	27 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
12	1	3.0 IN	14 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
13	1	4.0 IN	140 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
14	1	4.0 IN	59 FT	B	[NEW (1) 9/*14, (1) 5/*14, (1) 4/*18, (3) 2/*14, (1) *6 GROUND]
15	1	4.0 IN	108 FT	O	[NEW (2) 2/*14, (1) *6 GROUND]
16	1	4.0 IN	64 FT	B	[NEW (1) 9/*14, (1) 5/*14, (1) 4/*18, (1) *6 GROUND]
17	1	2.5 IN	11 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
18	1	3.0 IN	16 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
19	1	4.0 IN	47 FT	B	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
20	2	4.0 IN	41 FT	B	[NEW (1) 9/*14, (2) 5/*14, (1) 4/*18, (9) 2/*14, (2) *6 GROUND]
21	1	4.0 IN	211 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
22	2	4.0 IN	55 FT	B	[NEW (1) 9/*14, (2) 5/*14, (1) 4/*18, (5) 2/*14, (2) *6 GROUND]
23	1	2.5 IN	6 FT	T	[NEW (2) 5/*14, (1) *6 GROUND]
24	1	4.0 IN	37 FT	B	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
25	1	3.0 IN	17 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
26	1	4.0 IN	68 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
27*	1	2.5 IN	195 FT	-	EX. (2) FIBER OPTIC CABLES
28	1	4.0 IN	148 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
29**	1	2.0 IN	14 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LOAD SIDE]
30	2	2.0 IN	5 FT	T	[NEW (1) CCTV CONTROL/VIDEO CABLE, (2) *6 GROUND]
31	1	3.0 IN	6 FT	T	[NEW (1) CCTV CONTROL/VIDEO CABLE, (1) *6 GROUND]
32	1	4.0 IN	50 FT	B	[NEW (5) 2/*14, (1) *6 GROUND]
33	1	4.0 IN	53 FT	B	EMPTY
34	1	4.0 IN	248 FT	T	EMPTY
35	1	4.0 IN	12 FT	T	[NEW (1) FIBER OPTIC CABLE]
36	1	4.0 IN	13 FT	T	[NEW (1) FIBER OPTIC CABLE]
37*	1	2.5 IN	255 FT	-	EX. (2) FIBER OPTIC CABLES
38*	1	2.5 IN	XX FT	-	EX. (2) FIBER OPTIC CABLES

* DENOTES EXISTING CONDUIT
 ** PROPOSED RIGID GALVANIZED STEEL CONDUIT
 B = BORE, T = TRENCH, O = OPEN CUT

SEE NOTE 14
 SEE NOTE 14



- NOTES:
7. DELDOT TRAFFIC SHALL INSTALL A DAVIT ARM CCTV CAMERA ON THE PROPOSED SIGNAL POLE, AS SHOWN.
 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL UNDERGROUND SIGNAL EQUIPMENT - E.G., LOOP DETECTORS, JUNCTION WELLS, POLE BASES, CABINET BASES, AND CONDUIT. DELDOT'S TRAFFIC CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL ELECTRICAL CABLES AND ABOVE GROUND EQUIPMENT - E.G., HEADS, OPTICOM RECEIVERS, POLES, PUSHBUTTONS, OVERHEAD SIGNS, AND MAST ARMS.
 9. ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
 NNE = R10-3eR (RIGHT ARROW); NW = R10-3e-DE (NO ARROW); ENE, SE, & SW = R10-3eL (LEFT ARROW)
 10. DELDOT TRAFFIC SHALL INSTALL THE PUSHBUTTON SO THAT THE FACE OF THE PUSHBUTTON IS PARALLEL WITH THE CROSSING, AS SHOWN.
 11. PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (S01) OR FLATTER LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHOULD BE INSTALLED AT A HEIGHT OF 42 TO 48 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE DOES NOT EXCEED 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 7 FEET OR MORE THAN 10 FEET ABOVE SIDEWALK LEVEL.
 12. UNLESS OTHERWISE NOTED, EXISTING CONDUIT AND LOOPS TO BE ABANDONED IN PLACE.
 13. ALL PROPOSED SIGNAL POLES SHALL BE DELDOT MAST ARMS.
 14. THE CONTRACTOR SHALL CONTACT DENISE KSIAZEK (DELDOT) AT 302-981-6464 48 HOURS PRIOR TO THE INSTALLATION OF PROPOSED CONDUITS NOS. 35 AND 36 TO COORDINATE THE FIBER OPTIC CONNECTION IN THE PROPOSED SIGNAL CABINET AND CCTV CABINET.



LEGEND	
(AB)	ABANDON
(CA)	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
(CA)	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
(CO)	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
(CO)	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
(JW)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
(JW)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
(MA)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
(MA)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
(OH)	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(OH)	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(PB)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(PB)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(PL)	EXISTING POLE IDENTIFIER (* OF POLE)
(PL)	PROPOSED POLE IDENTIFIER (* OF POLE)
(RM)	REMOVE BY CONTRACTOR
(RM)	REMOVE BY OTHERS
(RM)	REMOVE BY TRAFFIC CONTRACTOR

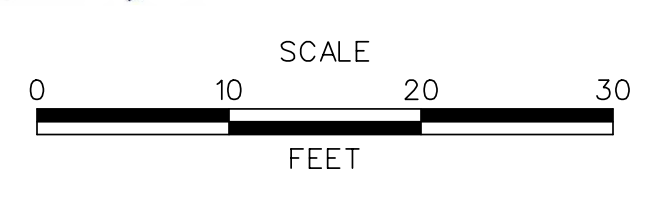
	EXISTING SYMBOL	PROPOSED SYMBOL
JUNCTION WELL	J.W.	■
LOOP DETECTOR, TYPE 1	□	□
LOOP DETECTOR, TYPE 2	□	□
LUMINAIRE	○	○
MAST ARM	▶	▶
MICROWAVE DETECTION	◀	▶
OPTICOM RECEIVER	○	○
OVERHEAD SIGNING	—	—
PEDESTRIAN POLE/BASE	⊙	⊙
PEDESTRIAN PUSHBUTTON	→	→
PEDESTRIAN SIGNAL HEAD	→	→
RIGHT-OF-WAY	—	— R/W
SERVICE PEDESTAL	□	□
SIGNAL CABINET	□	□
SIGNAL HEAD	→	→
SIGNAL POLE/BASE	⊙	⊙
SPAN INSULATOR	◇	◇
SPAN WIRE	— XX —	— XX —
UTILITY POLE	⊗	⊗
VIDEO DETECTION	◀	▶

- GENERAL SIGNAL NOTES:
1. PROPOSED LOOP DETECTORS
 TYPE #1 - 6' x 6' - TO BE INSTALLED ON SR 72 THROUGH MOVEMENTS.
 TYPE #2 - 6' x 25' - TO BE INSTALLED ON OLD BALTIMORE PIKE LEFT-TURN AND THROUGH MOVEMENTS.
 TYPE #3 - 6' x 25' - TO BE INSTALLED ON SR 72 LEFT-TURN MOVEMENTS.
 SYSTEM - 6' x 6' - TO BE INSTALLED ON SR 72 AND OLD BALTIMORE PIKE RECEIVING LANES, AS SHOWN.
 2. ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 3. ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS. SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
 4. POLE BASES, CABINET BASE AND CONDUIT JUNCTION WELLS ARE TO BE REMOVED IN ACCORDANCE WITH SECTION 211 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
 5. ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
 6. ALL CONDUIT SHALL BE SCHEDULE 80 PVC WHEN INSTALLED BY TRENCHING OR OPEN CUT AND SCHEDULE 80 HDPE WHEN INSTALLED BY BORING UNLESS OTHERWISE NOTED.

RECOMMENDED _____ DATE: _____
 RECOMMENDED _____ DATE: _____
 RECOMMENDED *[Signature]* DATE: *12/17*
 APPROVED TRAFFIC ENGINEER _____ DATE: _____
 APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER *[Signature]* DATE: *12/17*



ADDENDUM / REVISIONS



HEP NCC,
 SR 72 AND OLD BALTIMORE PIKE
 INTERSECTION IMPROVEMENTS

CONTRACT	T201200108
COUNTY	NEW CASTLE
PERMIT NO.	N182 & NCAM166
DESIGNED BY:	D.W.C. (WRA)
CHECKED BY:	M.J.B. (WRA)

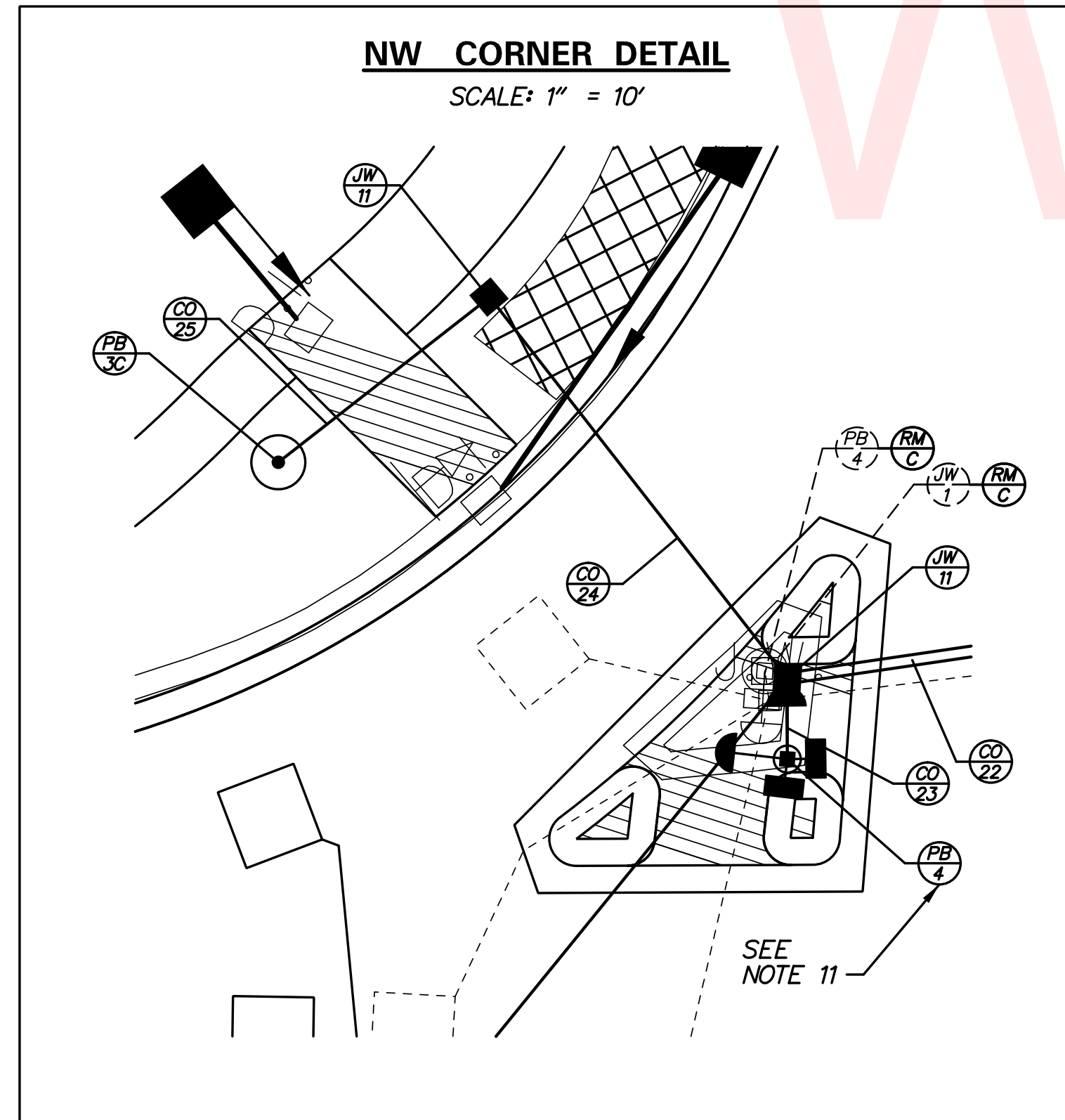
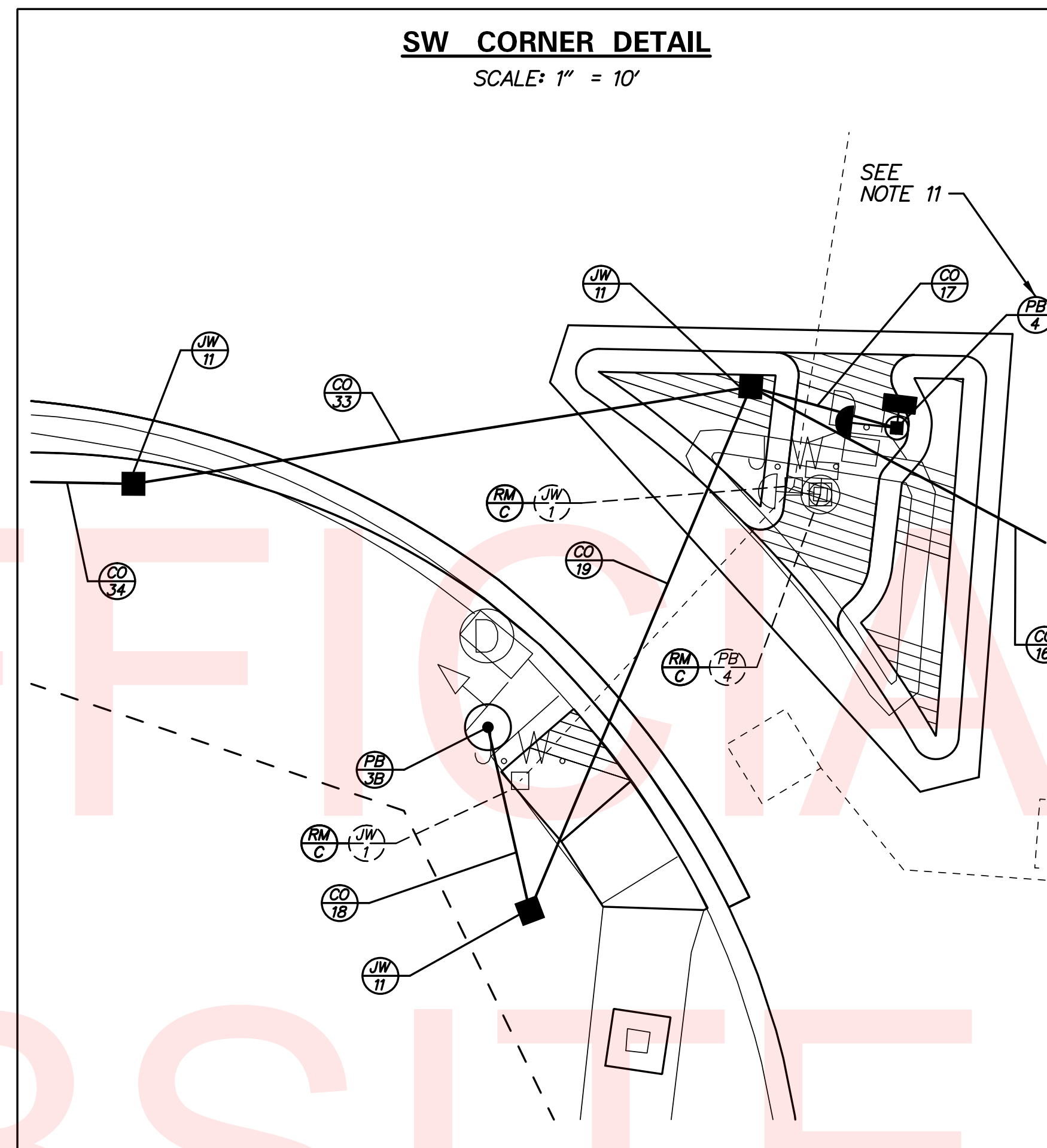
SIGNAL PLAN	SR 72 @ OLD BALTIMORE PIKE	SHEET NO.	51
TOTAL SHTS.			52

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CO#	* OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
7**	1	2.0 IN	23 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LINE SIDE]
2**	1	2.0 IN	8 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LOAD SIDE]
3	1	3.0 IN	9 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
4	3	4.0 IN	4 FT	T	[NEW (3) 9/*14, (6) 5/*14, (3) 4/*18, (21) 2/*14, (3) *6 GROUND]
5*	1	2.5 IN	XX FT	-	EX. (1) 48-S FIBER, EX. (1) 12-M FIBER
6	3	4.0 IN	55 FT	B	[NEW (3) 9/*14, (6) 5/*14, (3) 4/*18, (19) 2/*14, (3) *6 GROUND]
7	1	2.5 IN	6 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
8	1	2.5 IN	6 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
9	2	4.0 IN	106 FT	B	[NEW (2) 9/*14, (2) 5/*14, (2) 4/*18, (5) 2/*14, (2) *6 GROUND]
10	1	4.0 IN	56 FT	B	[NEW (5) 2/*14, (1) *6 GROUND]
11	1	2.5 IN	27 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
12	1	3.0 IN	14 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
13	1	4.0 IN	140 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
14	1	4.0 IN	59 FT	B	[NEW (1) 9/*14, (1) 5/*14, (1) 4/*18, (3) 2/*14, (1) *6 GROUND]
15	1	4.0 IN	108 FT	O	[NEW (2) 2/*14, (1) *6 GROUND]
16	1	4.0 IN	64 FT	B	[NEW (1) 9/*14, (1) 5/*14, (1) 4/*18, (1) *6 GROUND]
17	1	2.5 IN	11 FT	T	[NEW (1) 5/*14, (1) *6 GROUND]
18	1	3.0 IN	16 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
19	1	4.0 IN	47 FT	B	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
20	2	4.0 IN	41 FT	B	[NEW (1) 9/*14, (2) 5/*14, (1) 4/*18, (9) 2/*14, (2) *6 GROUND]
21	1	4.0 IN	211 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
22	2	4.0 IN	55 FT	B	[NEW (1) 9/*14, (2) 5/*14, (1) 4/*18, (5) 2/*14, (2) *6 GROUND]
23	1	2.5 IN	6 FT	T	[NEW (2) 5/*14, (1) *6 GROUND]
24	1	4.0 IN	37 FT	B	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
25	1	3.0 IN	17 FT	T	[NEW (1) 9/*14, (1) 4/*18, (1) *6 GROUND]
26	1	4.0 IN	68 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
27*	1	2.5 IN	195 FT	-	EX. (2) FIBER OPTIC CABLES
28	1	4.0 IN	148 FT	T	[NEW (2) 2/*14, (1) *6 GROUND]
29**	1	2.0 IN	14 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LOAD SIDE]
30	2	2.0 IN	5 FT	T	[NEW (1) CCTV CONTROL/VIDEO CABLE, (2) *6 GROUND]
31	1	3.0 IN	6 FT	T	[NEW (1) CCTV CONTROL/VIDEO CABLE, (1) *6 GROUND]
32	1	4.0 IN	50 FT	B	[NEW (5) 2/*14, (1) *6 GROUND]
33	1	4.0 IN	53 FT	B	EMPTY
34	1	4.0 IN	248 FT	T	EMPTY
35	1	4.0 IN	12 FT	T	[NEW (1) FIBER OPTIC CABLE]
36	1	4.0 IN	13 FT	T	[NEW (1) FIBER OPTIC CABLE]
37*	1	2.5 IN	255 FT	-	EX. (2) FIBER OPTIC CABLES
38*	1	2.5 IN	XX FT	-	EX. (2) FIBER OPTIC CABLES

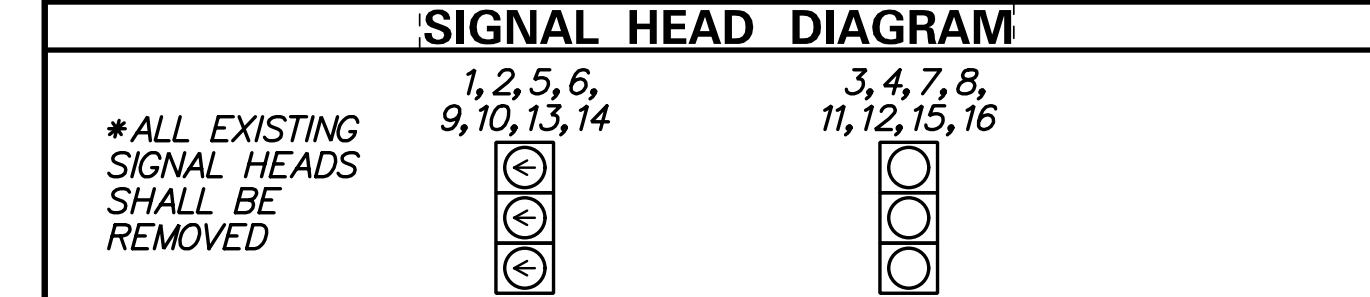
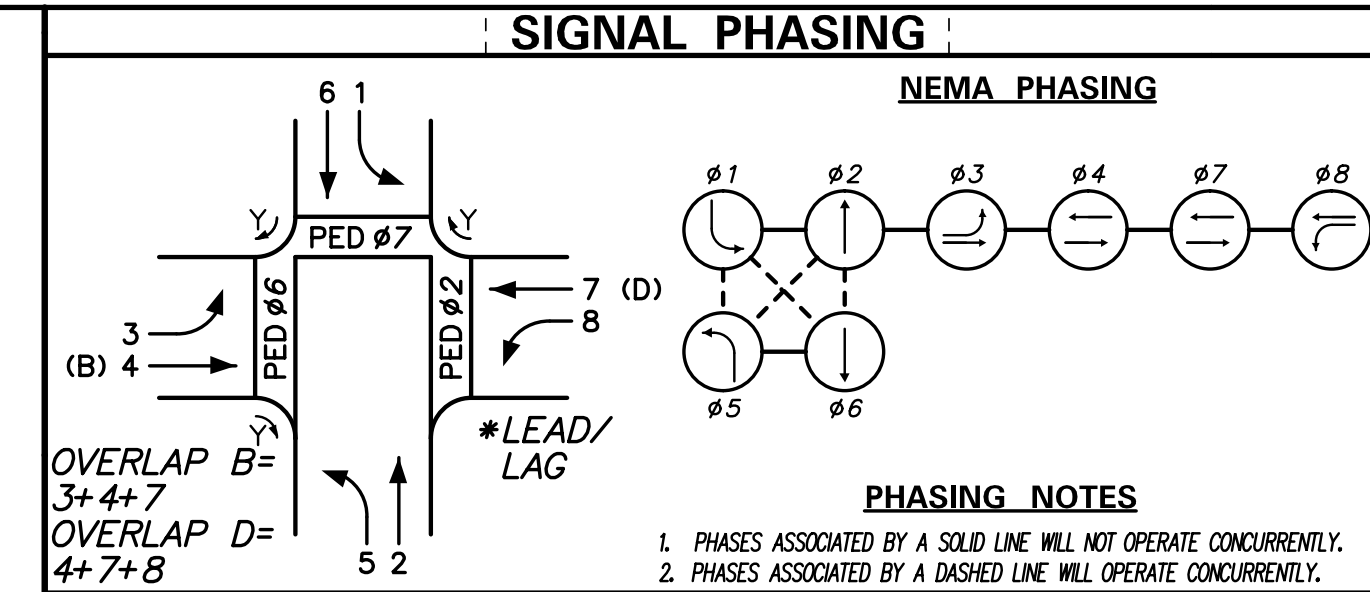
** DENOTES EXISTING CONDUIT
 * DENOTES PROPOSED RIGID GALVANIZED STEEL CONDUIT
 B = BORE, T = TRENCH, O = OPEN CUT

SEE NOTE 14
 SEE NOTE 14



NOTES:

- DELDOT TRAFFIC SHALL INSTALL A DAVIT ARM CCTV CAMERA ON THE PROPOSED SIGNAL POLE, AS SHOWN.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL UNDERGROUND SIGNAL EQUIPMENT - E.G., LOOP DETECTORS, JUNCTION WELLS, POLE BASES, CABINET BASES, AND CONDUIT. DELDOT'S TRAFFIC CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL ELECTRICAL CABLES AND ABOVE GROUND EQUIPMENT - E.G., HEADS, OPTICOM RECEIVERS, POLES, PUSHBUTTONS, OVERHEAD SIGNS, AND MAST ARMS.
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
 N1E = R10-3eR (RIGHT ARROW); NW = R10-3e-DE (NO ARROW); ENE, SE, & SW = R10-3eL (LEFT ARROW)
- DELDOT TRAFFIC SHALL INSTALL THE PUSHBUTTON SO THAT THE FACE OF THE PUSHBUTTON IS PARALLEL WITH THE CROSSING, AS SHOWN.
- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50:1 OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES, THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHOULD BE INSTALLED AT A HEIGHT OF 42 TO 48 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE DOES NOT EXCEED 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 7 FEET OR MORE THAN 10 FEET ABOVE SIDEWALK LEVEL.
- UNLESS OTHERWISE NOTED, EXISTING CONDUIT AND LOOPS TO BE ABANDONED IN PLACE.
- ALL PROPOSED SIGNAL POLES SHALL BE DELDOT MAST ARMS.
- THE CONTRACTOR SHALL CONTACT DENISE KSIAZEK (DELDOT) AT 302-981-6464 48 HOURS PRIOR TO THE INSTALLATION OF PROPOSED CONDUITS NOS. 35 AND 36 TO COORDINATE THE FIBER OPTIC CONNECTION IN THE PROPOSED SIGNAL CABINET AND CCTV CABINET.



LEGEND	
(AB)	ABANDON
(CA)	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
(CX)	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
(CO)	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
(CP)	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
(JW)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
(JX)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
(MA)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
(MX)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
(OH)	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(OP)	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(PB)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(PX)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(PL)	EXISTING POLE IDENTIFIER (* OF POLE)
(PX)	PROPOSED POLE IDENTIFIER (* OF POLE)
(RM)	REMOVE BY CONTRACTOR
(RO)	REMOVE BY OTHERS
(TC)	REMOVE BY TRAFFIC CONTRACTOR

	EXISTING SYMBOL	PROPOSED SYMBOL
JUNCTION WELL	J.W.	■
LOOP DETECTOR, TYPE 1	□	□
LOOP DETECTOR, TYPE 2	□	□
LUMINAIRE	○	○
MAST ARM	▶	▶
MICROWAVE DETECTION	◀	▶
OPTICOM RECEIVER	○	○
OVERHEAD SIGNING	—	—
PEDESTRIAN POLE/BASE	⊙	⊙
PEDESTRIAN PUSHBUTTON	→	→
PEDESTRIAN SIGNAL HEAD	⊣	⊣
RIGHT-OF-WAY	—	— R/W —
SERVICE PEDESTAL	⊞	⊞
SIGNAL CABINET	⊞	⊞
SIGNAL HEAD	→	→
SIGNAL POLE/BASE	⊙	⊙
SPAN INSULATOR	◇	◇
SPAN WIRE	— XX —	— XX —
UTILITY POLE	⊞	⊞
VIDEO DETECTION	⊞	⊞

- GENERAL SIGNAL NOTES**
- PROPOSED LOOP DETECTORS
 TYPE #1 - 6' x 6' - TO BE INSTALLED ON SR 72 THROUGH MOVEMENTS.
 TYPE #2 - 6' x 25' - TO BE INSTALLED ON OLD BALTIMORE PIKE LEFT-TURN AND THROUGH MOVEMENTS.
 TYPE #3 - 6' x 25' - TO BE INSTALLED ON SR 72 LEFT-TURN MOVEMENTS.
 TYPE #4 - 6' x 6' - TO BE INSTALLED IN SR 72 AND OLD BALTIMORE PIKE RECEIVING LANES, AS SHOWN.
 - ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 - ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS. SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
 - POLE BASES, CABINET BASE AND CONDUIT JUNCTION WELLS ARE TO BE REMOVED IN ACCORDANCE WITH SECTION 211 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
 - ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
 - ALL CONDUIT SHALL BE SCHEDULE 80 PVC WHEN INSTALLED BY TRENCHING OR OPEN CUT AND SCHEDULE 80 HDPE WHEN INSTALLED BY BORING UNLESS OTHERWISE NOTED.

RECOMMENDED _____ DATE: _____	RECOMMENDED _____ DATE: _____	RECOMMENDED <i>[Signature]</i> DATE: <i>12/17</i>	APPROVED TRAFFIC ENGINEER _____ DATE: _____	APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER <i>[Signature]</i> DATE: <i>12/17</i>
ADDENDUM / REVISIONS		SCALE 0 10 20 30 FEET		CONTRACT T201200108 COUNTY NEW CASTLE
DELAWARE DEPARTMENT OF TRANSPORTATION		HEP NCC, SR 72 AND OLD BALTIMORE PIKE INTERSECTION IMPROVEMENTS		PERMIT NO. N182 & NCAM166 DESIGNED BY: D.W.C. (WRA) CHECKED BY: M.J.B. (WRA)
				SIGNAL PLAN SR 72 @ OLD BALTIMORE PIKE SHEET NO. 52 TOTAL SHTS. 52